

***The Colonies at San Antonio***  
Upland, California

***SPECIFIC PLAN  
AS AMENDED THROUGH  
No. SPR-7: MOD#6***



Colonies Crossroads, Inc.

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## 1.0 INTRODUCTION

### 1.1 AUTHORITY AND SCOPE

California state law authorizes cities and counties to adopt Specific Plans for implementing their general plans in designated areas. The Colonies at San Antonio Specific Plan has been prepared in accordance with California Government Code Sections 65450 et al., and the applicable ordinances of the City of Upland, and will constitute the zoning for the project site. Land use standards and regulations contained within this document shall govern future development within the boundaries of this Specific Plan.

California Government Code Section 65451 states that a Specific Plan shall include text and graphics that discuss all of the following subjects:

- The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.
- The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area and needed to support the land uses described in the plan.
- Any and all development and design standards, including criteria for conserving, developing, and utilizing natural resources.
- A program of implementation measures including regulations, programs, public works projects, and financing measures to implement all of the above.
- The relationship of the Specific Plan to the General Plan.

In addition, the Specific Plan may address other subjects that in the judgment of the planning agency are necessary or desirable for implementation of the General Plan. State law permits a specific plan to be prepared, adopted, or amended in the same manner as a general plan, except that a specific plan may be adopted by resolution or by ordinance, and may be amended as often as is deemed necessary by the legislative body. State law requires that specific plan and amendments thereto be consistent with the general plan.

In response to government requirements, this Specific Plan has been prepared to provide the essential link between the policies of the City of Upland's General Plan and actual development in the project area. By functioning as a regulatory document, the Colonies at San Antonio Specific Plan provides a means of implementing and detailing the City of Upland's General Plan.

The establishment of specific performance, design, and land use guidelines will direct future development within the Specific Plan area while maintaining a high degree of development flexibility. The Colonies at San Antonio Specific Plan will assure consistent and sensitive land usage, landscape design, architectural treatment, logical development phasing, and adequate and efficient provision of public services over the life of the development plan.

## **1.2 PURPOSE AND INTENT**

This Specific Plan represents a comprehensive approach to planning for the development of the Colonies at San Antonio community. The Specific Plan provides detailed text and exhibits describing the development of the proposed project, including land use, circulation, infrastructure, grading, and landscaping concepts, as well as proposed zoning regulations.

A specific plan serves as both a City’s policy statement regarding the development of a proposed project, as well as a tool to implement the provisions of the City’s General Plan as it applies to a specific project area. As a result, the emphasis in a specific plan is on concrete standards and development criteria for use in the review of subsequent residential subdivisions and commercial site plans. The California Government Code permits the use of specific plans to regulate site development including permitted uses, densities, community design, and building size and placement. Specific plans also govern the type and extent of open space, landscaping, roadways, and the provision of infrastructure and utilities. Since the development guidelines established in a Specific Plan focus on the unique needs of a specific area, specific plans allow for greater flexibility than is possible with conventional zoning.

The purpose of the Colonies at San Antonio Specific Plan is to assist in the development of the 447.8-acre project site in a manner that will benefit local residents, regional consumers, the general public, and the City of Upland. The Specific Plan achieves this goal by establishing land uses and precise development/design controls within the limits of the Specific Plan. The customized development regulations contained in the Specific Plan address the unique characteristics of the site and surrounding properties, as well as the needs of the residential, commercial, community, and open space uses proposed for the site. This effort is intended to foster greater economic development and design opportunities than could be achieved through the use of conventional zoning and development standards. The provisions of this Specific Plan will provide the developers and City decision-makers with a set of specific guidelines designed to accomplish the objectives of the project’s sponsors and the City of Upland.

This Specific Plan has several distinct advantages. The Specific Plan is a comprehensive document that provides the project area with specific limits on the number and type of residential dwelling units, architectural and landscaping guidelines, development standards and controls, environmental performance standards, clearly defined land uses, and essential development and infrastructure phasing. Given that the Colonies at San Antonio Specific Plan will establish the standards for what is essentially an “infill” project, development of the project site without the Specific Plan would most likely result in haphazard development or underutilization of the project site.

## **1.3 PROJECT GOALS AND OBJECTIVES**

### **1.3.1 Overall Goal**

The intent of the Colonies at San Antonio Specific Plan is to provide the City of Upland, its citizens, nearby residents, and project developers with a document that identifies objectives, directions for development, and development regulations which will aid in the achievement of mutual goals. These goals, which evolved during a series of meetings and conferences with all parties concerned, include responding to public demand for a variety of residential product types and housing densities; providing compatible commercial facilities and open space areas; facilitating efficient vehicular circulation flow

within the project area while minimizing through traffic on Colonies Parkway; providing essential flood control detention facilities; and responding to existing natural and man-made site conditions. These goals, then, are the framework upon which the Specific Plan is constructed. The success of this Specific Plan depends, in large part, upon its ability to respond to and adequately meet these goals. When implemented, the Colonies at San Antonio project will prove a safe, attractive, and economically viable addition to the City of Upland and the region. Thus, the overall goal of the Colonies at San Antonio Specific Plan is:

*To establish a clearly recognizable mixed use community that provides a broad range of commercial opportunities, maintains a high quality living environment, and enhances the quality of life for present and future residents of the City of Upland.*

To further define this overall goal statement, goals and objectives are provided for Community Design, Land Use, Circulation, Infrastructure, and Project Implementation. These goals and objectives, by definition, are broad in scope. The detailed standards, guidelines, and policies that will serve to implement these goals and objectives are found in the individual chapters of this Specific Plan.

### **1.3.2 Community Design Goal and Objectives**

**a. Goal:** *To create an identifiable image of the Colonies that builds upon the positive character of Upland, and establishes a distinguishable visual identity so that one can recognize when they enter the community. Rather than limiting the new community to only one architectural style, a palette of compatible architectural styles and related design elements should be selected to encourage visual diversity and avoid monotony, creating a harmonious community.*

**b. Objectives:**

- To create an identifiable “sense of place” within the City by using a central natural open space, trails, and community landscaping to define individual neighborhoods.
- To create a visually attractive development through consistent application of architectural and landscape standards/guidelines that will encourage commercial activities and a unique residential lifestyle.
- To establish a pattern of land uses which will place regional- and freeway-oriented commercial uses north of 19<sup>th</sup> Street, and which will separate residential land uses from major noise generators, such as the freeway and 19<sup>th</sup> Street.
- To provide easy pedestrian connections between residential neighborhoods and neighborhood-level shopping opportunities that serve residents’ daily needs (e.g., market, drug store, bank).
- To establish a complementary streetscape program to reinforce the Colonies at San Antonio design identity, while supporting the buffering needs of residential uses and the visibility needs of commercial uses.
- To establish a palette of compatible residential and commercial architectural design styles which will provide a visually attractive built environment within the Specific Plan area.
- To promote the use of design features such as walls, fencing, lighting, signs, and street furniture to enhance overall community image.
- To provide identifiable project and neighborhood entries to provide a sense of arrival within the project and to individualize each neighborhood area.

- To capitalize on potential views of the central open space area and of the San Gabriel Mountains.
- To minimize the potential visual intrusiveness of the Interstate 210 (I-210) freeway from future residential areas.

### **1.3.3 Land Use Goal and Objectives**

**a. Goal:** *To provide a mixture of commercial, residential, and open space uses which is consistent with Upland's community image.*

**b. Residential Objectives:**

- To meet anticipated public demand by providing housing that is marketable in the West Valley portion of San Bernardino County.
- To provide high quality housing that will attract a broad spectrum of buyers, including attractive, upper end housing that will provide “move-up” opportunities for local residents within the project vicinity and the City of Upland.
- To establish residential neighborhoods that are identifiable by their mix of compatible architectural styles and location within the community, each with safe and convenient pedestrian and vehicular access to a central open space area, as well as to commercial and recreational opportunities.

**c. Commercial Objectives:**

- To establish a well balanced, productive, and carefully planned collection of specialized and general retail uses that serve both regional and neighborhood needs, taking advantage of the site's visibility and accessibility from the I-210 freeway and convenient location near residential uses.
- To attract retail commercial uses that will:
  - serve as a destination for a regional market (e.g., automobile dealerships adjacent to I-210 to attract shoppers from a market area substantially larger than the City of Upland);
  - provide services to travelers along I-210 on parcels located adjacent to the freeway (e.g., restaurants, automobile service stations, hotel);
  - serve community needs by providing large scale commercial uses that are visible from I-210, attracting shoppers and patrons from throughout the City of Upland and adjacent areas outside the City (e.g., home improvement, durable goods, entertainment, electronics, clothing and similar retailers); and
  - expand the retail and service options for local consumers (e.g., supermarket, drug stores, and personal services) by providing a neighborhood commercial shopping area near the residential uses.
- To provide daytime and nighttime shopping and entertainment opportunities in a safe and secure environment.
- To create employment generating opportunities for the citizens of Upland and surrounding communities.
- To augment the City's economic base by providing a variety of sales tax generating uses which minimize the leakage of taxable sales from Upland and attract taxable purchases from a regional marketplace.

#### **d. Open Space Objectives:**

- To establish a central open space area as the dominant visual feature of the Colonies at San Antonio, as well as for passive recreational opportunities.
- To provide a system of trails providing access from neighborhoods within the Colonies at San Antonio to the central open space area.
- To mitigate potential impacts on existing riparian areas.
- To create a central open space area capable of maintaining historic levels of groundwater recharge.
- To provide for the safety of Upland and San Bernardino County residents by detaining flood flows within the central open space area and the flood control basin located in the southeastern portion of the Specific Plan area.

#### **1.3.4 Circulation Goal and Objectives**

**a. Goal:** *To provide for safe and efficient vehicular, pedestrian, and bicycle movement within and through the Specific Plan area, while protecting the integrity of the Colonies at San Antonio planned community.*

#### **b. Objectives:**

- To recognize the substantial traffic carrying capacity of the I-210 freeway, maximizing its use for regional through traffic in order to minimize traffic speeds and noise along 19<sup>th</sup> Street adjacent to existing and future residential neighborhoods.
- To minimize through traffic and reduce traffic speeds on Colonies Parkway by providing a loop road connecting individual neighborhoods within the Specific Plan area, rather than a direct connection between 19<sup>th</sup> Street and residential areas to the south.
- To ensure adequate infrastructure to safely move vehicles through and within the Specific Plan area without congestion, and to provide for mitigation for the impacts of off-site traffic that will be generated by the Colonies at San Antonio development.
- To provide safe and convenient pedestrian and bicycle connections between residential neighborhoods, the central open space area, and commercial uses within the Specific Plan area.

#### **1.3.5 Infrastructure Goal and Objectives**

**a. Goal:** *To provide adequate utility, drainage, and fire protection facilities to serve the needs of all uses within the Specific Plan area.*

#### **b. Objectives:**

- To provide the water, sewer, electrical, and natural gas utility systems needed to support build out of the uses proposed for the Colonies at San Antonio Specific Plan.
- To enhance fire protection services within the eastern portion of the City of Upland by providing a site for a new fire station.
- To provide adequate capacity for the detention of local and regional drainage flows, including drainage from I-210, protecting residents within and downstream of the Colonies at San Antonio.

#### **1.3.6 Project Implementation Goal and Objectives**

a. **Goal:** *To provide a clear and comprehensive program for implementation of the development proposed by the Colonies at San Antonio Specific Plan.*

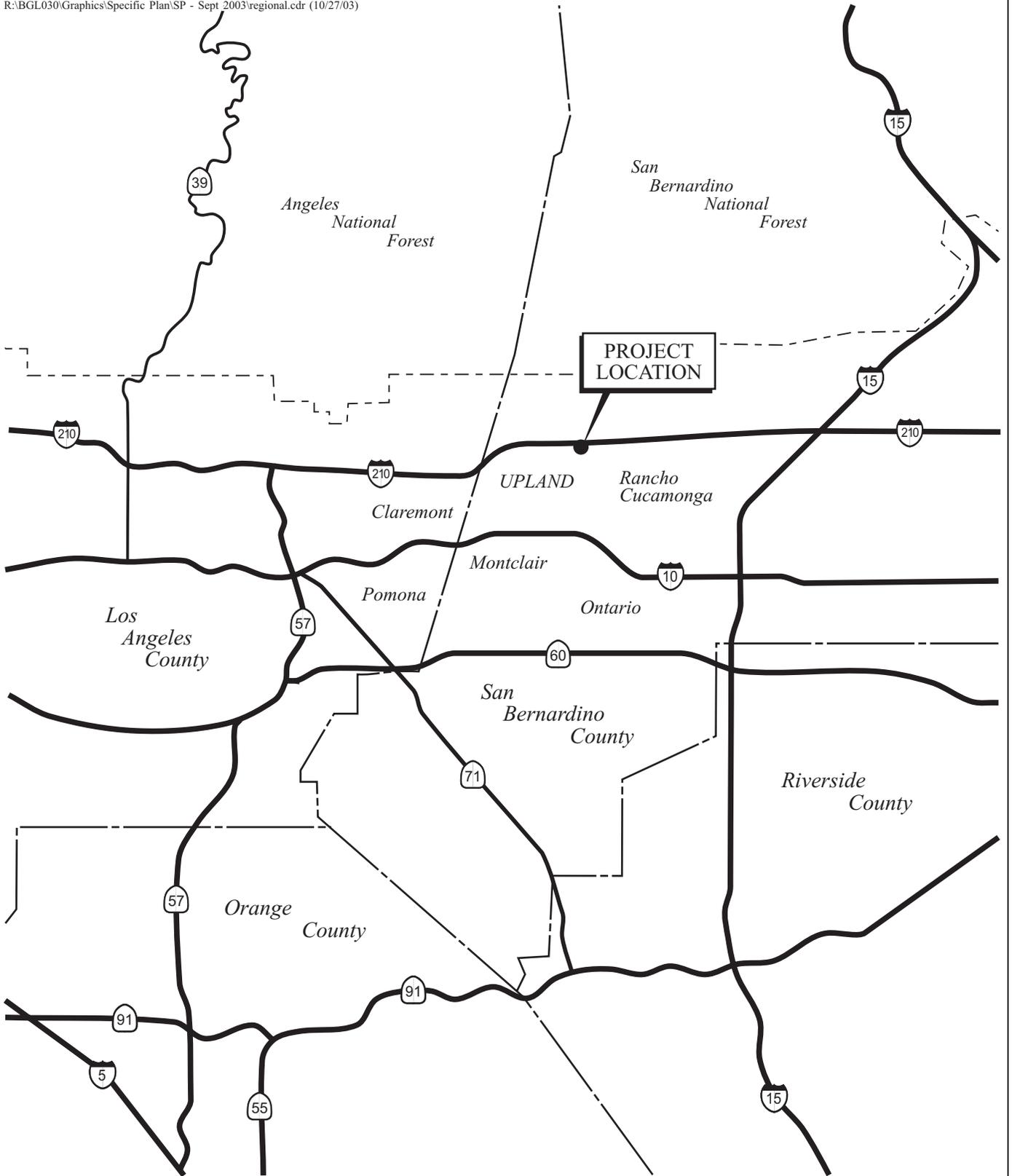
**b. Objectives:**

- To correlate the installation of water, sewer, electrical, and natural gas utility systems, and project open space and amenities with land development in a manner that is economically feasible and that ensures adequate service to residential uses and businesses within the Specific Plan area.
- To establish environmental performance standards within the Specific Plan’s development regulations chapter that minimize the potential impacts of project development.
- To ensure the ongoing maintenance of project open space and landscaped areas through identification of responsible parties.
- To provide comprehensive, understandable zoning regulations and design guidelines that will result in a high quality of development within the Specific Plan area that is consistent with the goals of the project.
- To provide a clear program for review of development proposals within individual neighborhoods and commercial planning areas, which recognizes the detailed planning and review which went into the Colonies at San Antonio Specific Plan.

## **1.4 PROJECT LOCATION AND LOCAL LAND USES**

The City of Upland is located in the western portion of San Bernardino County, north of the cities of Ontario and Montclair, and between the cities of Claremont and Rancho Cucamonga (Figure 1.1).

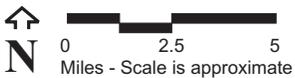
The 447.8-acre project site is located in the northeastern portion of the City of Upland, near the Upland-Rancho Cucamonga city boundary (Figure 1.2). At the time of preparation of this document, negotiations to annex a portion of the Specific Plan area (located to the north of 19<sup>th</sup> Street at the eastern Specific Plan boundary) from the City of Rancho Cucamonga to the City of Upland are underway (Figure 1.3). The Specific Plan area is known as the Cucamonga Basin “spreading grounds,” an alluvial fan configuration historically utilized to control surface water drainage. The project site is bounded by 20<sup>th</sup> Street to the north, residential areas and the Upland Hills Country Club Golf Course to the south, the Cucamonga Creek Channel to the east, and the current alignment of Campus Avenue to the west. The Specific Plan site is bisected by 19<sup>th</sup> Street and the I-210 freeway, which run in an east-west direction through the northern portion of the project site.



# REGIONAL VICINITY MAP

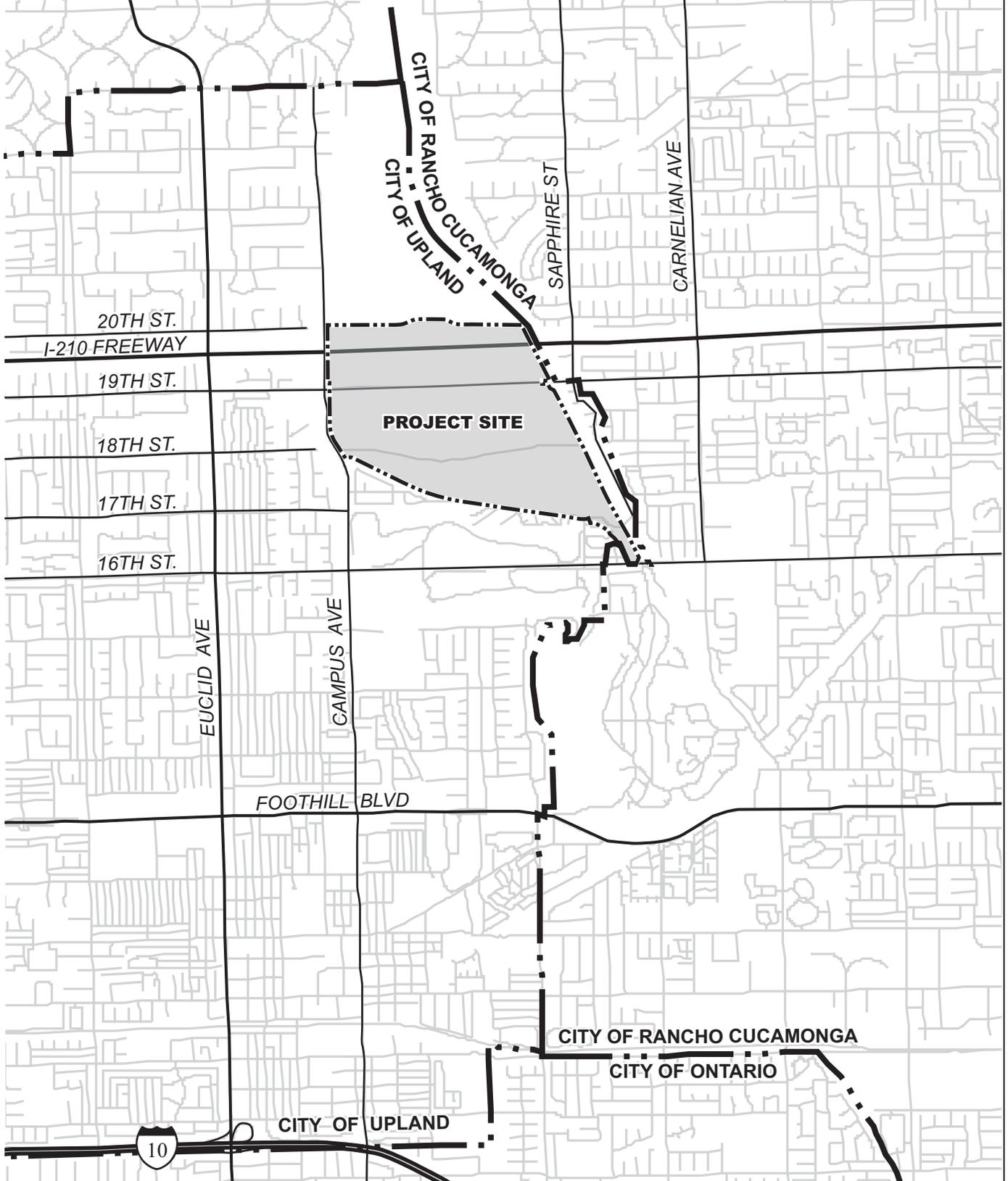
*The Colonies at San Antonio Specific Plan*

Figure 1.1



Source: LSA Associates, Inc.

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# SITE LOCATION MAP

*The Colonies at San Antonio Specific Plan*

↑  
N Not to Scale

Source: Thomas Bros. Maps and LSA Associates, Inc.

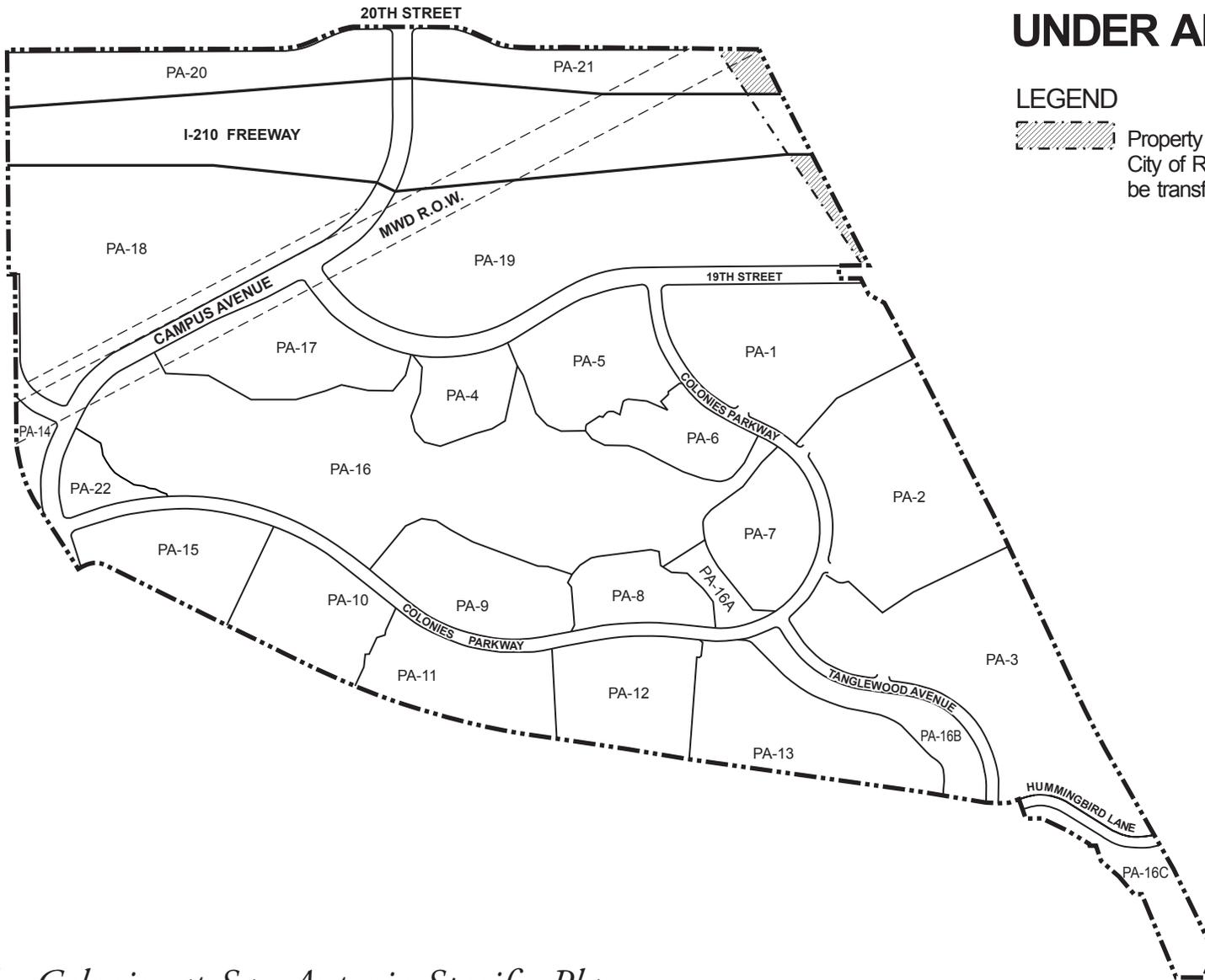
Figure 1.2

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# SPECIFIC PLAN AREA UNDER ANNEXATION

## LEGEND

 Property located within the City of Rancho Cucamonga to be transferred to the City of Upland



*The Colonies at San Antonio Specific Plan*

↑  
N Not to Scale

Source: Associated Engineers, Inc.

Figure 1.3

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### 1.4.1 Existing On-Site Land Uses Prior to the Initiation of Project Development

Figure 1.4 illustrates existing land uses at and adjacent to the project site prior to initiating its physical development. North of 19<sup>th</sup> Street, the property is essentially undisturbed and covered with native grass and dense brush. Man-made features include the following:

- 19th Street, a heavily traveled collector, running east-west through the central portion of the site, carrying traffic to and from Upland and Rancho Cucamonga.
- I-210 freeway.
- The Cucamonga Creek concrete channel along the eastern margin.
- The Holliday Rock Company sand and gravel operation, industrial asphalt facility, and truck-loading area at Campus Avenue and the proposed extension of 20th Street as shown on the City's existing Master Plan.
- The 200-foot-wide Metropolitan Water District (MWD) right-of-way trending northeast-southwest through the center of the property. A 10-foot diameter pipe is buried underground within this right-of-way.
- A few dirt roadways and trails associated primarily with the MWD right-of-way.

South of 19<sup>th</sup> Street, the project site is largely altered due to sand and gravel mining activities. Vegetation consists of native grasses and dense brush. Although mining has been discontinued, the land surface remains irregular, and there are a number of abandoned mining pits varying in size, some periodically retain water. A large basin is located in the south central portion of the property. A dense growth of bushes and small trees is present in the bottom of this basin area.

Man-made features on the southern half of the property prior to initiating the site's physical development include the following:

- An extensive, abandoned sand and gravel mining works characterizes the site. Much of the area west of the Cucamonga Creek Channel has been excavated 40 to 60 feet below the original ground surface, necessitating remedial grading to create stable slopes. In addition, mining debris and derelict equipment further hinder the use of the site.
- San Bernardino County Flood Control District flood control facilities. Some existing excavations in the southern part of the site may be from flood control activities rather than mining. Surface waters from west of the site are diverted into the existing sand and gravel pit by way of an improved channel that lies under Campus Avenue. The main area of ponding water is in the western end of the abandoned mining pit. In February 1986, the water level was at an elevation of 1,527.5 feet, approximately 72 feet below the original ground surface. A gunited rock fill levee is located about 300 feet west of the Cucamonga Creek Channel, at the eastern end of the pit. The levee base is visible above the top of the channel.
- The recently constructed outlet of the County Flood Control District's 20<sup>th</sup> Street storm drain. Additional flood control features south of 19<sup>th</sup> Street include an elevated east-west facility located south of the existing pit. A road exists along the top, and service walkways stem from it. Another significant flood control feature is the east-west levee along the southern margin of the site. A roadway runs on top of the levee and a concrete box culvert-type outlet is located halfway across. From this point south, the culvert lies underground and connects with an open air, improved trapezoidal channel at the edge of the Upland Hills Country Club Golf Course.

Isolated areas of trailings from mining operations are scattered widely. An area of old construction debris is located about 600 feet northwest of the levee outlet.

## 1.4.2 Existing Surrounding Land Uses

Existing and planned residential uses are located to the east, west, and south of the project site. Cucamonga Creek borders the east side of the project and separates the project site from single-family residential in the cities of Upland and Rancho Cucamonga. North of the site is 20th Street and Holliday Rock Company's sand and gravel extraction operation. South of the project site is Tract No. 12613, a planned community of 160 medium density (7 to 12 du/ac) dwelling units. The western boundary of the project site is formed by Campus Avenue. West of Campus Avenue is the Southern California Edison north/south transmission line. West of the transmission line are low density single-family residences and two City reservoirs between 19th and 20th Streets. Other miscellaneous uses in the area include the Great Western Rose Warehouse, groves and sheds, the extension of MWD's right-of-way, and an area of vacant land.

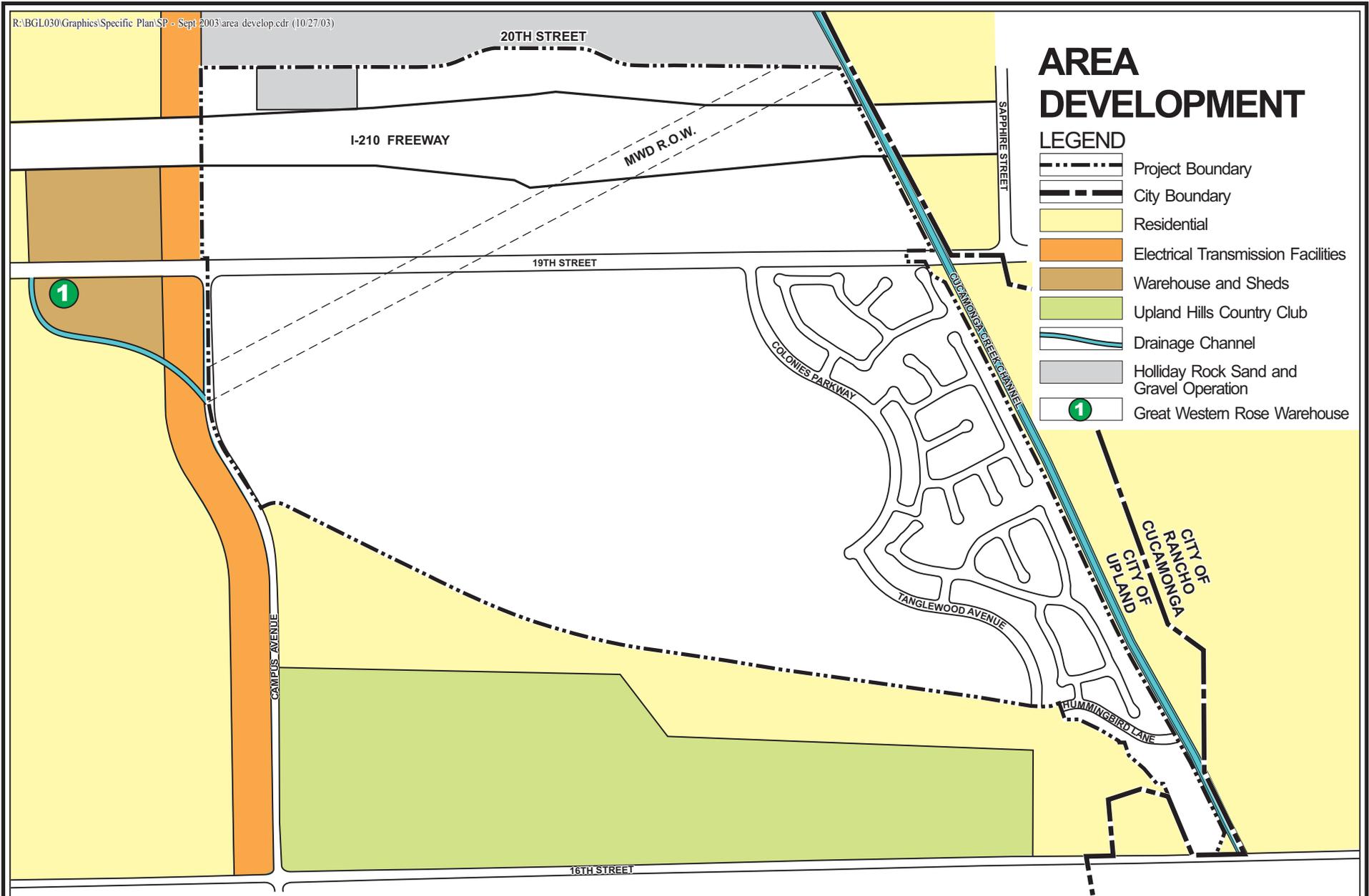
## 1.4.3 Existing General Plan and Zoning Designations

**a. General Plan.** With the exception of the I-210 right-of-way that is designated Freeway Open Space, the project site is designated Residential/Commercial-SP. The General Plan designation north of the project site is Open Space. The area immediately west of Campus Avenue is Edison Easement for an electrical transmission facility. To the south of the Specific Plan area is a designation of Single Family Residential (7 to 10 du/ac) and Multi-Family Residential (7 to 12 du/ac condominium). East of the Cucamonga Creek Channel within the City of Upland boundaries is a designation of Single Family Residential (4 to 6 du/ac). Figure 1.5 depicts on-site and adjacent General Plan designations.

**b. Zoning.** With the exception of the I-210 right-of-way, the project site is currently designated "Specific Plan" on the City's Zoning Map. North of 20<sup>th</sup> Street, the project site is bordered by land zoned "OS" (Open Space). The area east of the Cucamonga Creek Channel within the City of Upland is zoned "RS-7.5" (Residential, Single-Family, 7,500 square foot minimum lot area). Immediately west of Campus Avenue are electrical transmission facilities on land zoned "OS" (Open Space). Land immediately south of the Specific Plan site is zoned "(RM-4.4)S" (Residential, Multiple-Family, 4,400 square foot minimum lot area per dwelling) and "(RM-3.6)S" (Residential, Multiple-Family, 3,600 square foot minimum lot area per unit). These two areas have a further designation of "Supplemental Use District." Figure 1.5 illustrates the current zoning on and adjacent to the project site.

## 1.5 PROJECT BACKGROUND AND HISTORY

The Colonies at San Antonio Specific Plan was originally adopted in September 1988 as the "San Antonio Lakes" Specific Plan, along with a final Environmental Impact Report (EIR) that was certified by the City of Upland. This action included an amendment to the City's General Plan and a Zone Change, granting specific development rights for an undeveloped 440.5-acre parcel of land located in the northeastern corner of the City of Upland. The Specific Plan included a mixed-use planned community of single and multi-family residential, commercial, recreational, and community uses surrounding a flood control lake at an interchange of I-210. A key feature of this original project was a 23.5-acre lake surrounded by 16.5 acres of "private greenbelt." Residential and commercial development was intended to be implemented in six phases over an eight-year period.



# AREA DEVELOPMENT

## LEGEND

- Project Boundary
- City Boundary
- Residential
- Electrical Transmission Facilities
- Warehouse and Sheds
- Upland Hills Country Club
- Drainage Channel
- Holliday Rock Sand and Gravel Operation
- Great Western Rose Warehouse

*The Colonies at San Antonio Specific Plan*

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N Not to Scale

Figure 1.4

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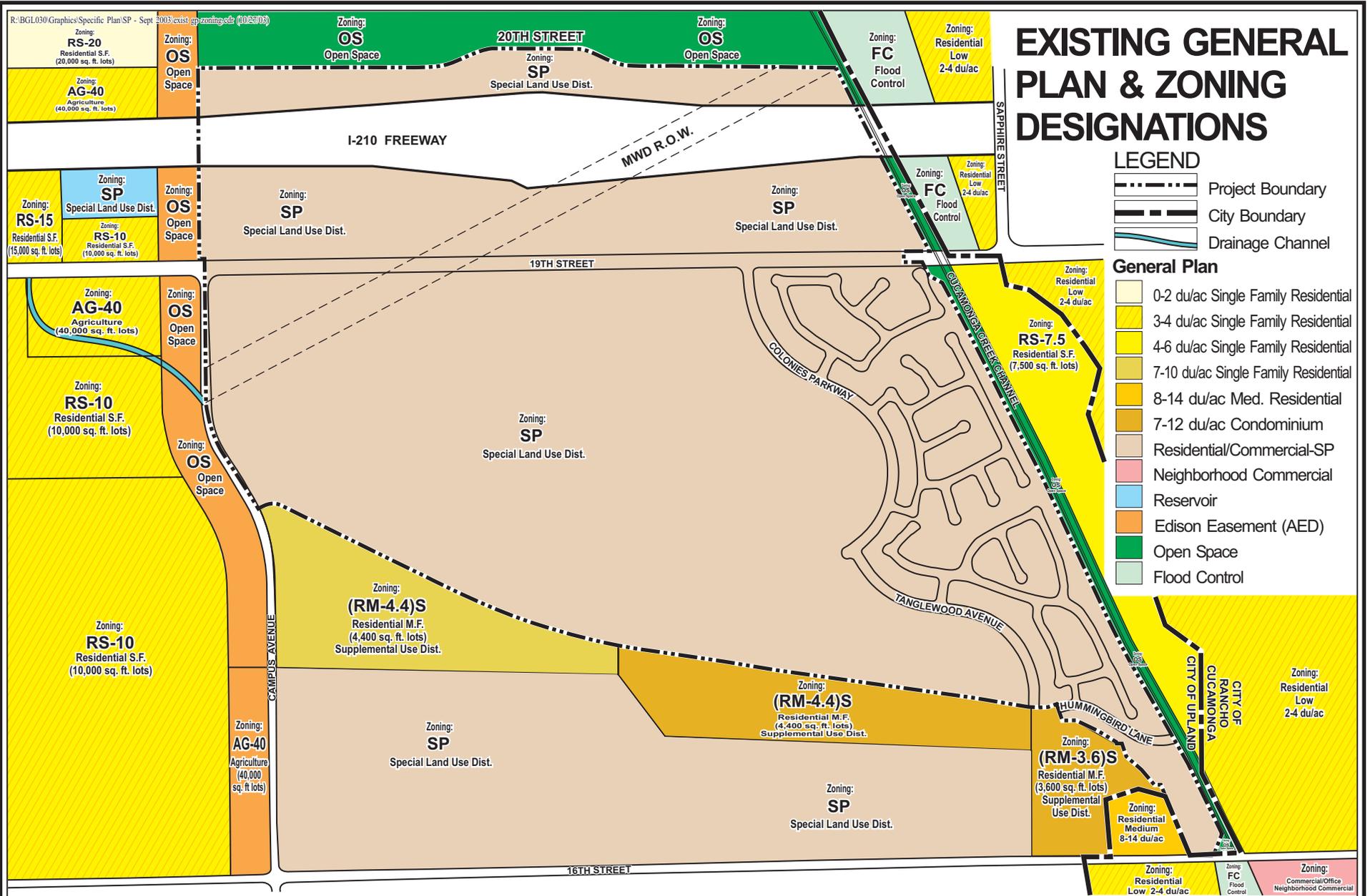
# EXISTING GENERAL PLAN & ZONING DESIGNATIONS

## LEGEND

- Project Boundary
- City Boundary
- Drainage Channel

## General Plan

- 0-2 du/ac Single Family Residential
- 3-4 du/ac Single Family Residential
- 4-6 du/ac Single Family Residential
- 7-10 du/ac Single Family Residential
- 8-14 du/ac Med. Residential
- 7-12 du/ac Condominium
- Residential/Commercial-SP
- Neighborhood Commercial
- Reservoir
- Edison Easement (AED)
- Open Space
- Flood Control



*The Colonies at San Antonio Specific Plan*

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N Not to Scale

Figure 1.5

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Soon after adoption of the Specific Plan, the regional development market and the general economy suffered a depression, and the project was abandoned. Nine years later, there was renewed interest in developing the site. However, to realistically proceed with site development required certain modifications to the original Specific Plan.

Therefore, the owner of the property proposed an amendment to the adopted Specific Plan to reflect current economic realities. The City of Upland approved the Specific Plan amendment in October 1998, and certified a Supplemental Environmental Impact Report. In general, the specific plan amendment accomplished the following:

- Changed the name of the project from “San Antonio Lakes” to “Colonies at San Antonio.”
- Modified the Specific Plan’s phasing to reverse the order of Phases 1 and 2. As a result, the development of the Specific Plan would begin along its eastern boundary and proceed westerly. The Specific Plan Amendment also modified the infrastructure development plan to correspond to the proposed modification of land development phasing.
- Reduced the minimum lot size in Planning Area 1 from 7,500 square feet to 6,000, 5,000, and 4,500 square feet.
- Changed the zoning of Planning Area 1 from Single Family Residential SFR-7.5 (7,500 sq. ft. lots) to SFR-4.5 (4,500 sq. ft. lots), SFR-5 (5,000 sq. ft. lots), and SFR-6 (6,000 sq. ft. lots).
- Realigned roadways to discourage through traffic on 19<sup>th</sup> Street through the project.
- Modified the configuration of planning areas along Tanglewood Avenue and Colonies Parkway, reflecting the roadway realignments.
- Revised certain development standards in the zoning regulations portion of the Specific Plan.

Along with the Specific Plan amendment, the City of Upland approved tentative tract maps for 305 single-family dwelling units within Phase 1 in the eastern portion of the Specific Plan area.

The October 1998 Specific Plan amendment was the first phase of a comprehensive revision to the Colonies at San Antonio Specific Plan, which was completed with a second amendment in September 2002, along with a certification of a Program Environmental Impact Report. In general, the 2002 Specific Plan amendment accomplished the following:

- Expanded commercial development to encompass nearly all of the area north of the central open space area;
- Modified the phasing plan for the Colonies to permit individual Planning Areas to be built in any order, tying their development to the provision of specific infrastructure items;
- Modified the landscape and grading plan for the central open space area to:
  - Provide for mitigation of potential impacts to riparian vegetation; and
  - Provide for maintenance of historic levels of groundwater recharge;
- Develop detailed design guidelines for single-family, multifamily, and commercial uses, as well as for potential parking structures;
- Modify sign standards to provide for expanded commercial use within the Specific Plan area; and
- Formulate a development review process to expedite subsequent project review within the Specific Plan area.

The Specific Plan was amended in 2009 to respond to changing market demands and unforeseen demands on land uses. In general, the 2009 Specific Plan amendment accomplished the following:

- Create a new three-acre planning area (PA 22) within the western portion of PA 16, encompassing land not being used or needed for flood control purposes.
- Create a process for the transfer of residential unit densities for PAs 4, 15, and 22, not to exceed a total of 1,150 residential units in the Specific Plan.
- Convert the Luxury Attached Housing Zone to a Mixed Use Zone for PA 4.
- Modify the land use for PA 15 to Flexible Residential.
- Create a land use of Flexible Residential for PA 22. Allow for neighborhood commercial uses in PA 22 if the residential unit count is met prior to development of PA 22.
- Modify the parking ratio requirements for PAs 19 and 21.
- Allow private and charter schools as a use in the Commercial Highway Zone.

## 1.6 PROJECT SUMMARY

The Specific Plan area encompasses more than 447 acres. The planning areas include single-family dwellings, luxury attached dwellings, mixed use development, neighborhood and community-serving commercial uses, natural and open space areas, drainage and flood control features, and community facilities.

At build out, the Specific Plan has been designed to accommodate up to 1,150 new dwelling units of various sizes and types on over 168 acres. A variety of housing, ranging from single-family detached homes on 6,000-square-foot lots to mixed use residential units, will provide housing options for a broad cross section of residents, as detailed in Table 1.A.

**Table 1.A – Colonies at San Antonio Land Use**

	Acres	Units
<b>Residential</b>		
Single Family Residential		
SFR 4.0	6.17	33
SFR 4.5	19.08	92
SFR.5.0	97.12	456
SFR 6.0	40.11	161
<b>Total Residential Constructed as of September 2008</b>	<b>162.48</b>	<b>742</b>
<b>Remaining Allowable Residential Units, to be constructed as follows, not to exceed 1,150:</b>	<b>19.11</b>	<b>408</b>
Flexible Residential (RF)	13.20	Maximum of 232
Mixed Use (MU)	5.64	Maximum of 350
<b>Total Residential</b>	<b>181.59</b>	<b>1,150</b>
<b>Commercial</b>		
Highway Commercial		
North of Freeway	23.68	0
South of Freeway	78.84	0
Neighborhood Commercial	12.40	0
<b>Total Commercial</b>	<b>114.92</b>	<b>0</b>

**Table 1.A - Colonies at San Antonio Land Use (Continued)**

	<b>Acres</b>	<b>Units</b>
<b><i>Public Use, Open Space, and Drainage Features</i></b>		
Fire Station Site	1.72	0
Open Space	55.64	0
Natural Drainage Path	8.53	0
Drainage Basin	5.55	0
<b>Total Public Use, Open Space and Drainage</b>	<b>71.44</b>	<b>0</b>
<b><i>Freeways/Roads</i></b>		
Freeway	45.36	0
Roadways	34.76	0
<b>Total Freeways and Roads</b>	<b>80.12</b>	<b>0</b>
<b>Total</b>	<b>447.80</b>	<b>1,150</b>

Approximately 115 acres of commercial uses are planned. Highway commercial development will be located north and south of I-210 and provide large scale retail and entertainment uses. Neighborhood commercial uses will serve the nearby residential uses.

A fire station site was dedicated to provide the necessary community facilities within the Specific Plan area. The open space located near the center of the Specific Plan area offers a focal point for the project, a passive recreational area with trails and views, and flood control facilities with a natural appearance. These uses total approximately 71 acres.

In order to achieve compatibility between adjacent land uses, the Specific Plan has been designed to consider access links, transitions between neighboring uses, views, and local landforms. The Specific Plan's roadway system has been designed to provide safe and efficient access through the various planning areas, as well as connections to I-210 and the City's existing circulation system. While unifying and connecting the various areas of the Specific Plan site, the approximately 34.66 acres of surface streets and 45.36 acres of freeway seek to minimize through traffic within the Specific Plan site and facilitate access to I-210.

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## **2.0 SPECIFIC PLAN LAND USE**

### **2.1 INTRODUCTION**

The Colonies at San Antonio Specific Plan is the result of a multi-disciplinary effort by a team of planners, architects, landscape architects, engineers, and environmental specialists. This Specific Plan considers issues such as engineering feasibility, market acceptance, economic viability, compatibility of land uses, development standards, development and infrastructure sequencing, and environmental mitigation requirements.

In developing this Specific Plan, the project team, the City of Upland, the citizens of Upland, and the developer formulated several goals and objectives. These objectives have been identified in Section 1.3 of the Specific Plan. The land use goal for the Colonies at San Antonio Specific Plan is to provide a mixture of residential, commercial, open space, and community facility uses that is consistent with Upland's community image. This is done by providing single family housing on 4,000-square-foot lots to 6,000-square-foot lots, along with areas that provide for a mix of commercial and luxury attached housing products and other multi-family residential product types. The Specific Plan emphasizes high quality housing with compatible architectural styles, an open space amenity, and good access to commercial uses and the freeway. The residential units within the Specific Plan area also provide opportunities for residents within the project vicinity and the City of Upland to "move-up" to the higher end housing that will characterize the residential units within the Plan. Upper end housing opportunities are provided by the luxury attached housing units and the single family units on 6,000-square-foot lots. The residential neighborhoods within the Colonies at San Antonio Specific Plan provide clusters of homes distinguished by lot size and architectural mix, yet a harmonious character is created through the use of streetscape, entry monumentation, and site furnishings.

The Specific Plan's commercial objectives include establishing a well balanced, productive, and carefully planned collection of specialized and general retail uses that serve both regional and neighborhood needs, taking advantage of the site's visibility and accessibility from the I-210 freeway and convenient location near residential uses. The commercial uses proposed for the Specific Plan include those that will serve the regional market, such as automobile dealerships that will draw from a market area beyond the City limits. The site's location along I-210 provides the opportunity to cater to those traveling through the City of Upland by placing uses such as restaurants, automobile gas stations, and a hotel in locations with visibility and easy access. Home improvement, durable goods, electronics, clothing, and similar retailers will serve Specific Plan residents, the City of Upland, and other nearby residential areas. Commercial development within the Colonies at San Antonio increases the choice of retail and service uses for local consumers, as well as providing shopping and entertainment opportunities in a pleasant, safe, and secure environment both during the day and evening. Commercial development within the Specific Plan adds jobs to the local economy, and provides sales tax generating uses to increase the City of Upland's economic base and decrease the leakage of taxable sales to other jurisdictions.

The aim of the Specific Plan's design approach is to establish a recognizable community identity that builds upon Upland's existing character. The Colonies at San Antonio creates a unique identity by focusing on its open space amenity, which includes pedestrian and bicycle trails linking to the Plan's residential neighborhoods and local shopping opportunities. By laying out land uses appropriately, views

of the central open space amenity and San Gabriel Mountains are emphasized, and the Specific Plan's residential neighborhoods are buffered from I-210. The Specific Plan's architectural and landscape standards and guidelines will also produce a quality development that is pleasing to residents, commercial patrons, and the citizens of Upland (for details, see Chapters 8.0 -Design Guidelines and 9.0 - Landscaping). Design elements such as walls, fences, light fixtures, signage, and street furniture complement the overall architectural and landscape elements, enhancing the community image. This is further strengthened by appropriate project and neighborhood entries that help create an identifiable sense of arrival.

The open space feature at the center of the Specific Plan area serves multiple purposes by enhancing riparian habitats through careful design of both functional and aesthetic elements (for details, see Chapter 9.0 - Landscaping). This area not only creates an identity and focus for the surrounding residential areas, but serves as a passive recreational opportunity with a system of trails. Besides providing riparian habitat, the open space area provides opportunities for groundwater recharge, and protects the safety of nearby residents by detaining flood flows in conjunction with the flood control basin located in the southeastern portion of the Specific Plan area.

## **2.2 SUMMARY OF DEVELOPMENT TYPES**

Development types within the Colonies at San Antonio Specific Plan are summarized below. A detailed description of each of the individual Planning Areas, including the acreage, location, uses, and access, can be found in Section 2.3.

### **2.2.1 Residential**

The Specific Plan proposes a mix of single-family residential neighborhoods and attached housing products, encompassing up to 1,150 residential dwelling units. Included in the planned housing mix are the following.

- *Single-family Residential (Minimum Lot Size of 6,000 Square Feet)*: This residential type occupies 40.11 acres. 161 dwelling units were developed.
- *Single-family Residential (Minimum Lot Size of 5,000 Square Feet)*: This residential type occupies 97.12 acres, located within eight Planning Areas. 456 dwelling units were developed.
- *Single-family Residential (Minimum Lot Size of 4,500 Square Feet)*: 92 units of this housing type were developed on 19.08 acres.
- *Single-family Residential (Minimum Lot Size of 4,000 Square Feet)*: 33 dwelling units of this type were developed on 6.17 acres.
- *Mixed Use (MU)*: It is planned that multi-story mixed use units will be developed on 5.64 acres on Planning Area 4. The buildings will be developed with complementary commercial uses and up to 350 residential units, depending on the buildout of units in PAs 15 and 22. If a total of 1,150 units are developed within the Specific Plan area prior to build-out of the Mixed Use area (Planning Area 4), it will be developed as Neighborhood Commercial, as detailed in Section 11.2.
- *Flexible Residential (RF)*. This residential type is planned to occupy a total of 13.23 acres (Planning Areas 15 and 22). Up to 160 units will be developed on Planning Area 15, with the flexibility to develop up to 16 units per acre; up to 72 units will be developed on Planning Area 22, with the flexibility to build up to 24 units per acre, based on project viability and market demand. This designation allows site plans to be developed with residential units that are either attached or detached housing, based on market demand. Regulations for specific residential types are included in this Specific Plan. If a total of 1,150 units are developed within the Specific Plan area prior to build-out

of Planning Area 15, a Specific Plan Amendment will be required to allow more units for development in the Specific Plan, or to change the allowable land use on that Planning Area. If a total of 1,150 units are developed within the Specific Plan area prior to build-out of Planning Area 22, it will be developed as Neighborhood Commercial, as detailed in Section 11.2.

### **2.2.2 Commercial**

Neighborhood, community, and regional commercial uses will be conveniently located adjacent to the I-210 freeway. The following commercial land uses are proposed.

- *Highway Commercial:* Approximately 102.52 acres of highway-related commercial uses are planned, focusing on the I-210/Campus Avenue interchange. This location provides high visibility and convenient access to and from I-210 and internal roadways. Development of highway commercial uses has been planned to provide commercial and entertainment opportunities for Specific Plan residents and regional consumers, and to act as a buffer between I-210 and residential uses located south of 19<sup>th</sup> Street. It is anticipated that the planned commercial development will provide a variety of employment opportunities for area residents, as well as generate significant sales and property tax revenues for the City of Upland.
- *Neighborhood Commercial Development:* Over 15 acres of neighborhood commercial uses are envisioned to provide retail, service, and convenience uses within a neighborhood center designed to meet the daily needs of the immediate community. Pedestrian links between the neighborhood commercial center and the adjacent residential area will facilitate access.

### **2.2.3 Open Space**

In order to adequately provide local and regional flood control, groundwater recharge, and provide recreational amenities to project residents, the core of the Specific Plan area will be devoted to open space use. This area will provide a focal point for the residential areas bordering the open space, and an aesthetic amenity for the entire project site. Within this area, acreage will be devoted to restored wetland and riparian habitats passive open space, and water conservation facilities. Trails are located so as not to conflict with the area's habitat, water conservation, and flood control functions. The open space area will also be designed to handle local and regional storm water flows that currently run through the site, and thereby ensure that the capacity of the adjacent Cucamonga Creek Channel will not be exceeded. The outlet to the central open space area will be designed so as to retain water and promote groundwater conservation, as well as restoration of wetlands and riparian habitats. (Details regarding the open space design and function can be found in Chapter 9.0 - Landscaping and Chapter 4.0 - Drainage. Provisions regarding open space maintenance can be found in Section 11.3.2.)

### **2.2.4 Fire Station**

A public fire station site is proposed on the west edge of the Specific Plan area. The fire station and emergency equipment will be managed and maintained by the City of Upland Fire Department.

### **2.2.5 Flood Control and Water Conservation Facilities**

As discussed in Section 2.2.3, the central open space area will fulfill water conservation and flood control functions. Located in the southeastern portion of the Specific Plan area, a natural drainage path and storm water retention basin will be established as flood control features in addition to the

## 2.2.6 Roadways

Approximately 80.12 acres of roadways are included within the Specific Plan area. This figure includes 34.76 acres of backbone surface roadways (19th Street, Colonies Parkway, Campus Avenue, and Tanglewood Avenue), in addition to local roadways within individual residential Planning Areas and 45.36 acres within the right-of-way for I-210. The roadway network will be compatible with the existing City roadway network, and will provide an appropriate level of access to the project site from adjacent land uses in the Cities of Upland and Rancho Cucamonga, while providing project residents with safe, efficient, and convenient access to internal land uses.

## 2.3 LAND USE AND DEVELOPMENT PLAN

To facilitate the discussion and analysis of pertinent issues, the Specific Plan area has been partitioned into 22 separate Planning Areas, based on land use and/or location within the project site. Figure 2.1 illustrates the location and configuration of the Planning Areas within the Specific Plan.

Each Planning Area has been designed to facilitate vehicular access from primary roadways, provide pedestrian access where appropriate, and ensure compatibility between adjacent uses through orientation and design. Following is a description and diagram for each of the Planning Areas within the Specific Plan, including its location, acreage, planned land uses, and physical elements such as intersections, trails, and buffers. (See Chapter 9.0 for discussion of the design concepts for primary, secondary, commercial, and neighborhood intersection monumentation; community trail and greenbelt; community trail and greenbelt at utility easement; enhanced parkway; community open space hiking trail; overlook; bio-swale drainage channel; outlet structures; Cucamonga Creek Channel buffer; and freeway landscaping at the Caltrans right-of-way referenced in the following planning area descriptions and figures.)

### 2.3.1 Planning Area 1

**Location.** Planning Area 1 (19.08 acres) is located along the eastern boundary of the Specific Plan, bordered by 19<sup>th</sup> Street to the north, the regional trail along Cucamonga Creek Channel to the east, Planning Area 2 to the south, and Colonies Parkway to the west (Figure 2.2).

**Use.** This Planning Area features detached single family residences with minimum lot sizes of 4,500 square feet.

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 1 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 1 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

# LAND USE PLAN

## Legend

Project Boundary

Land Use:	Acreage:	Units:
SFR 6.0 (Single Family Residential)	40.11	161
SFR 5.0	97.12	456
SFR 4.5	19.08	92
SFR 4.0	6.17	33
MU (Mixed Use)	5.91	up to 350
RF (Flexible Residential)	13.20	up to 317

**Total:** 181.59 1,150

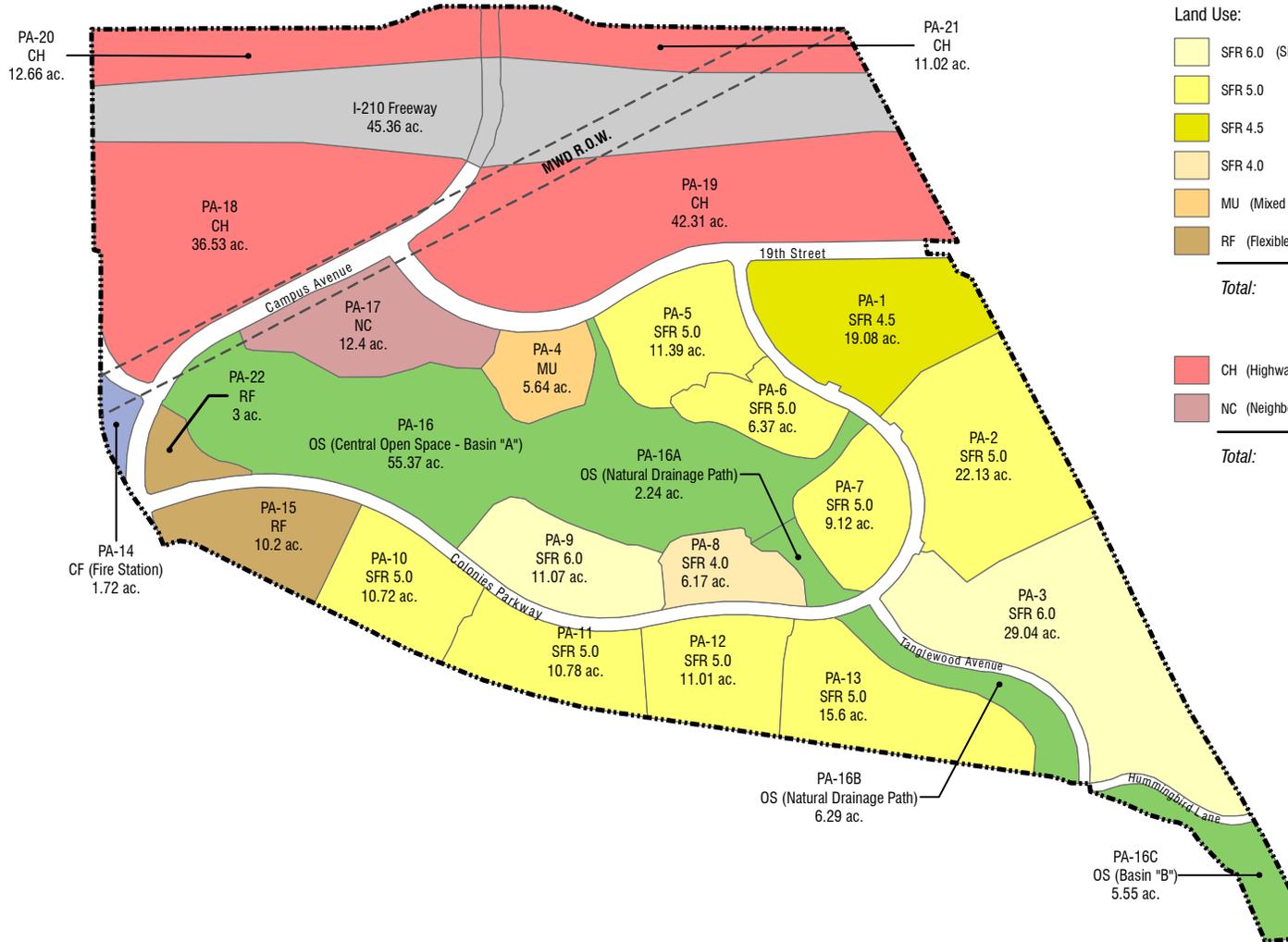
	Acreage:	Sq. Ft.:
CH (Highway Commercial)	102.52	1,070,519
NC (Neighborhood Commercial)	12.40	129,481

**Total:** 114.92 1,200,000

	Acreage:
OS (Central Open Space)	55.37
OS (Natural Drainage Path)	8.53
OS (Basin "B")	5.55
CF (Fire Station)	1.72
I-210 Freeway	45.36
Roads	34.76

**Total:** 151.29

**GRAND TOTAL:** 447.80



## The Colonies at San Antonio Specific Plan

Not to Scale

Source: LSA Associates, Inc.

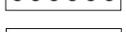
Figure 2.1

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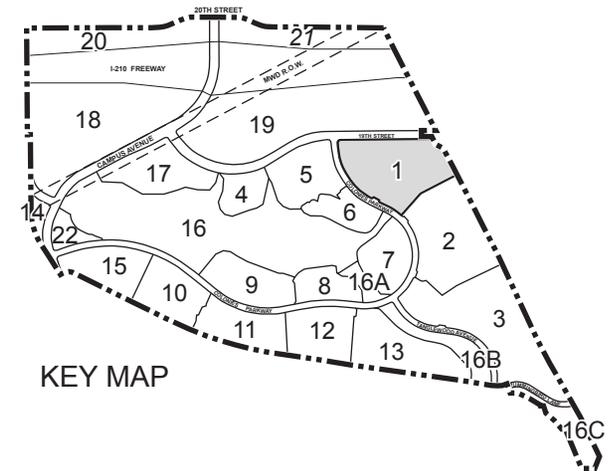
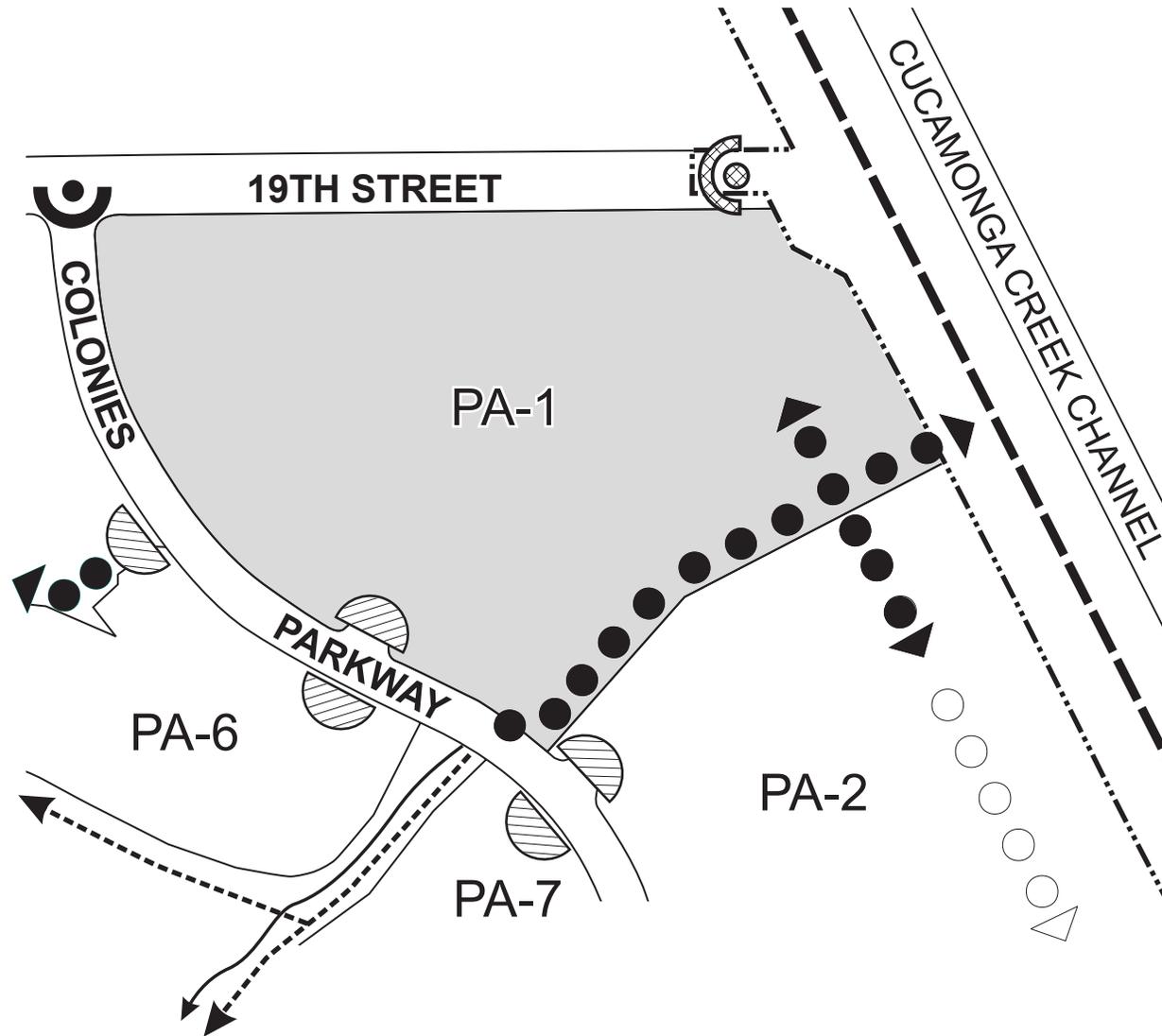
# PLANNING AREA 1

SFR 4.5 (Single Family Residential)  
19.08 Acres

## LEGEND

-  Project Boundary
-  Regional Hiking and Bike Trail
-  Community Open Space Hiking Trail
-  Community Trail and Greenbelt
-  Enhanced Parkway
-  Bio-Swale Drainage Channel
-  Service Drive
-  Primary Intersection
-  Easterly Project Entry
-  Neighborhood Intersection  
*(locations of are conceptual only)*

NOTE: See Chapter 9 for design concepts.



KEY MAP

Figure 2.2

*The Colonies at San Antonio Specific Plan*

↑  
N Not to Scale

Source: RJM Design Group.

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**Vehicular and Pedestrian Access.** Access into Planning Area 1 is provided by a neighborhood intersection, identified with a monument sign, at the west side of the Planning Area at Colonies Parkway (for neighborhood intersection design, see Section 9.3 - Streetscapes). This access leads to a community trail and greenbelt follows the southern boundary of Planning Area 1, and intersects with the enhanced parkway (which runs along the local residential street that parallels the Cucamonga Creek Channel) (for trail design details, see Section 9.2.2 - Trails and for enhanced parkway design, see Section 9.5 - Residential Landscape Concepts). Just to the east of the Specific Plan and Planning Area 1 boundary lies the Cucamonga Creek regional hiking and bike trail.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 Water and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards including minimum lot area, minimum lot dimensions, maximum height, parking space requirements, and setback requirements, see Chapter 7.0 - Development Regulations.

**Architectural Styles.** See Section 8.3 - Residential Design Guidelines.

**Landscaping.** Primary intersection monumentation is located at the intersection of 19<sup>th</sup> Street and Colonies Parkway and project entry monumentation is located along 19<sup>th</sup> Street at the easterly project boundary (for primary intersection and easterly project entry monumentation design see Section 9.3 - Streetscapes). For bio-swale drainage channel description, see Section 9.2.1 - Open Space Concept. For residential streetscape landscaping, see Chapter 9.0 - Landscaping.

### 2.3.2 Planning Area 2

**Location.** Planning Area 2 (22.13 acres) is located on the east side of the Colonies at San Antonio, south of Planning Area 1 and north of Planning Area 3 (Figure 2.3). To the west is Colonies Parkway and to the east is Cucamonga Creek Channel.

**Use.** Planning Area 2 features detached single family residential uses on lots at least 5,000 square feet in size.

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 2 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 2 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

**Vehicular and Pedestrian Access.** Access is via two neighborhood intersections (identified with monument signs) at Colonies Parkway, which lead to a network of local residential streets within the Planning Area (for roadway cross sections, see Chapter 3.0 - Transportation and for neighborhood intersection design, see Section 9.3 - Streetscapes). Community trails and greenbelts flank both the north and south boundaries of the Planning Area, continuing across Colonies Parkway and an offshoot leading south to Tanglewood Avenue (for trail design details, see Section 9.2.2 - Trails). Near the eastern edge of the Planning Area is an enhanced parkway, including a pedestrian walkway, that parallels the Cucamonga Creek Channel located to the east (for enhanced parkway design, see Section 9.5 – Residential Landscape Concepts). The Cucamonga Creek regional hiking and bike trail is located immediately next to the creek channel just outside the boundaries of Planning Area 2.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 Water and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards including minimum lot area, minimum lot dimensions, maximum height, parking space requirements, and setback requirements, see Chapter 7.0 - Development Regulations.

**Architectural Styles.** See Section 8.3 - Residential Design Guidelines.

**Landscaping.** For residential streetscape landscaping, see Chapter 9.0 - Landscaping.

### 2.3.3 Planning Area 3

**Location.** Planning Area 3 (29.04 acres) is located in the southeasterly corner of the Specific Plan area (Figure 2.4). To the north is Planning Area 2 and to the south is drainage basin B (Planning Area 16C).

**Use.** Planning Area 3 features detached single-family residential units on minimum 6,000 square feet lots.

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 3 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 3 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

**Vehicular and Pedestrian Access.** Planning Area 3 is accessed by two neighborhood intersections (identified with monument signs), one located at Tanglewood Avenue approximately halfway between Colonies Parkway and Hummingbird Lane and the other located off Hummingbird Lane between Tanglewood Avenue and the eastern Specific Plan boundary. (For neighborhood intersection design, see Section 9.3 - Streetscapes and for roadway cross sections, see Chapter 3.0 Transportation). Just beyond the northwest edge of the Planning Area is another neighborhood intersection, but this serves Planning Area 2 located to the north.

A community trail and greenbelt follows the northern boundary of Planning Area 3, coming across Colonies Parkway and heading east to the Cucamonga Creek regional hiking and bike trail just outside the Specific Plan boundary. This community trail and greenbelt also diverts from the north boundary of the Planning Area and connects to the western neighborhood intersection located at Tanglewood Avenue. The enhanced parkway, a local residential street featuring enhanced landscaping and a pedestrian walkway, is located near the eastern edge of the Planning Area. It connects to the Cucamonga Creek regional hiking and bike trail, located just east of Planning Area 3 outside of the Specific Plan area, via a community trail. (For trail design details see, Section 9.2.2 -Trails and for enhanced parkway design, see Section 9.5 - Residential Landscape Concepts).

**Development Regulations.** For development standards including minimum lot area, minimum lot dimensions, maximum height, parking space requirements, and setback requirements, see Chapter 7.0 - Development Regulations.

**Architectural Styles.** See Section 8.3 - Residential Design Guidelines.

**Landscaping.** Two intersections border the Planning Area; however, they do not provide access but do feature landscape designs and monumentation. At the southwest corner of the Planning Area, primary intersection monumentation is located on the northeast and northwest corners of Tanglewood Avenue and Hummingbird Lane. On the west edge of the Planning Area, secondary intersection monumentation is located at Tanglewood Avenue and Colonies Parkway. (For primary intersection and secondary intersection design, see Section 9.3 -Streetscapes. For residential streetscape landscaping, see Chapter 9.0 - Landscaping).

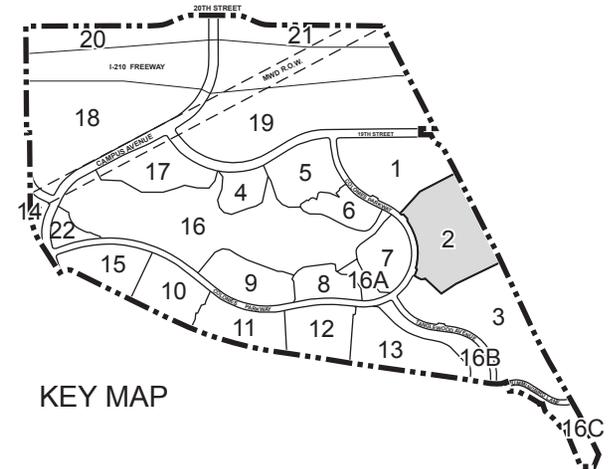
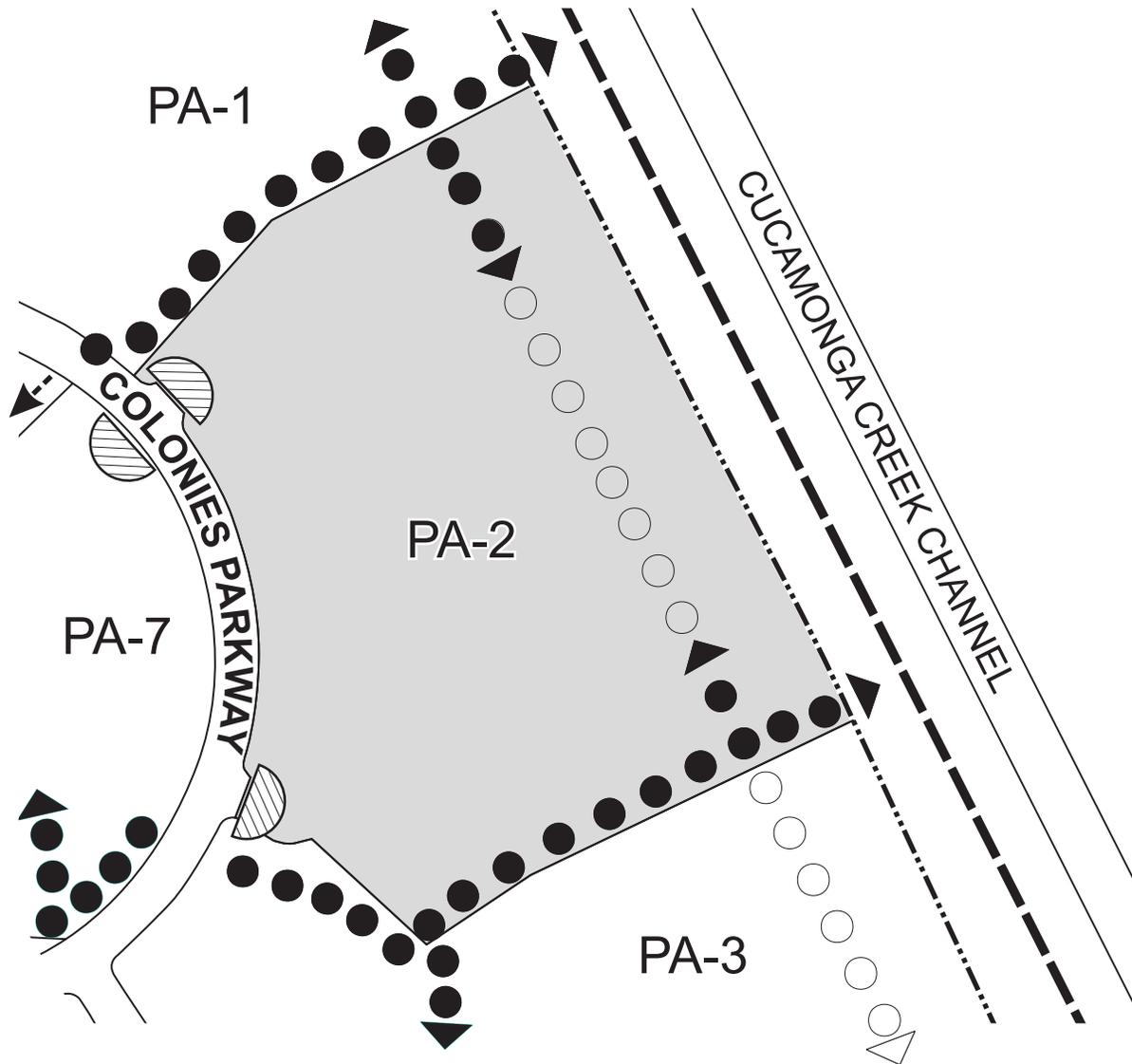
# PLANNING AREA 2

SFR 5.0 (Single Family Residential)  
22.13 Acres

## LEGEND

-  Project Boundary
-  Regional Hiking and Bike Trail
-  Community Trail and Greenbelt
-  Enhanced Parkway
-  Community Open Space Hiking Trail
-  Service Drive
-  Neighborhood Intersection  
*(locations of are conceptual only.)*

NOTE: See Chapter 9 for design concepts.



KEY MAP

*The Colonies at San Antonio Specific Plan*

↑  
N Not to Scale

Source: RJM Design Group.

Figure 2.3

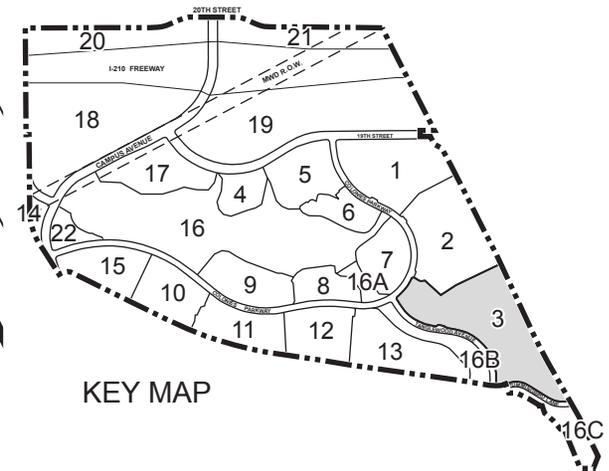
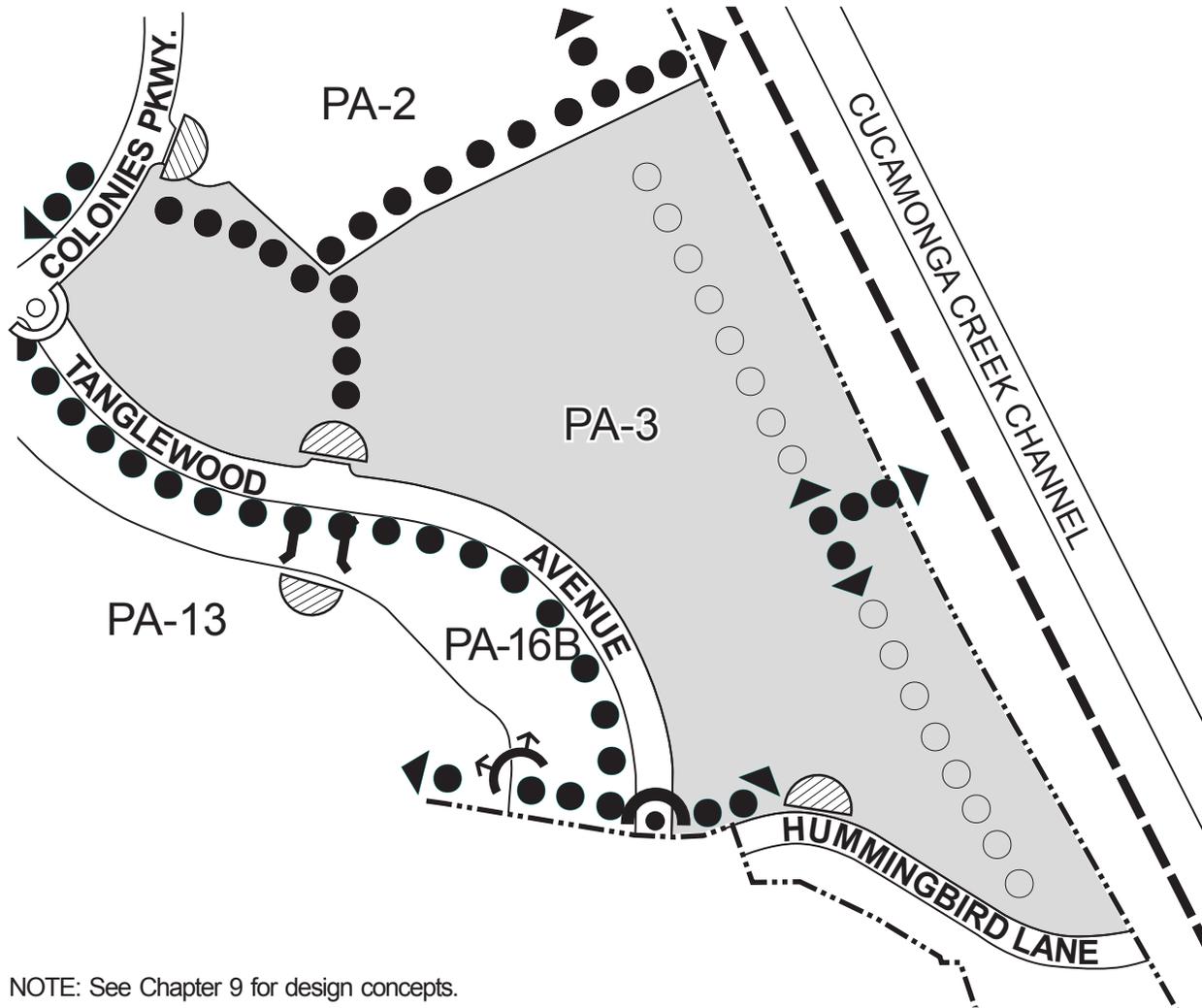
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# PLANNING AREA 3

SFR 6.0 (Single Family Residential)  
29.04 Acres

## LEGEND

-  Project Boundary
-  Regional Hiking and Bike Trail
-  Community Open Space Hiking Trail
-  Community Trail and Greenbelt
-  Enhanced Parkway
-  Overlook
-  Primary Intersection
-  Secondary Intersection
-  Neighborhood Intersection  
*(locations of are conceptual only)*
-  Bridge



NOTE: See Chapter 9 for design concepts.

### The Colonies at San Antonio Specific Plan

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N Not to Scale

Source: RJM Design Group.

Figure 2.4

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### 2.3.4 Planning Area 4

**Location.** Planning Area 4 is located north of the central open space area, fronting on the south side of 19<sup>th</sup> Street, between the neighborhood commercial uses of Planning Area 17 to the west and the single family residential uses of Planning Area 5 to the east (Figure 2.5).

**Use.** Planning Area 4 provides for approximately 6 acres of mixed use development in a multistory setting. The buildings will be developed with complementary commercial uses and up to 350 residential units (62 dwelling units per acre), as detailed in Table 7.B. Additionally, structured parking will be accommodated in the buildings. The residential uses within the Planning Area 4 will be subject to the existing Covenants, Conditions, and Restrictions (CC&R's) for the Colonies Master Association. Further; additional CC&R's governing the use of the residential components within Planning Area 4 must be reviewed and approved by the Community Development Director, and must include provisions addressing the relationship of governance of commercial uses, if applicable. The density and mixed use of the proposed buildings is appropriate given the proximity of the sites to the 19<sup>th</sup> Street arterial and the Highway Commercial designation across the street. If a total of 1,150 units are developed within the Specific Plan area prior to build-out of the Mixed Use area (Planning Area 4), it will be developed as Neighborhood Commercial, as detailed in Section 11.2.

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 4 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a "✓" in the Table for Planning Area 4 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

**Vehicular and Pedestrian Access.** Planning Area 4 has a vehicular access point along 19<sup>th</sup> Street, toward the eastern side of the Planning Area's northern border. A community open space hiking trail lies just outside the eastern, southern, and western perimeters of Planning Area 4 within the adjacent open space area (for trail design details, see Section 9.2.2 - Trails). An overlook showcasing views of the open space area is located just to the west of the Planning Area (for overlook design, see Section 9.2.1 - Open Space Concept).

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Development Regulations.** For residential and neighborhood commercial development standards including maximum height, parking space requirements, and setback requirements, see Chapter 7.0 - Development Regulations.

**Architectural Styles.** The mixed use buildings will be designed as multi-story, terraced buildings that reflect the slope of the land towards the central open space area. These buildings will have great view potential towards the open space amenity. (For details regarding development regulation, see Section 7.5 – General Development Standards. For details regarding architectural styles, see Section 8.3 - Residential Design Guidelines).

**Landscaping.** Bio-swale drainage channels are designed to collect storm and nuisance water runoff from adjacent developments and are located within the open space area to the west and east of Planning Area 4 (for bio-swale drainage channel design see Section 9.2.1 - Open Space Concept). For residential landscaping provisions see Chapter 9.0 - Landscaping).

### 2.3.5 Planning Area 5

**Location.** Planning Area 5 is located north of the central open space area at the southwest corner of the intersection of 19<sup>th</sup> Street and Colonies Parkway between Planning Areas 4 and 6 (Figure 2.6).

**Use.** Planning Area 5 consists of over 11 acres of detached single family residential uses on minimum 5,000-square-foot lots.

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 5 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 5 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

**Vehicular and Pedestrian Access.** A neighborhood intersection identified with a monument sign, located at the southeast boundary, will provide access to Planning Area 5 and its local residential streets. (For neighborhood intersection design, see Section 9.3 - Streetscapes). A community trail and greenbelt is located at the boundary with Planning Area 6 to the south (for trail design details, see Section 9.2.2 - Trails). This trail leads from the neighborhood intersection to the open space area, where it transitions into a community open space hiking trail. The general location of this transition between the two types of trails also features an overlook towards the open space area (for overlook design, see Section 9.2.1 - Open Space Concept). At the northwest corner of the Planning Area, another community open space hiking trail leads into and out of the open space area.

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 Water and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards including minimum lot area, minimum lot dimensions, maximum height, parking space requirements, and setback requirements, see Chapter 7.0 - Development Regulations.

**Architectural Styles.** See Section 8.3 - Residential Design Guidelines.

**Landscaping.** Primary intersection monumentation is located at the intersection of 19th Street and Colonies Parkway (for primary intersection design, see Section 9.3 - Streetscapes). Bio-swales provide for storm and nuisance water runoff from adjacent developments via channels located to the south and east of Planning Area 5 (for bio-swale drainage channel design, see Section 9.2.1 - Open Space Concept). For residential streetscape landscaping, see Chapter 9.0 - Landscaping.

### 2.3.6 Planning Area 6

**Location.** Planning Area 6 consists of over 6 acres located northeast of the open space area adjacent to Planning Area 5 west of Colonies Parkway (Figure 2.7).

**Use.** Planning Area 6 is planned for detached single family residential units on lots at least 5,000 square feet in size.

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 6 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 6 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

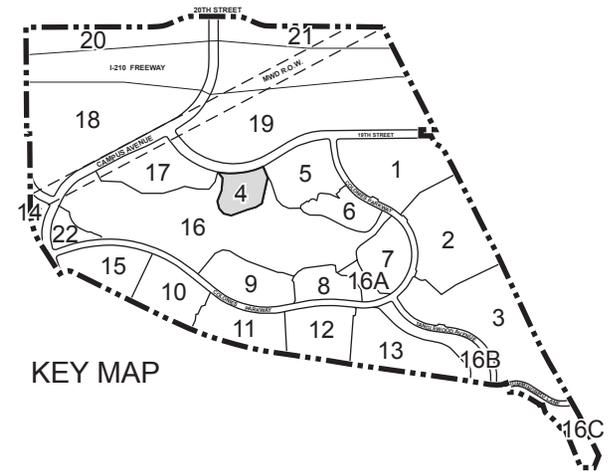
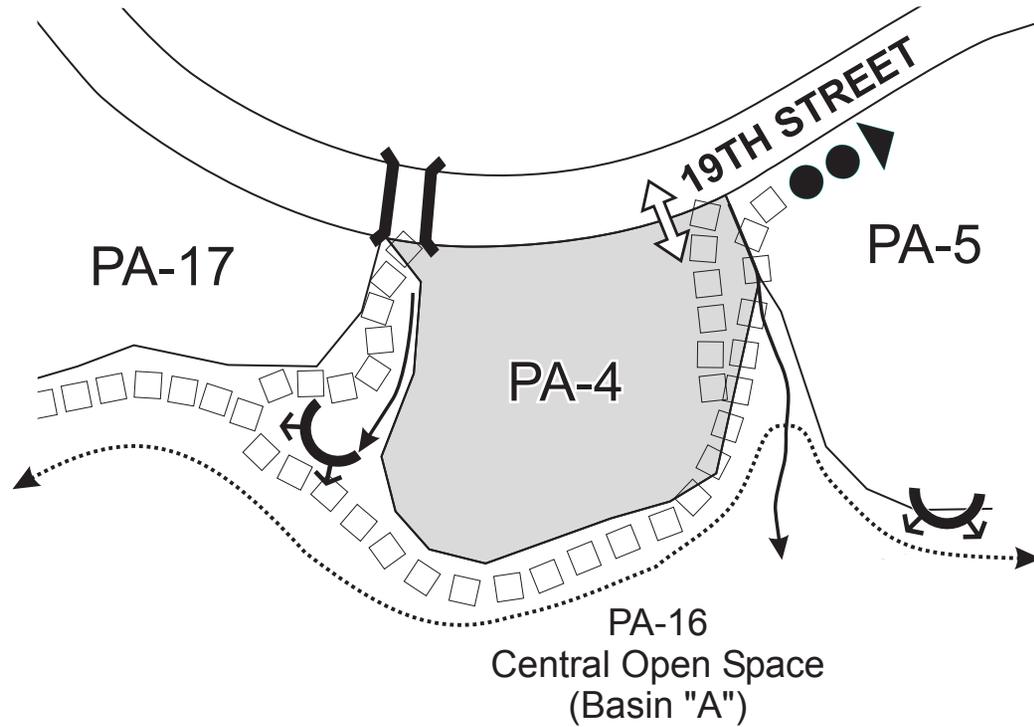
# PLANNING AREA 4

MU (Mixed Use)  
Up to 350 Units  
5.64 Acres

## LEGEND

-  Community Trail and Greenbelt
-  Community Open Space Hiking Trail
-  Bio-Swale Drainage Channel
-  Service Drive
-  Overlook
-  Vehicular Access
-  Enhanced Pedestrian Crossing

NOTE: See Chapter 9 for design concepts.



KEY MAP

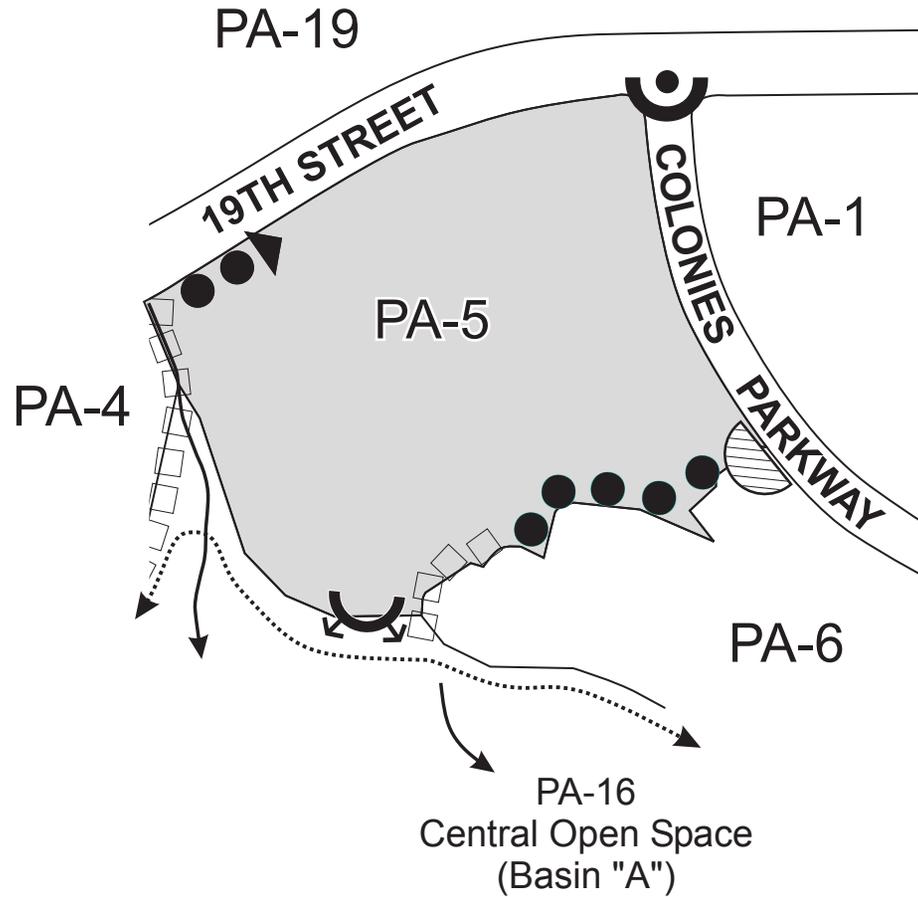
Figure 2.5

*The Colonies at San Antonio Specific Plan*

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Source: RJM Design Group.

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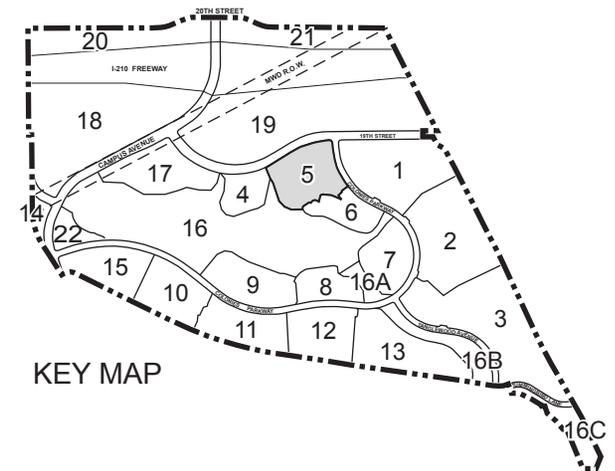
## PLANNING AREA 5

SFR 5.0 (Single Family Residential)  
11.39 Acres

### LEGEND

-  Community Trail and Greenbelt
-  Community Open Space Hiking Trail
-  Bio-Swale Drainage Channel
-  Service Drive
-  Overlook
-  Primary Intersection
-  Neighborhood Intersection  
*(locations of are conceptual only.)*

NOTE: See Chapter 9 for design concepts.



KEY MAP

Figure 2.6

*The Colonies at San Antonio Specific Plan*

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Source: RJM Design Group.

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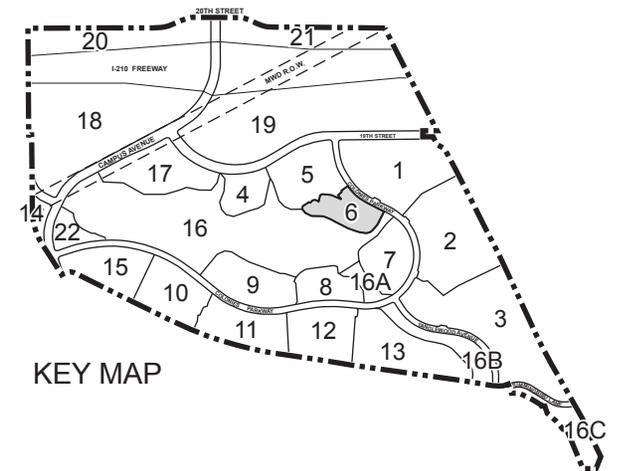
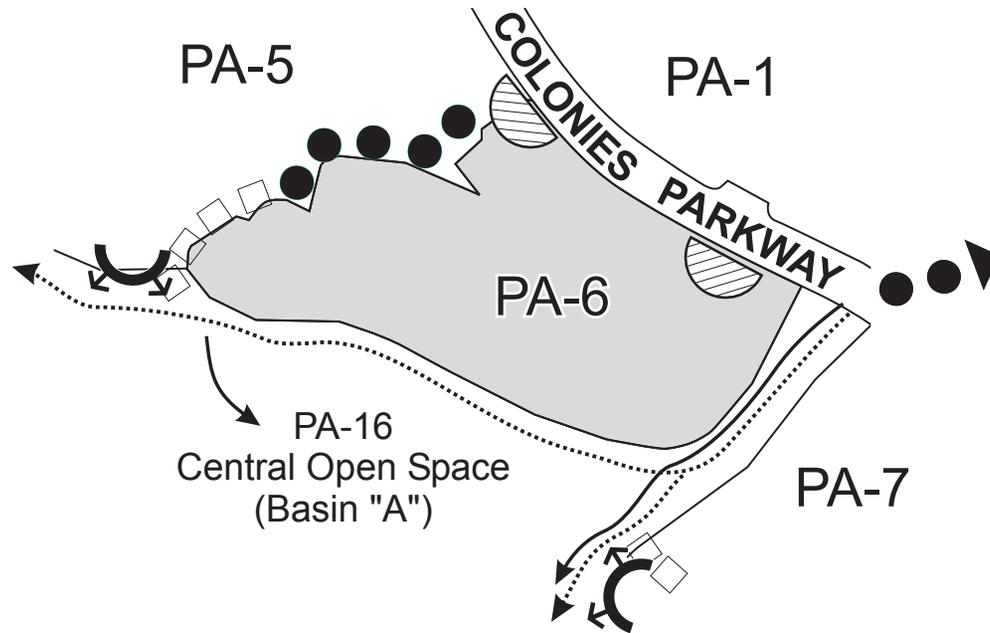
# PLANNING AREA 6

SFR 5.0 (Single Family Residential)  
6.37 Acres

## LEGEND

-  Community Trail and Greenbelt
-  Community Open Space Hiking Trail
-  Bio-Swale Drainage Channel
-  Service Drive
-  Overlook
-  Neighborhood Intersection  
*(locations of are conceptual only.)*

NOTE: See Chapter 9 for design concepts.



KEY MAP

Figure 2.7

*The Colonies at San Antonio Specific Plan*

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Source: RJM Design Group.

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**Vehicular and Pedestrian Access.** Planning Area 6 is accessed by a neighborhood intersection, identified with a monument sign, at the west side of Colonies Parkway (for neighborhood intersection design see, Section 9.3 - Streetscapes). Another neighborhood intersection is located just north of the Planning Area 6 boundary; however, this serves Planning Area 5 to the north. A community trail and greenbelt follows the northern perimeter of the Planning Area and a community open space hiking trail is located within the adjacent open space to the southeast and southwest of the Planning Area. (For trail design details, see Section 9.2.2 - Trails.)

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 - Water and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards including minimum lot area, minimum lot dimensions, maximum height, parking space requirements, and setback requirements, see Chapter 7.0 - Development Regulations.

**Architectural Styles.** See Section 8.3 - Residential Design Guidelines.

**Landscaping.** Bio-swale drainage channels located on the north and northwest perimeters and below the southern perimeter within the open space area collect storm and nuisance water runoff from adjacent development (for bio-swale drainage channel design, see Section 9.2.1 - Open Space Concept). For residential streetscape landscaping, see Chapter 9.0 - Landscaping.

### 2.3.7 Planning Area 7

**Location.** Planning Area 7 is located east of the central open space area, northeast of the drainage pathway leading from Basin A in the central open space area, and to the west of Colonies Parkway (Figure 2.8).

**Use.** The approximately 9-acre Planning Area provides for detached single family residential units on minimum 5,000-square-foot lots.

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 7 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 7 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

**Vehicular and Pedestrian Access.** Access into the Planning Area is via a neighborhood intersection identified with a monument sign (for neighborhood intersection design, see Section 9.3 -Streetscapes), located off Colonies Parkway along the northeast boundary of Planning Area 7. An emergency vehicle access is located in the southern portion of the Planning Area at the terminus of Tanglewood Avenue.

A community open space hiking trail lies just beyond the northwest perimeter, and provides access into the Planning Area (for trail design details, see Section 9.2.2 - Trails). An overlook feature provides for an amenity area with views of the central open space and the rock basin outlet of the floodway located in Planning Area 16A immediately to the southwest (for overlook, open space, and basin outlet design, see Section 9.2.1 - Open Space Concept). A roadway bridge crossing featuring a community trail and greenbelt over the floodway below is located at Colonies Parkway at the southern end of Planning Area 7.

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 Water

and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards including minimum lot area, minimum lot dimensions, maximum height, parking space requirements, and setback requirements, see Chapter 7.0 - Development Regulations.

**Architectural Styles.** See Section 8.3 - Residential Design Guidelines.

**Landscaping.** Adjacent to the community open space hiking trail located northwest of the Planning Area, a bio-swale drainage channel collects storm and nuisance water runoff (for bio-swale drainage channel design, see Section 9.2.1 - Open Space Concept). For residential streetscape landscaping, see Chapter 9.0 - Landscaping.

### 2.3.8 Planning Area 8

**Location.** Planning Area 8 lies south of the central open space area between Planning Area 9 to the west and the floodway in Planning Area 16A to the east (Figure 2.9).

**Use.** Planning Area 8 consists of over 6 acres planned for detached single family residential units on minimum 4,000-square-foot lots.

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 8 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 8 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

**Vehicular and Pedestrian Access.** Colonies Parkway provides access into Planning Area 8 through a neighborhood intersection, identified with a monument sign (for neighborhood intersection design, see Section 9.3 - Streetscapes). A community open space hiking trail follows the perimeter of the Planning Area within the open space area, with access directly into the Planning Area provided by trail spurs (for trail design details, see Section 9.2.2 - Trails).

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 Water and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards including minimum lot area, minimum lot dimensions, maximum height, parking space requirements, and setback requirements, see Chapter 7.0 - Development Regulations.

**Architectural Styles.** See Section 8.3 - Residential Design Guidelines.

**Landscaping.** Near the northwest side of the Planning Area, a bio-swale drainage channel directs water runoff into Basin A within the central open space area (for bio-swale drainage channel design, see Section 9.2.1 - Open Space Concept). For residential streetscape landscaping, see Chapter 9.0 - Landscaping.

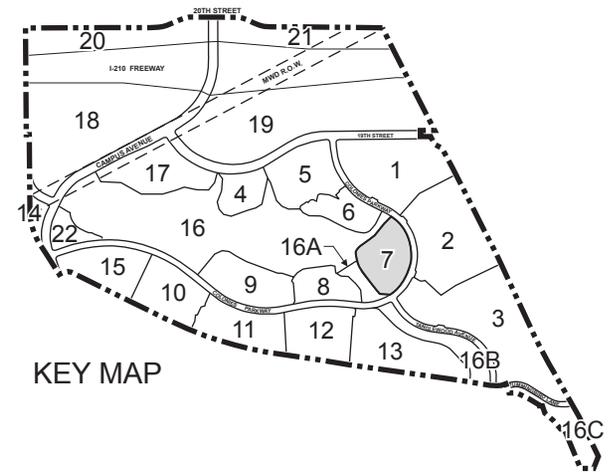
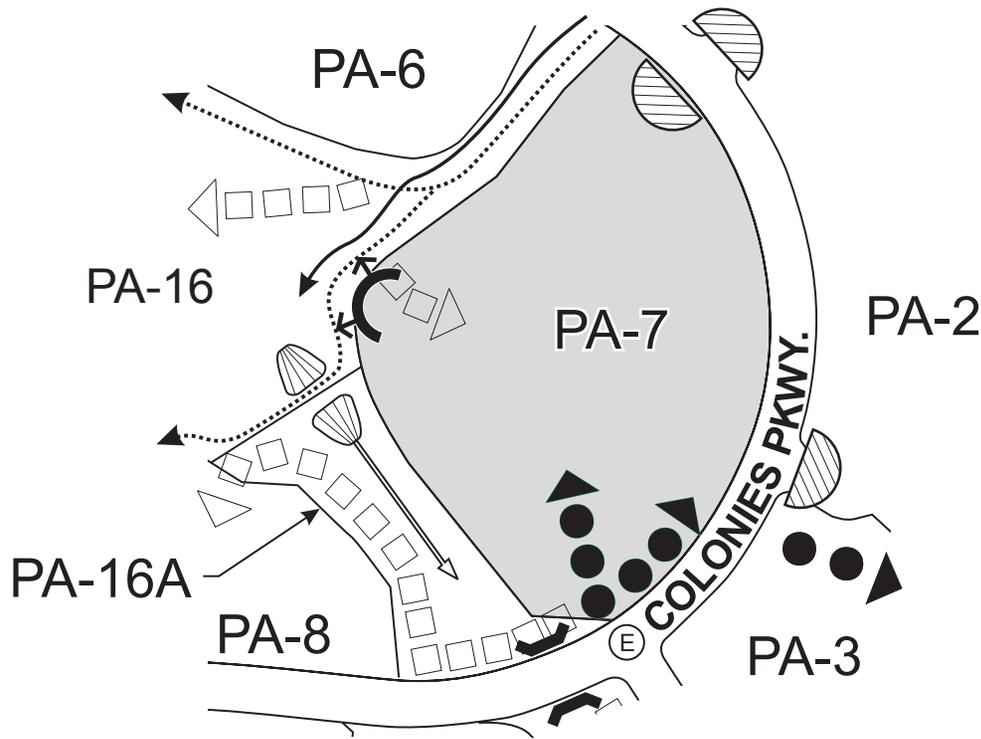
# PLANNING AREA 7

SFR 5.0 (Single Family Residential)  
9.12 Acres

## LEGEND

-  Community Trail and Greenbelt
-  Community Open Space Hiking Trail
-  Bio-Swale Drainage Channel
-  Floodway
-  Service Drive
-  Emergency Vehicles Access
-  Basin Outlet
-  Overlook
-  Neighborhood Intersection  
*(locations of are conceptual only.)*
-  Bridge

NOTE: See Chapter 9 for design concepts.



KEY MAP

*The Colonies at San Antonio Specific Plan*

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N Not to Scale

Source: RJM Design Group.

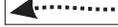
Figure 2.8

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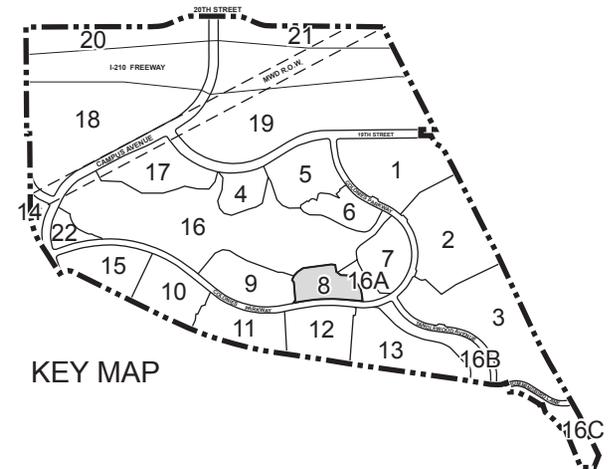
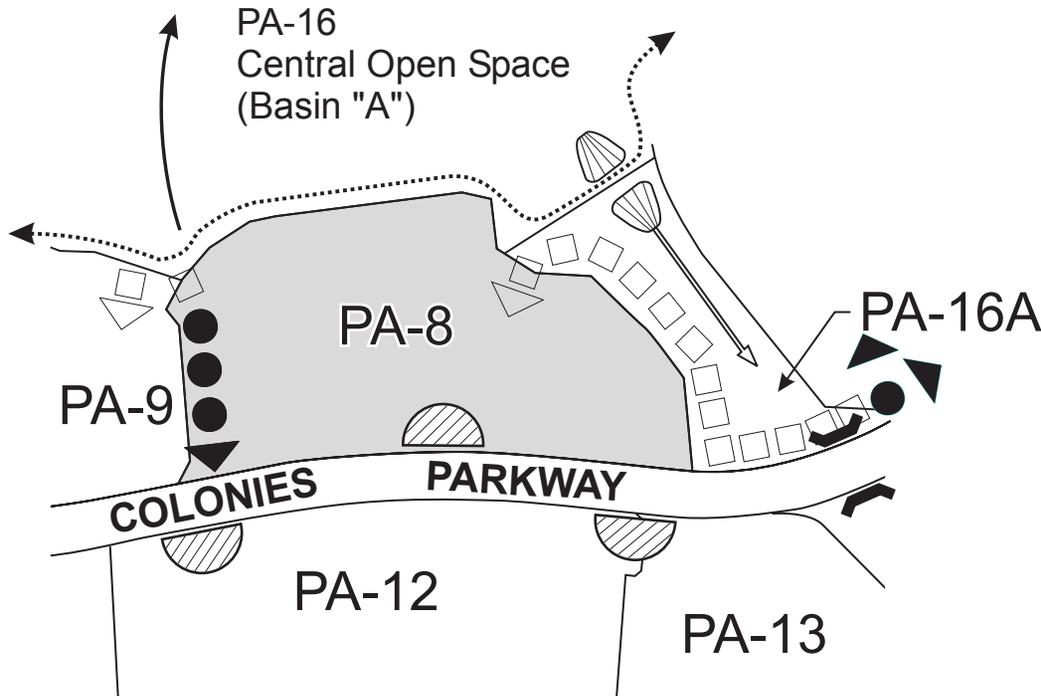
# PLANNING AREA 8

SFR 4.0 (Single Family Residential)  
6.17 Acres

## LEGEND

-  Community Trail and Greenbelt
-  Community Open Space Hiking Trail
-  Bio-Swale Drainage Channel
-  Floodway
-  Service Drive
-  Basin Outlet
-  Neighborhood Intersection  
*(locations of are conceptual only.)*
-  Bridge

NOTE: See Chapter 9 for design concepts.



KEY MAP

*The Colonies at San Antonio Specific Plan*

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N Not to Scale

Source: RJM Design Group.

Figure 2.9

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### 2.3.9 Planning Area 9

**Location.** Planning Area 9 is located south of the central open space area and west of Planning Area 8 (Figure 2.10).

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 9 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 9 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

**Vehicular and Pedestrian Access.** Access to Planning Area 9 is provided by two neighborhood intersections, identified with monument signs, located on the south side of the Planning Area at Colonies Parkway (for neighborhood intersection design, see Section 9.3 - Streetscapes). A community open space hiking trail, located within the adjacent open space area, follows the north and west perimeter of Planning Area 9 (for trail design details, see Section 9.2.2 - Trails). This hiking trail transitions into a community trail and greenbelt on the east side. An overlook amenity along the northern edge of the Planning Area allows views into the central open space area (for overlook and open space design, see Section 9.2.1 - Open Space Concept).

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 Water and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards including minimum lot area, minimum lot dimensions, maximum height, parking space requirements, and setback requirements, see Chapter 7.0 - Development Regulations.

**Architectural Styles.** See Section 8.3 -Residential Design Guidelines.

**Landscaping.** For residential streetscape landscaping, see Chapter 9.0 - Landscaping.

### 2.3.10 Planning Area 10

**Location.** Planning Area 10 is situated south of Colonies Parkway at the southern edge of the Specific Plan area (Figure 2.11).

**Use.** Close to 11 acres in size, it features detached single family residential uses on minimum 5,000-square-foot lots.

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 10 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 10 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

**Vehicular and Pedestrian Access.** Vehicular access is provided by a neighborhood intersection, identified with a monument sign, located in the northern portion of the Planning Area off Colonies Parkway (for neighborhood intersection design, see Section 9.3 - Streetscapes). A community trail and greenbelt is located at the southern perimeter of the Planning Area along a utility easement (for trail design details, see Section 9.2.2 - Trails). This trail segment links to similar sections in adjacent Planning Areas and also provides a spur allowing pedestrian access directly into the interior of Planning Area 10.

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 Water and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards including minimum lot area, minimum lot dimensions, maximum height, parking space requirements, and setback requirements, see Chapter 7.0 - Development Regulations.

**Architectural Styles.** See Section 8.3 - Residential Design Guidelines.

**Landscaping.** For residential streetscape landscaping see Chapter 9.0 - Landscaping.

### 2.3.11 Planning Area 11

**Location.** Planning Area 11 is located at the southern edge of the Specific Plan area, south of Colonies Parkway, between Planning Area 10 to the west and Planning Area 12 to the east (Figure 2.12).

**Use.** This almost 11-acre Planning Area is designated for detached single family residential uses on minimum 5,000-square-foot lots.

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 11 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 11 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

**Vehicular and Pedestrian Access.** Access for vehicles is provided by a neighborhood intersection, identified with a monument sign, on the north side of the Planning Area off Colonies Parkway (for neighborhood intersection design, see Section 9.3 - Streetscapes). Pedestrian access into Planning Area 11 is to be provided at multiple locations via trail spurs off the community trail and greenbelt located at the utility easement on the south side of the Planning Area (for trail design details, see Section 9.2.2 - Trails). This backbone trail links with similar trails along the southern edge of adjacent Planning Areas, providing continuous pedestrian access along the southern perimeter of the Specific Plan area.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 - Water and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards including minimum lot area, minimum lot dimensions, maximum height, parking space requirements, and setback requirements, see Chapter 7.0 - Development Regulations.

**Architectural Styles.** See Section 8.3 - Residential Design Guidelines.

**Landscaping.** For residential streetscape landscaping, see Chapter 9.0 - Landscaping.

### 2.3.12 Planning Area 12

**Location.** Planning Area 12, just over 11 acres in size, is located at the southern edge of the Specific Plan area south of Colonies Parkway, adjacent to Planning Areas 11 and 13 on the west and east, respectively (Figure 2.13).

**Use.** Planning Area 12 features detached single family residential uses on minimum 5,000-square-foot lots.

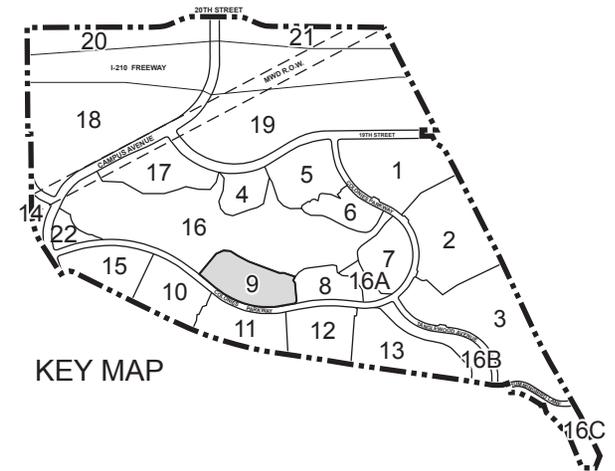
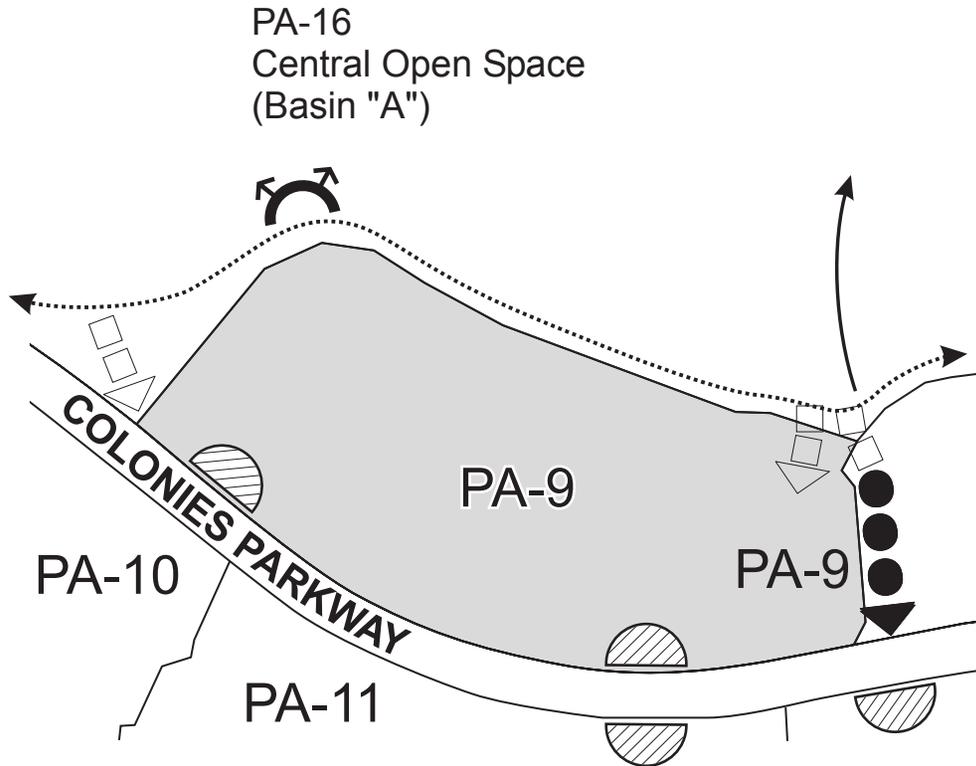
# PLANNING AREA 9

SFR 6.0 (Single Family Residential)  
11.07 Acres

## LEGEND

-  Community Trail and Greenbelt
-  Community Open Space Hiking Trail
-  Bio-Swale Drainage Channel
-  Service Drive
-  Overlook
-  Neighborhood Intersection  
*(locations of are conceptual only.)*

NOTE: See Chapter 9 for design concepts.



KEY MAP

Figure 2.10

*The Colonies at San Antonio Specific Plan*

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Source: RJM Design Group.

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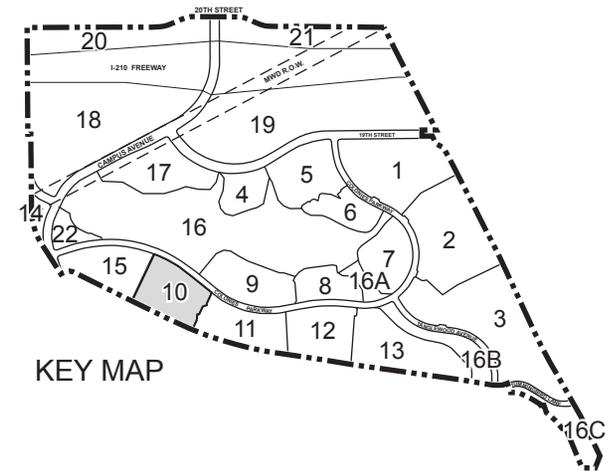
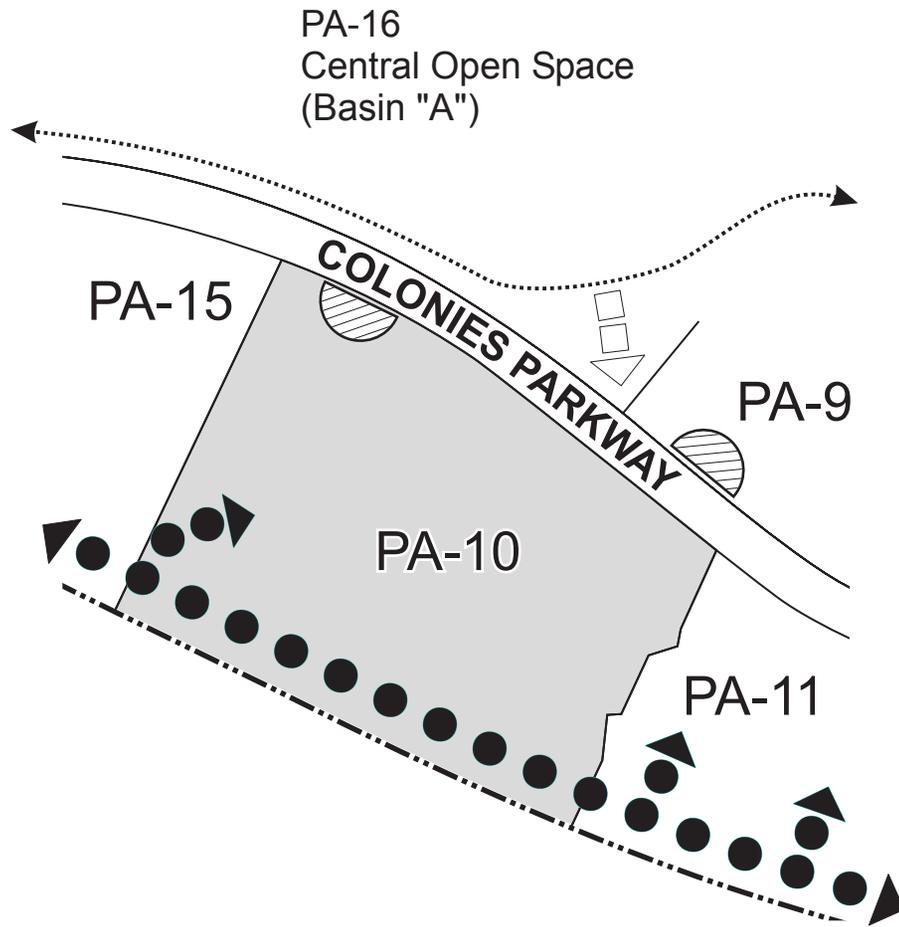
# PLANNING AREA 10

SFR 5.0 (Single Family Residential)  
10.72 Acres

## LEGEND

-  Project Boundary
-  Community Trail and Greenbelt (at utility easement)
-  Community Open Space Hiking Trail
-  Service Drive
-  Neighborhood Intersection (locations of are conceptual only)

NOTE: See Chapter 9 for design concepts.



KEY MAP

Figure 2.11

*The Colonies at San Antonio Specific Plan*

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Source: RJM Design Group.

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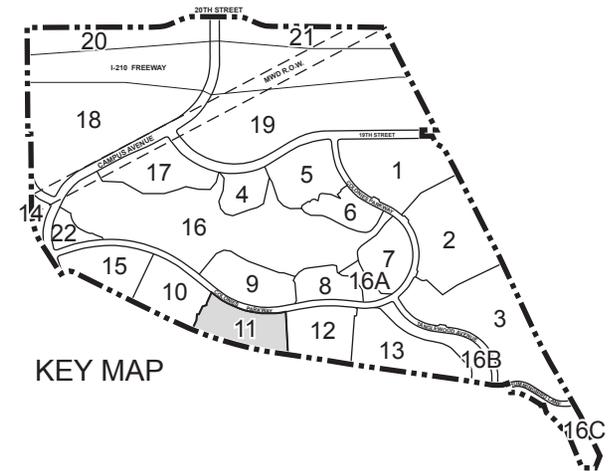
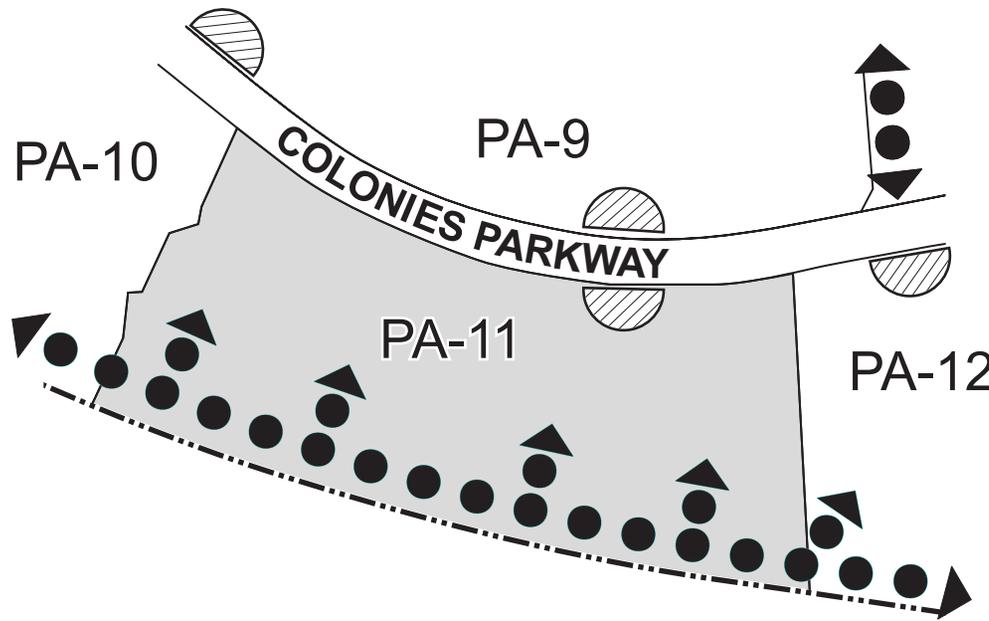
# PLANNING AREA 11

SFR 5.0 (Single Family Residential)  
10.78 Acres

## LEGEND

-  Project Boundary
-  Community Trail and Greenbelt (at utility easement)
-  Neighborhood Intersection (locations of are conceptual only)

NOTE: See Chapter 9 for design concepts.



KEY MAP

Figure 2.12

*The Colonies at San Antonio Specific Plan*

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Source: RJM Design Group.

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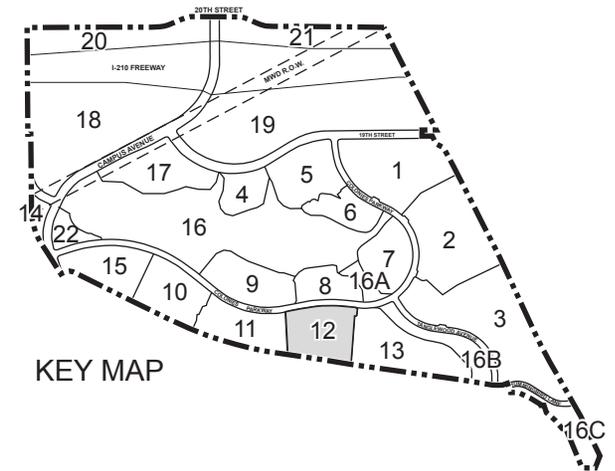
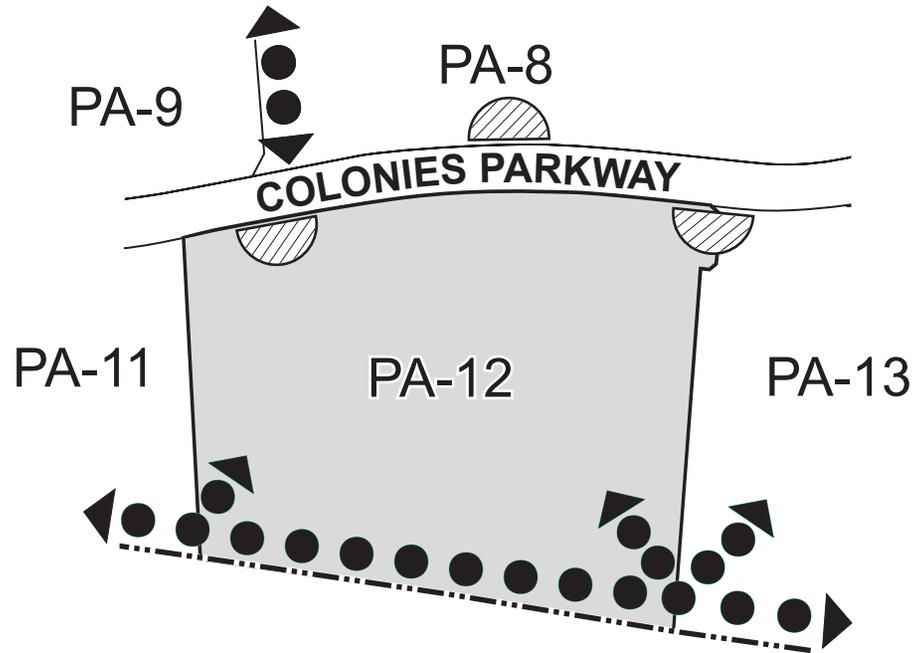
# PLANNING AREA 12

SFR 5.0 (Single Family Residential)  
11.01 Acres

## LEGEND

-  Project Boundary
-  Community Trail and Greenbelt (at utility easement)
-  Neighborhood Intersection (locations of are conceptual only)

NOTE: See Chapter 9 for design concepts.



KEY MAP

Figure 2.13

*The Colonies at San Antonio Specific Plan*

↑  
N Not to Scale

Source: RJM Design Group.

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**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 12 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 12 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

**Vehicular and Pedestrian Access.** Access to Planning Area 12 is via a neighborhood intersection, identified with a monument sign, off Colonies Parkway (for neighborhood intersection design, see Section 9.3 - Streetscapes). Another neighborhood intersection near the northeast corner of the Planning Area actually serves the adjacent Planning Area to the east. A community trail and greenbelt lies at the southern edge of Planning Area 12 along the utility easement, providing pedestrian access into the Planning Area interior and connecting with similar trail segments to the west and east (for trail design details, see Section 9.2.2 - Trails).

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 - Water and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards including minimum lot area, minimum lot dimensions, maximum height, parking space requirements, and setback requirements, see Chapter 7.0 - Development Regulations.

**Landscaping.** For residential streetscape landscaping, see Chapter 9.0 - Landscaping.

### 2.3.13 Planning Area 13

**Location.** Planning Area 13 (15.60 acres in size) is located on the southern boundary of the Specific Plan area south of Colonies Parkway, between Planning Area 12 to the west and the drainage path of Planning Area 16B to the east (Figure 2.14).

**Use.** The land use for Planning Area 13 will be detached single family residential units on lots at least 5,000 square feet in size.

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 13 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 13 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

**Vehicular and Pedestrian Access.** Vehicular access to Planning Area 13 occurs at two neighborhood intersections (identified with monument signs), one located in the northwest section of the Planning Area and the other on the northeast via a roadway bridge crossing from Tanglewood Avenue across the adjacent floodway path (for neighborhood intersection design, see Section 9.3 -Streetscapes). Another roadway bridge crossing occurs to the northeast of Planning Area 13 as Colonies Parkway crosses the drainage path underneath. The community trail and greenbelt along the southern perimeter of the Planning Area (which will be visible from the existing residential development to the south located outside the Specific Plan area) provides pedestrian access into the interior of the Planning Area at several locations and connects to similar trail segments in adjacent Planning Areas (for trail design details, see Section 9.2.2 - Trails). An overlook at the southeast corner of the Planning Area allows views of the natural drainage path and surrounding area (for overlook, natural drainage path, and open space details, see Section 9.2.1 - Open Space Concept).

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 - Water and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards including minimum lot area, minimum lot dimensions, maximum height, parking space requirements, and setback requirements, see Chapter 7.0 - Development Regulations.

**Architectural Styles.** See Section 8.3 - Residential Design Guidelines.

**Landscaping.** For residential streetscape landscaping, see Chapter 9.0 - Landscaping.

#### **2.3.14 Planning Area 14**

**Location.** Planning Area 14 lies to the west of the intersection of the western portion of 19<sup>th</sup> Street and Campus Avenue at the western edge of the Specific Plan area (Figure 2.15).

**Use.** This 1.72-acre Planning Area will be dedicated to the City of Upland, and a fire station, which would be operated and maintained by the City of Upland Fire Department, will be constructed.

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 14 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 14 will need to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 Water and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards that may be applicable to Planning Area 14 if the land use is not a public facility, see Chapter 7.0 - Development Regulations.

**Architectural Styles.** For commercial design guidelines that may be applicable to Planning Area 14 if the land use is not a public facility, see Section 8.2 - Commercial Design Guidelines.

**Landscaping.** An MWD right-of-way lies within the northern half of the Planning Area. Development within the right-of-way in Planning Area 14 will comply with the MWD’s guidelines. For landscaping provisions that may be applicable to Planning Area 14 if the land use is not a public facility, see Chapter 9.0 - Landscaping.

#### **2.3.15 Planning Area 15**

**Location.** Planning Area 15 is located in the southwest corner of the Specific Plan area, along the south side of Colonies Parkway, adjacent to Planning Area 10 (Figure 2.16).

**Use.** Although this 10.20-acre Planning Area was offered for purchase by the Upland Unified School District, the District has indicated that they will not purchase the site. Therefore, this Planning Area will be developed with flexible residential uses at a density up to 16 dwelling units per acre with a maximum of 160 dwelling units (RF) . The residential units could be developed as either attached or detached units, depending on market demand at the time of construction. A portion of these dwelling units will be transferred from Planning Area 4, as well as from unused dwelling units in other planning areas so that the maximum buildout of the specific plan area will not exceed 1,150 dwelling units.

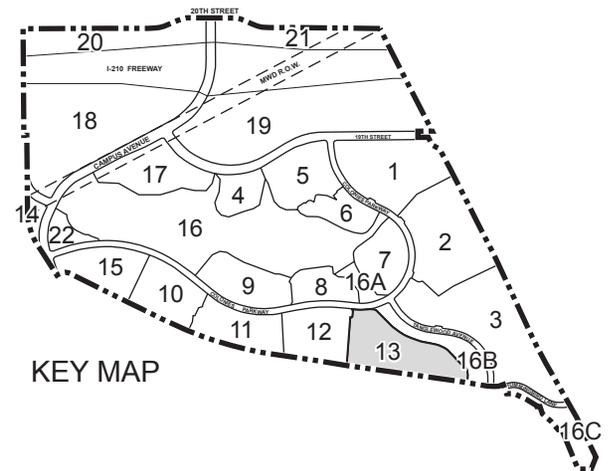
# PLANNING AREA 13

SFR 5.0 (Single Family Residential)  
15.60 Acres

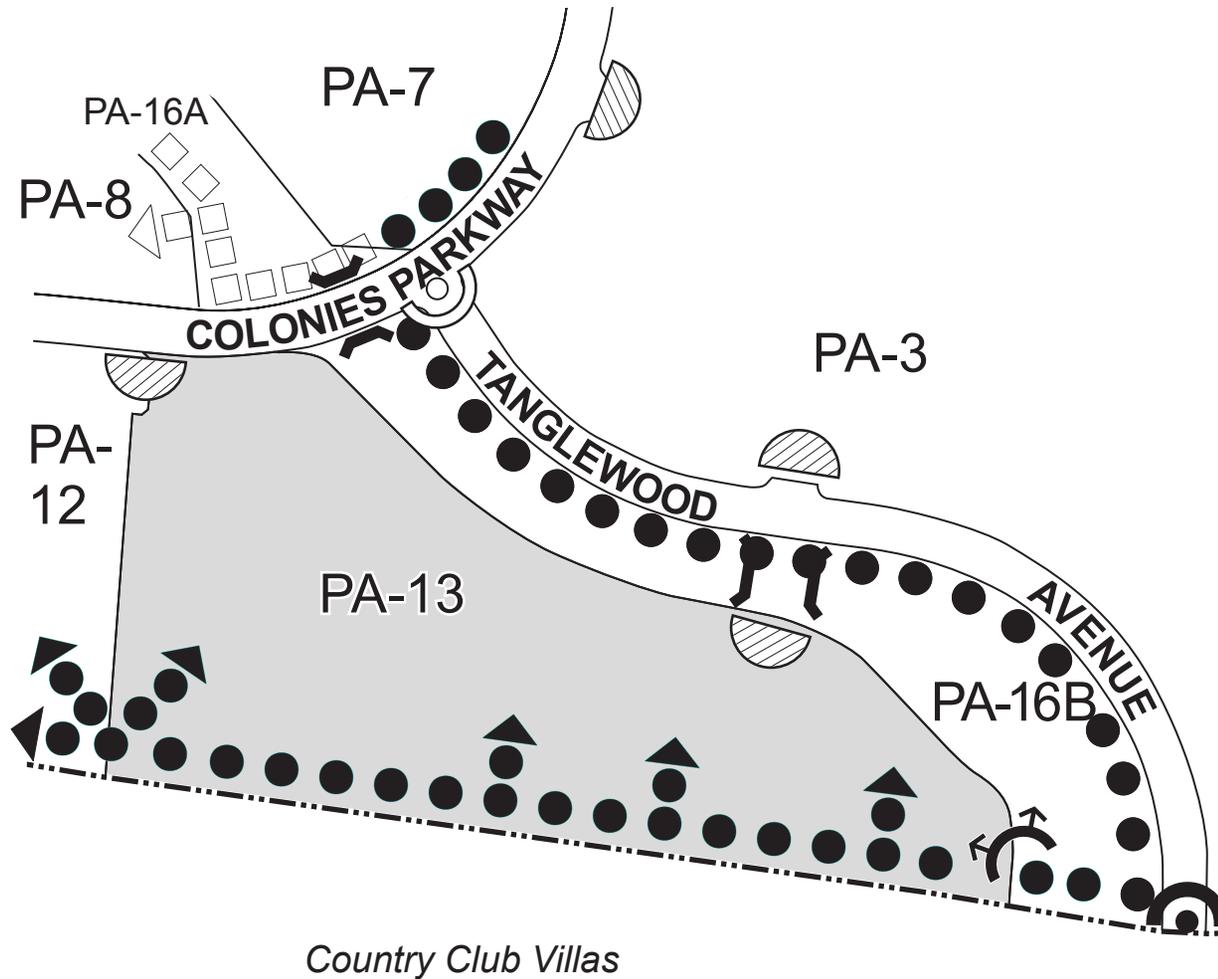
## LEGEND

-  Project Boundary
-  Community Trail and Greenbelt
-  Community Open Space Hiking Trail
-  Overlook
-  Primary Intersection
-  Secondary Intersection
-  Neighborhood Intersection  
*(locations of are conceptual only)*
-  Bridge

NOTE: See Chapter 9 for design concepts.



KEY MAP



Country Club Villas

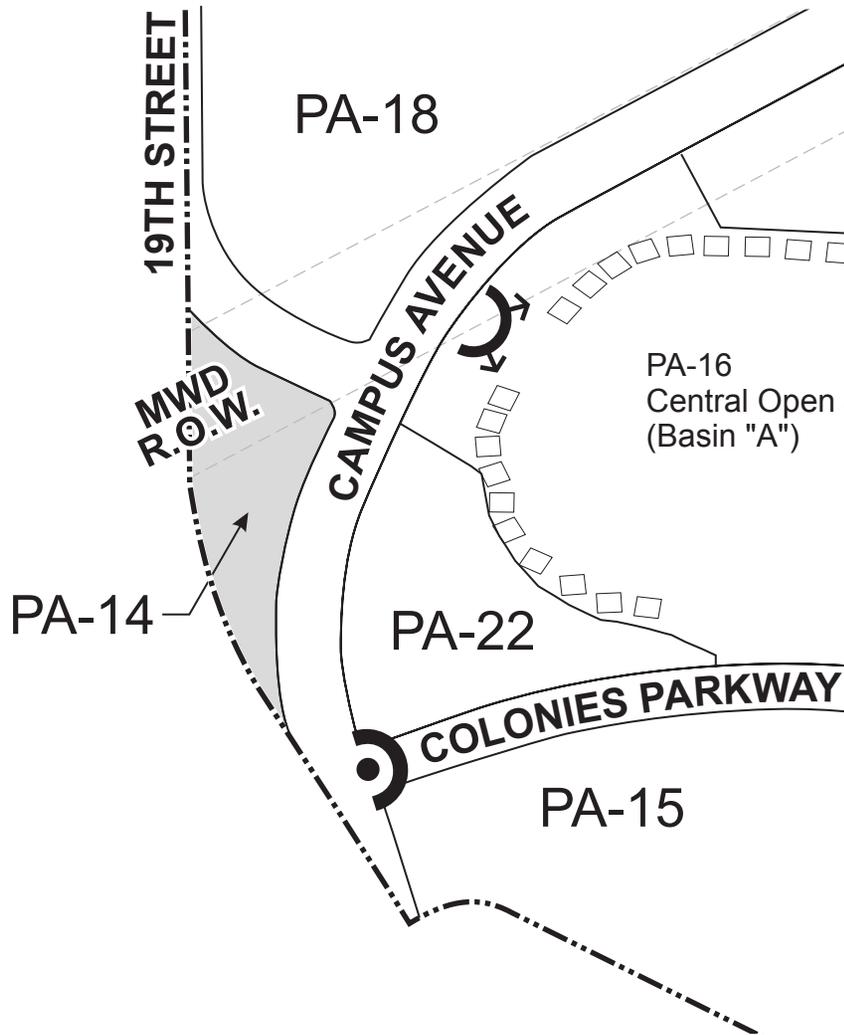
*The Colonies at San Antonio Specific Plan*

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N Not to Scale

Source: RJM Design Group.

Figure 2.14

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# PLANNING AREA 14

CF (Community Facility - Fire Station)  
1.72 Acres

## LEGEND

-  Project Boundary
-  Community Open Space Hiking Trail
-  Overlook
-  Primary Intersection

NOTE: See Chapter 9 for design concepts.

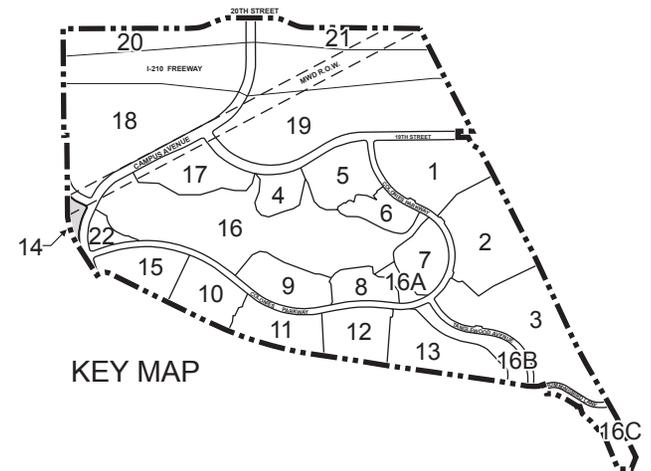


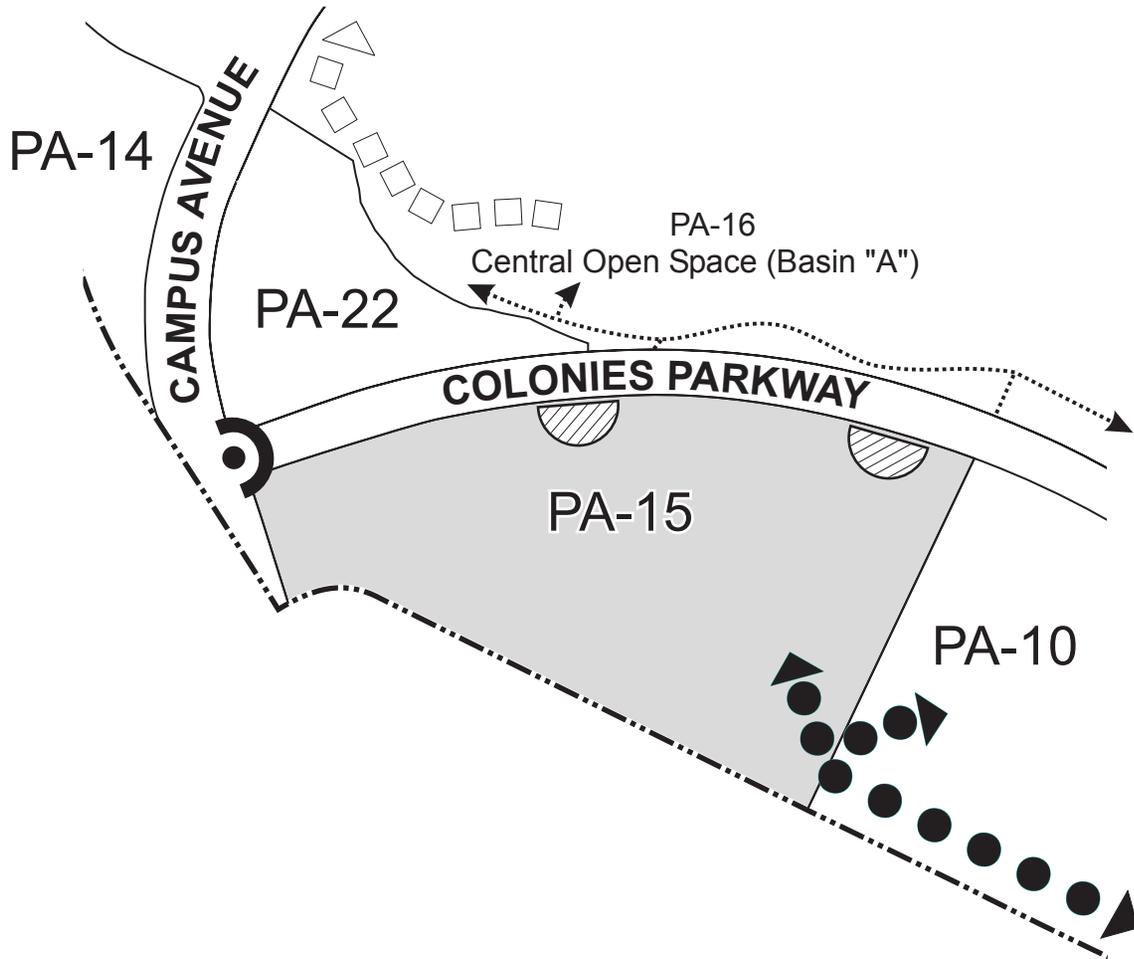
Figure 2.15

*The Colonies at San Antonio Specific Plan*

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Source: RJM Design Group.

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# PLANNING AREA 15

*RF (Flexible Residential)*

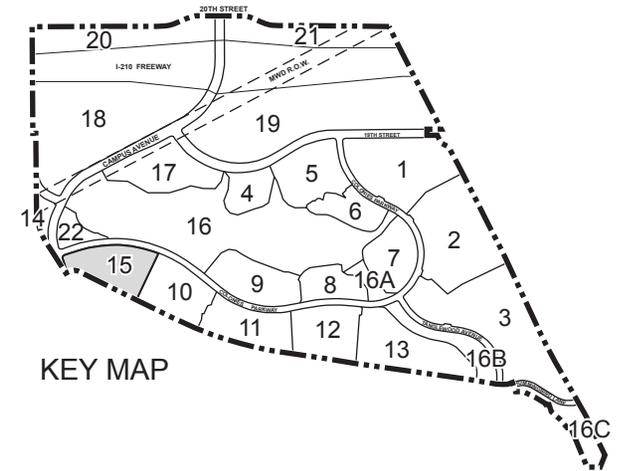
Up to 160 Units

10.20 Acres

## LEGEND

-  Project Boundary
-  Community Trail and Greenbelt
-  Community Open Space Hiking Trail
-  Service Drive
-  Primary Intersection
-  Neighborhood Intersection  
*(locations of are conceptual only)*

NOTE: See Chapter 9 for design concepts.



KEY MAP

*The Colonies at San Antonio Specific Plan*

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N Not to Scale

Source: RJM Design Group.

Figure 2.16

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**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 15 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 15 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

**Vehicular and Pedestrian Access.** If the Planning Area is developed with residential uses, two neighborhood intersections (identified with monument signs) will be located on the north side of the Planning Area off Colonies Parkway (for neighborhood intersection design, see Section 9.3 - Streetscapes). A community trail and greenbelt spur will terminate into Planning Area 15 (for trail design details, see Section 9.2.2 - Trails).

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 - Water and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards for both the attached and detached residential unit options that may be applicable to Planning Area, see Chapter 7.0 - Development Regulations.

**Architectural Styles.** For design guidelines that may be applicable to Planning Area 15, see Section 8.3 - Residential Design Guidelines).

**Landscaping.** Primary intersection monumentation will be located at Campus Avenue and Colonies Parkway to the northwest of the Planning Area (for primary intersection design, see Section 9.3 - Streetscapes). For landscaping provisions that may be applicable to Planning Area 15, see Chapter 9.0 - Landscaping.

### 2.3.16 Planning Area 16

**Location.** The central open space area located near the center of the Specific Plan area encompasses Planning Area 16 (Figure 2.17).

**Use.** This 55.64 acre open space area provides a focal point for the Specific Plan area, local and regional flood control facilities, and water conservation with a natural appearance, a wetland/riparian area, and passive recreation with trails.

The central open space area of Planning Area 16 is designed to accommodate stormwater flows that the site accepts from local, regional, and I-210 tributary areas (for open space design details, see Section 9.2.1 - Open Space Concept and Chapter 4.0 - Drainage). The storage of these flows will ensure that the capacity of the adjacent Cucamonga Creek Channel will not be exceeded by the drainage from on-site *or* upstream development. By designing the open space basin as three sub-basins, flows to the open space area will also allow wetland and riparian habitats to be sustained by the stormwater runoff. A component of the sub-basins, the braided, sand-bottom channel will convey low-volume flows, allowing percolation to groundwater and serving to remove contaminants from first-flush storm flows and nuisance flows. A 4.11-acre sub-basin will be maintain as an open wash area to facilitate the Planning Area’s groundwater recharge ability A raised overflow area at the downstream end of the basin will detain flows, and a gated valve will be used to actively manage outflow from the basin, thereby increasing Planning Area 16’s groundwater recharge. Greater storm flows will flow into Planning Area 16A. Initial construction of the central open space area will include grading to create the basins and channels, as well as construction of a passive spillway. The basin will be hydroseeded with a mix of native wetland, riparian, and upland species.

Ultimate construction will include raising the spillway and constructing a gated valve outlet, landscaping

per the provisions of the Specific Plan and the California Department of Fish and Game 1603 Stream Alteration Agreement for the Colonies at San Antonio, and construction of trails and other amenities within the central basin area

The passive recreational area within Planning Area 16 is designed for picnicking, jogging, strolling, and viewing wildlife habitat. A system of community trails provides convenient access between neighborhoods, commercial centers, and the open space area, with connections to the regional trail system along the Cucamonga Creek Channel.

**Infrastructure Phasing.** The infrastructure required in Planning Area 16 is delineated in Table 10.A (see Chapter 10.0). Each required backbone infrastructure component is identified by a “✓” in the Table for Planning Area 16.

**Vehicular and Pedestrian Access.** Vehicular access within Planning Area 16 is limited to maintenance vehicles. Service drives lead from public streets to the 20<sup>th</sup> Street and 19<sup>th</sup> Street outlet structures and all recharge basins to allow for necessary maintenance. A pedestrian system within the Planning Area is provided via community open space hiking trails located at the perimeter, with some offshoot trails linking to 19<sup>th</sup> Street, Colonies Parkway, or Campus Avenue (for trail design details, see Section 9.2.2 - Trails). Overlook features are located along the community open space hiking trails at several locations throughout the perimeter of Planning Area 16 to provide views of the open space amenity (for overlook design, see Section 9.2.1 - Open Space Concept).

**Drainage and Grading.** Within Planning Area 16, flood control facilities are designed so that water will flow east from the 19<sup>th</sup> Street and 20<sup>th</sup> Street outlet structures and be supplemented by water from the bio-swale drainage channels bringing runoff from adjacent development (for 19<sup>th</sup> Street and 20<sup>th</sup> Street outlet structure design and bio-swale drainage channel description, see Section 9.2.1 Open Space Concept). Water will settle into Basin A of the central open space area, or, if of a sufficient volume, will continue east through Basin A1, A2 and A3 to the outlet facility in the southeast portion of the Planning Area. (For additional drainage information, see Chapter 4.0 Drainage) and for grading information, see Chapter 6.0 - Grading).

**Landscaping.** A primary intersection featuring monumentation is located adjacent to the Planning Area at the intersection of Campus Avenue and Colonies Parkway (for primary intersection design, see Section 9.3 - Streetscapes). The open space area will feature trail entry monuments, trail markers, interpretive signage, and site furniture (for design and conceptual locations, see Section 9.2.1 - Open Space Concept). Planning Area 16 will be divided into three different plant zones: a) wetlands; b) riparian; and c) upland Southern California natives. The upper reaches of the drainage channel located in the bottom of the basin will have moisture present throughout the year, generated from nuisance water from adjacent developments and from the San Antonio Water Company (SAWCO) water flows, for groundwater recharge. Sufficient moisture will be available to sustain wetland and riparian vegetation within these channels. In the areas above the riparian channel, which are subject to the seasonal storm events, a grassland area will provide a transition to the upland native plant community. The upland plant community will consist primarily of Southern California native plant material. A partial list of recommended plants is provided in Section 9.2.1 - Open Space Concept.

### 2.3.17 Planning Area 16A

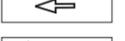
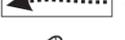
**Location.** Planning Area 16A is located southeast of the open space area (Planning Area 16) between Planning Areas 7 and 8 (Figure 2.18).

**Use.** This 2.24-acre Planning Area is designed to appear as a natural drainage path for conveying water.

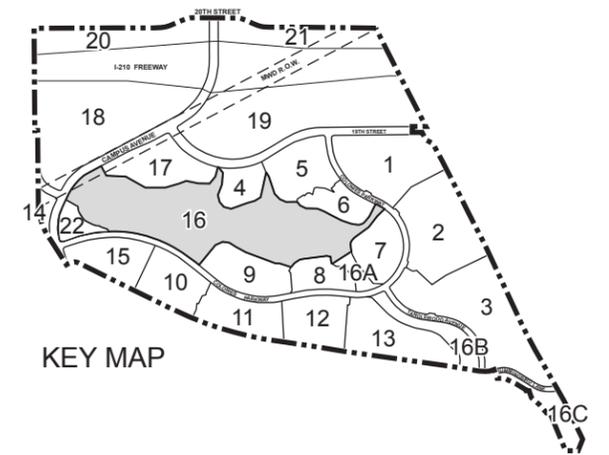
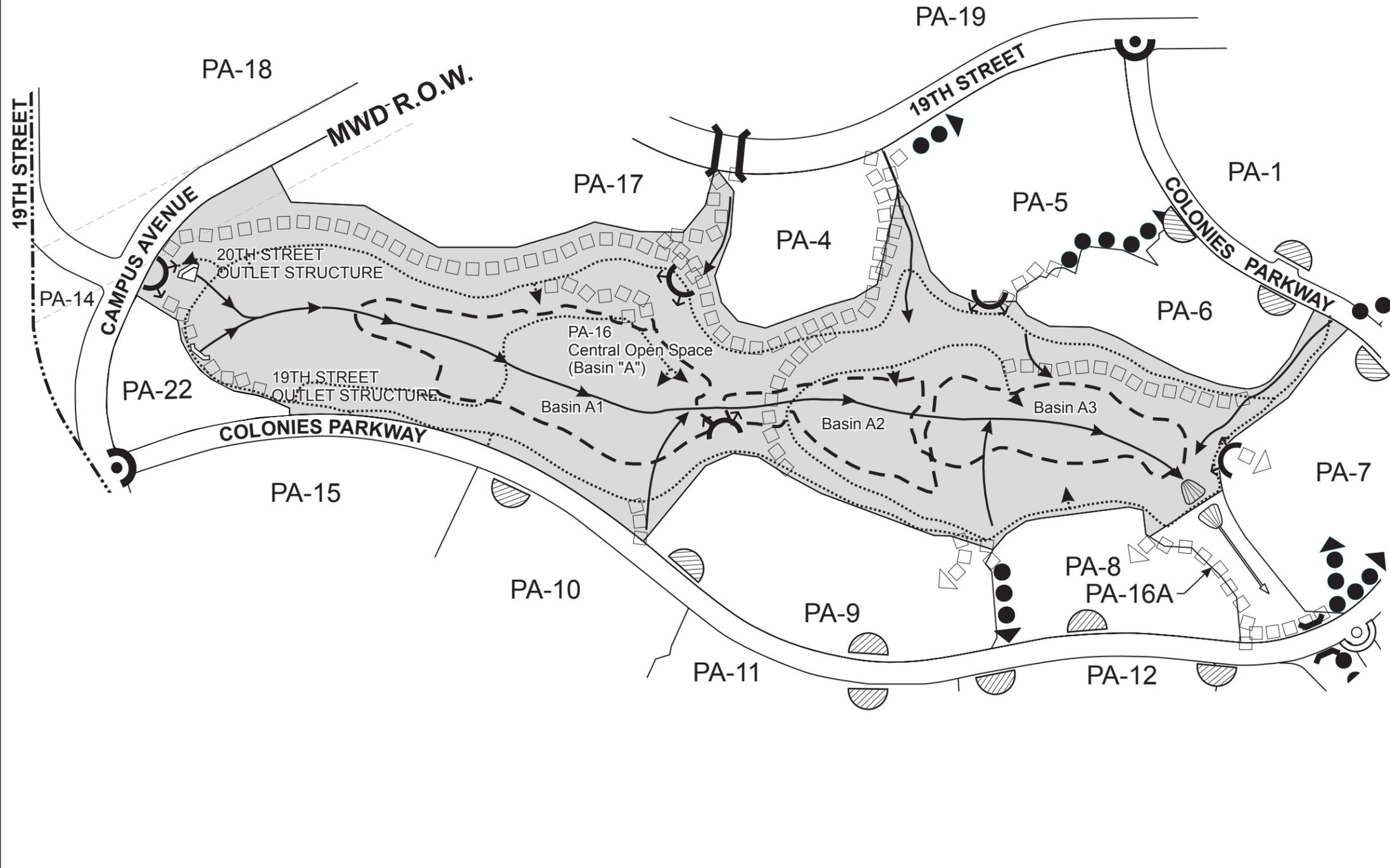
# PLANNING AREA 16

OS (Central Open Space - Basin "A")  
55.37 Acres

## LEGEND

-  Project Boundary
-  Community Trail and Greenbelt
-  Community Open Space Hiking Trail
-  Direction of Drainage Flow
-  Bio-Swale Drainage Channel
-  Floodway
-  Service Drive/Trail
-  Basin Outlet
-  Overlook
-  Primary Intersection
-  Secondary Intersection
-  Pedestrian Bridge

NOTE: See Chapter 9 for design concepts.



*The Colonies at San Antonio Specific Plan*

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Source: RJM Design Group.

Figure 2.17

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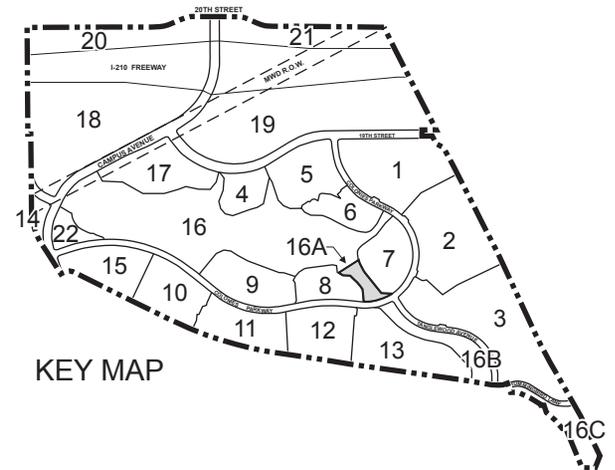
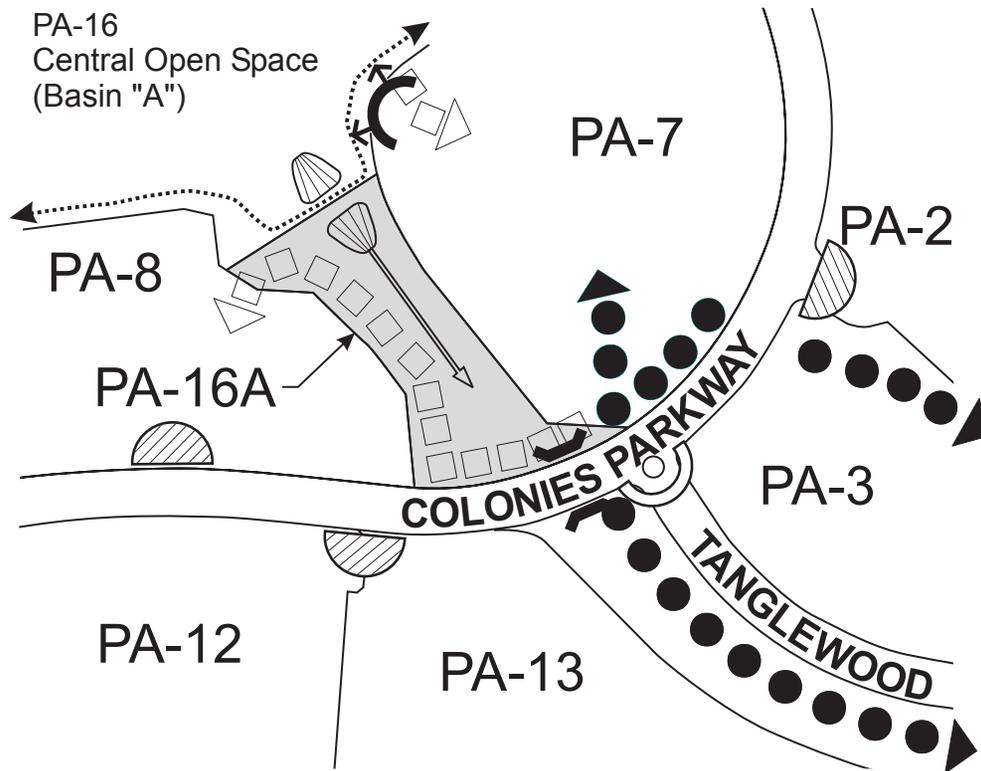
# PLANNING AREA 16A

OS (Open Space - Natural Drainage Path)  
2.24 Acres

## LEGEND

-  Project Boundary
-  Community Trail and Greenbelt
-  Community Open Space Hiking Trail
-  Floodway
-  Service Drive
-  Basin Outlet
-  Overlook
-  Secondary Intersection
-  Neighborhood Intersection (locations of are conceptual only)
-  Bridge

NOTE: See Chapter 9 for design concepts.



KEY MAP

Figure 2.18

*The Colonies at San Antonio Specific Plan*

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N Not to Scale

Source: RJM Design Group.

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**Infrastructure Phasing.** The infrastructure required in Planning Area 16A is delineated in Table 10.A (under the column designated Planning Area 16) (see Chapter 10.0). Each required backbone infrastructure component is identified by a “✓” in the Table.

**Pedestrian Access.** Planning Area 16A features a community open space hiking trail along the western edge of the Planning Area, which diverts into a community trail and greenbelt to provide pedestrian access to the adjacent Planning Area (for trail design details, see Section 9.2.2 - Trails).

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage.** The main feature of Planning Area 16A is the rock basin outlet that originates in the adjacent Planning Area 16. The floodway directs water south under Colonies Parkway. (For additional conceptual drainage information and landscape concepts, see Section 9.2.1 - Open Space Concept). For further technical drainage information, see Chapter 4.0 - Drainage.

**Grading.** See Chapter 6.0 - Grading.

### 2.3.18 Planning Area 16B

**Location.** Planning Area 16B is located in the southern portion of the Specific Plan area between Planning Areas 13 and 3 to the west and east, respectively (Figure 2.19).

**Use.** Planning Area 16B consists of 6.29 acres that serve as a floodway taking water south to the adjacent basin in Planning Area 16C.

**Infrastructure Phasing.** The infrastructure required in Planning Area 16B is delineated in Table 10.A (under the column designated Planning Area 16) (see Chapter 10.0). Each required backbone infrastructure component is identified by a “✓” in the table.

**Pedestrian Access.** A community trail and greenbelt follows Tanglewood Avenue beginning from Colonies Parkway at the roadway bridge crossing, continuing to the Specific Plan boundary at the south edge of the Planning Area, and turning west along the utility easement (for trail design details, see Section 9.2.2 - Trails). The trail features an overlook amenity located near the southern edge of the Planning Area to highlight views of the natural appearance of the drainage pathway (for overlook design, see Section 9.2.1 - Open Space Concept).

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage.** For conceptual drainage information and landscape concepts, see Section 9.2.1 - Open Space Concept. For technical drainage information, see Chapter 4.0 - Drainage.

**Grading.** See Chapter 6.0 - Grading.

**Landscaping.** A primary intersection with monumentation is designated along Tanglewood Avenue at Hummingbird Lane at the southern boundary of the Specific Plan area. A secondary intersection with monumentation is located at the junction of Colonies Parkway and Tanglewood Avenue. A neighborhood intersection, identified with a monument sign, lies just to the south of the Planning Area, allowing vehicular access into Planning Area 13 to the west via a road over the drainage path in Planning Area 16B. (For primary intersection, secondary intersection, and neighborhood intersection design, see Section 9.3 - Streetscapes). Another roadway bridge crossing at the north portion of the Planning Area allows Colonies Parkway to continue over the drainage path below (for design concept, see Section 9.2.1 - Open Space Concept).

### 2.3.19 Planning Area 16C

**Location.** Planning Area 16C (5.55 acres) is the southernmost Planning Area in the Colonies at San Antonio Specific Plan area (Figure 2.20). It does not have common boundaries with other Planning Areas, and is separated from Planning Area 3 to the north by Hummingbird Lane.

**Use.** This Planning Area encompasses “Detention Basin B,” where flood waters await their metered flow into the Cucamonga Creek Channel located to the east just outside the Specific Plan boundary.

**Infrastructure Phasing.** The infrastructure required in Planning Area 16A is delineated in Table 10.A (under the column designated Planning Area 16) (see Chapter 10.0). Each required backbone infrastructure component is identified by a “✓” in the Table.

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage.** For conceptual drainage information and landscape concepts, see Section 9.2.1 - Open Space Concept. For technical drainage information, see Chapter 4.0 - Drainage.

**Grading.** See Chapter 6.0 - Grading.

### 2.3.20 Planning Area 17

**Location.** Planning Area 17 is located north of the central open space area and south of the intersection of Campus Avenue and 19th Street (Figure 2.21).

**Use.** This Planning Area is designated Neighborhood Commercial. The 12.40-acre Planning Area provides for neighborhood-scale retail and commercial service uses such as a supermarket, drug store, bank, in-line and pad retail shops, gas station, car wash, and fast food and drive-through restaurants (for permitted uses, see Table 7.B in Chapter 7.0). Planning Area 17 is easily accessible from the nearby residential uses to the south and east within the Specific Plan area. This neighborhood center could also serve visitors drawn from surrounding neighborhoods because of its proximity to the freeway and 19<sup>th</sup> Street. Because it generally serves local neighborhoods, rather than the region, this planning area does not require freeway frontage as do the Highway Commercial designated areas. Planning Area 17 will be designed as a unified center with common access and parking for its uses.

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 17 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 17 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

**Vehicular and Pedestrian Access.** A commercial intersection provides vehicular access off Campus Avenue at the western edge of the Planning Area. An additional commercial intersection provides vehicular access into Planning Area 17 from 19<sup>th</sup> Street. A right in/right out vehicular access point also occurs on the northeast side of the Planning Area off 19<sup>th</sup> Street. A pedestrian bridge will be located at the eastern edge of the Planning Area across 19th Street (for design details, see Section 8.2.11 - Pedestrian Bridge). A community open space hiking trail follows the east side of the Planning Area (for trail design details, see Section 9.2.2 - Trails).

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 Water and Sewer, and Chapter 6.0 - Grading.

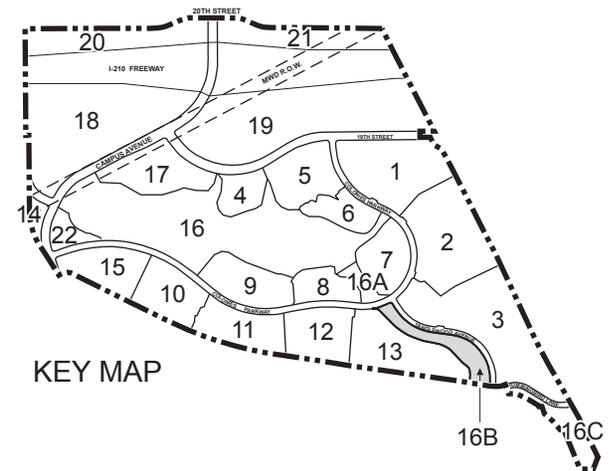
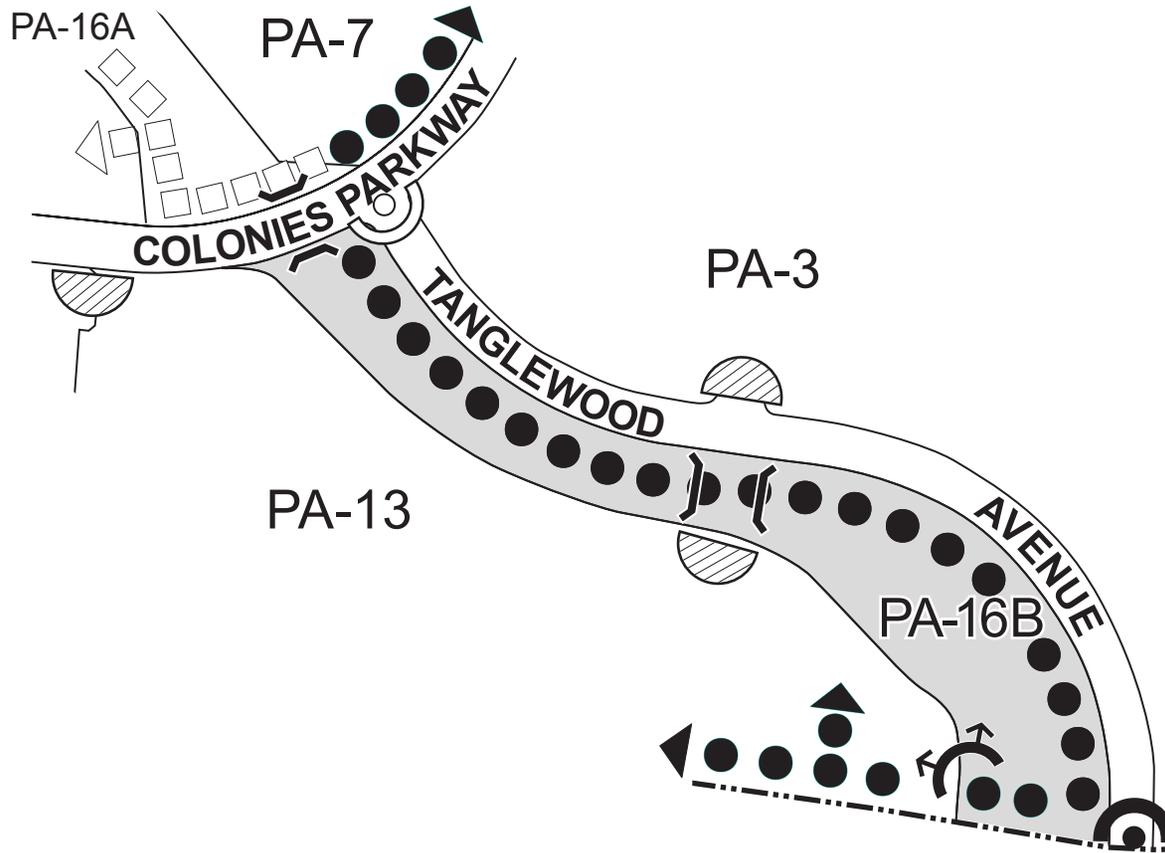
# PLANNING AREA 16B

OS (Open Space - Natural Drainage Path)  
6.29 Acres

## LEGEND

-  Project Boundary
-  Community Trail and Greenbelt
-  Community Open Space Hiking Trail
-  Overlook
-  Primary Intersection
-  Secondary Intersection
-  Neighborhood Intersection  
*(locations of are conceptual only)*
-  Bridge

NOTE: See Chapter 9 for design concepts.



KEY MAP

Figure 2.19

*The Colonies at San Antonio Specific Plan*

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N Not to Scale

Source: RJM Design Group.

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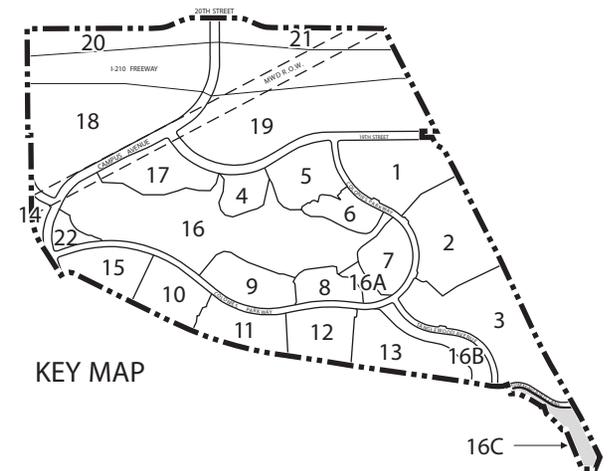
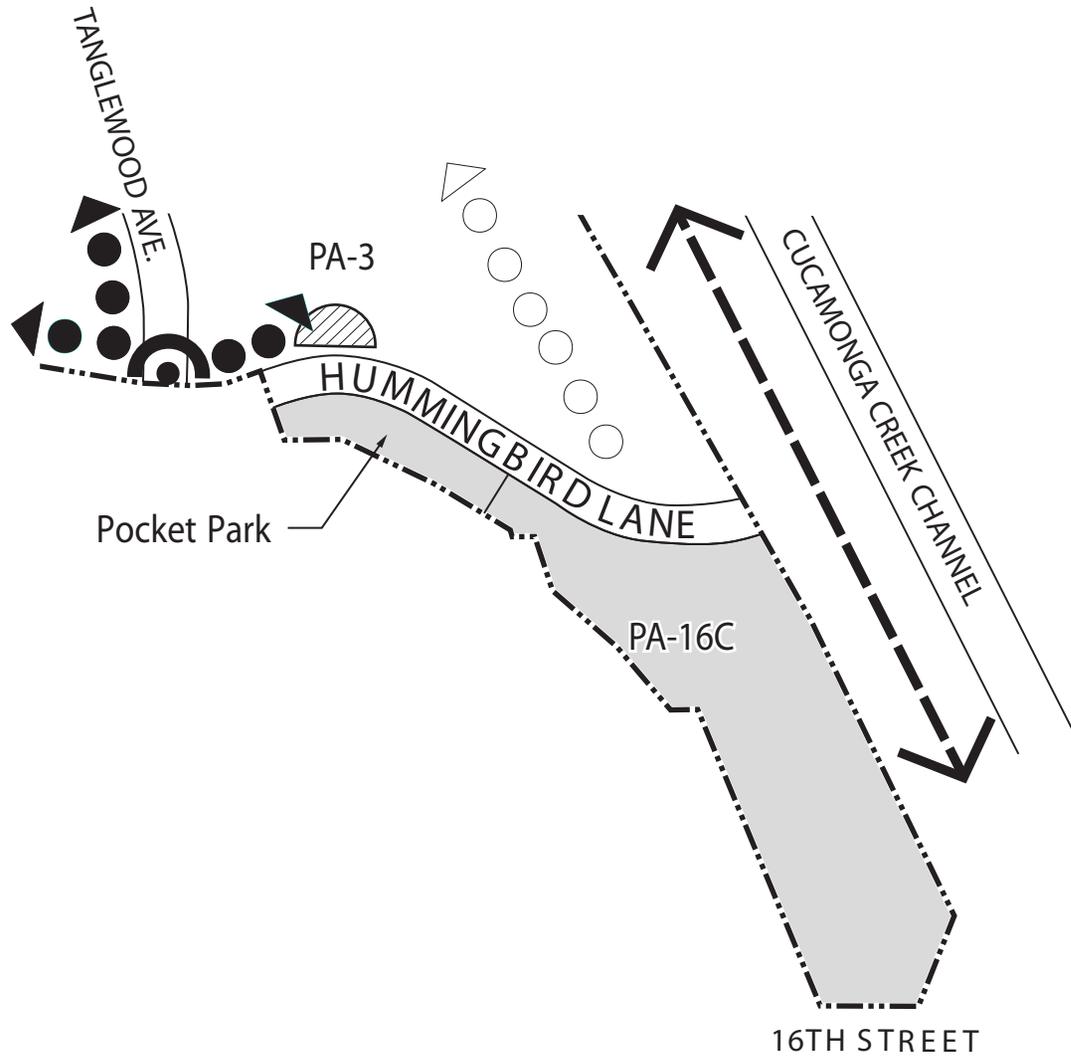
# PLANNING AREA 16C

OS (Open Space - Basin "B")  
5.55 Acres

## LEGEND

-  Project Boundary
-  Regional Hiking and Bike Trail
-  Community Trail and Greenbelt
-  Enhanced Parkway
-  Community Open Space Hiking Trail
-  Primary Intersection
-  Neighborhood Intersection (locations of are conceptual only)

NOTE: See Chapter 9 for design concepts.



KEY MAP

Figure 2.20

## The Colonies at San Antonio Specific Plan

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N Not to Scale

Source: RJM Design Group.

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# PLANNING AREA 17

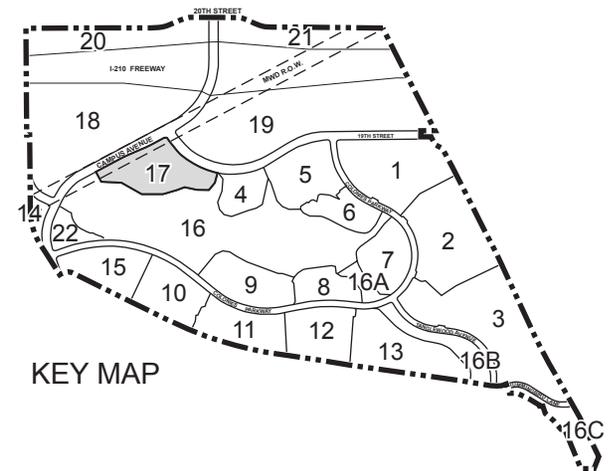
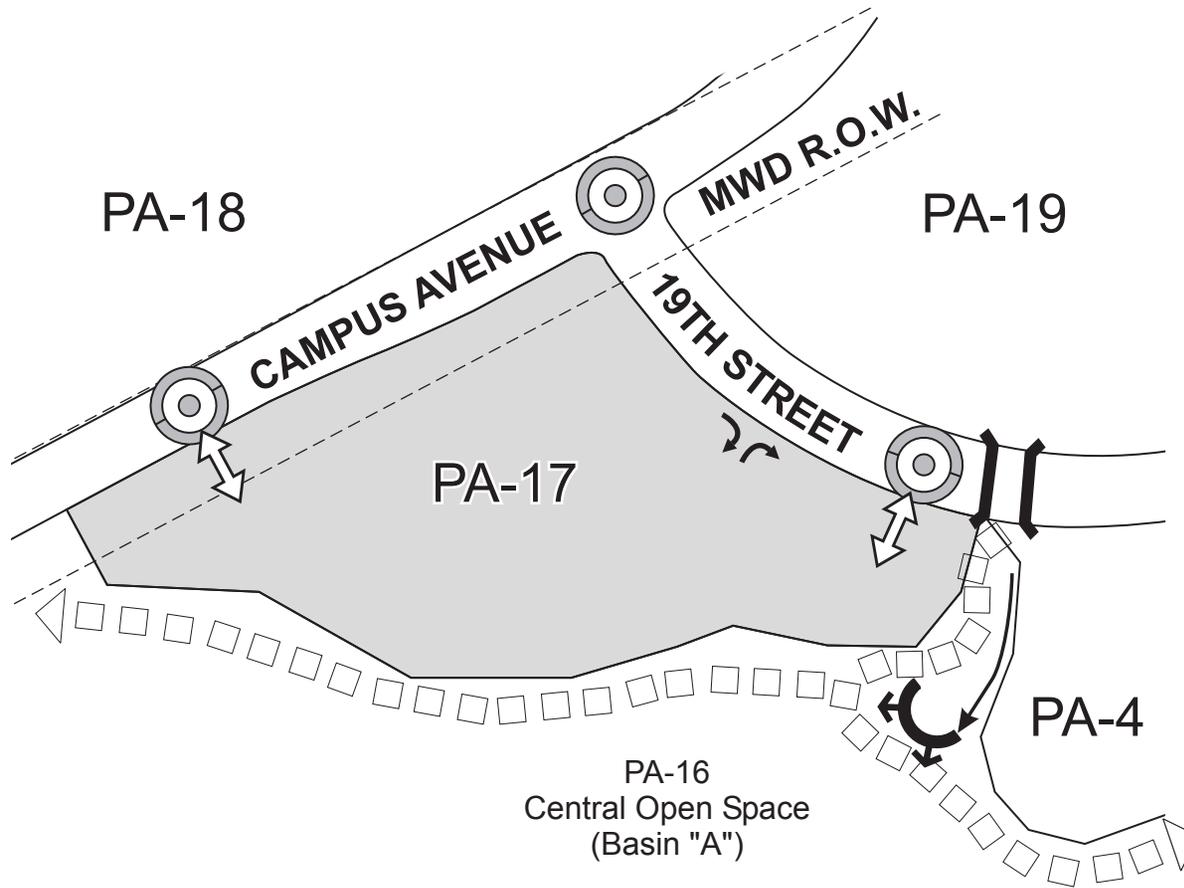
NC (Neighborhood Commercial)

12.40 Acres

## LEGEND

-  Community Open Space Hiking Trail
-  Bio-Swale Drainage Channel
-  Right In / Right Out Access
-  Overlook
-  Commercial Intersection
-  Vehicular Access (full turning movement)
-  Enhanced Pedestrian Crossing

NOTE: See Chapter 9 for design concepts.



KEY MAP

*The Colonies at San Antonio Specific Plan*

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N Not to Scale

Source: RJM Design Group.

Figure 2.21

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**Development Regulations.** For development standards (including maximum building height, parking space requirements, signage, and setback requirements), see Chapter 7.0 - Development Regulations.

**Architectural Styles.** For design theme and architectural criteria, see Section 8.2 - Commercial Design Guidelines.

**Landscaping.** Project-identification monumentation is located at the commercial intersection to the north of the Planning Area (for project commercial intersection design, see Section 9.3.3 Commercial Intersection and Monumentation). An MWD right-of-way lies along the northwest edge

### 2.3.21 Planning Area 18

**Location.** Located just south of I-210, Planning Area 18 is approximately 37 acres and lies on the west side of the Specific Plan area (Figure 2.22).

**Use.** Planning Area 18 is designated Highway Commercial and features uses that draw from the project site, City, and the region by providing for major retail uses, smaller retailers, and in-line shops and restaurants. Uses could include department store, discount retailer with a garden center, general merchandiser, in-line specialized retail stores and restaurants, food court, fast food restaurants, pad retail stores, and gas station and car wash facilities (for permitted uses, see Table 7.B in Chapter 7.0). Planning Area 18 will be designed as a unified center with common access and parking for its uses.

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 18 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 18 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

**Vehicular Access.** Primary vehicular access is from two commercial intersections, one off Campus Avenue located mid-way between the west and east segments of 19th Street . The other commercial intersection features project-identification monumentation and is located at the junction of Campus Avenue and 19th Street (for design of commercial intersection with project-identification, see Section 9.3.3 - Commercial Intersection and Monumentation). One right in/right out vehicular access point occurs off Campus Avenue and one right in/right out/left out off 19th Street. A right in only access point also occurs on Campus Avenue.

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 Water and Sewer, and Chapter 6.0 - Grading. A 30-foot wide easement at the southern edge of the Planning Area allows for placement of utility lines.

**Development Regulations.** For development standards (including maximum building height, parking space requirements, signage, and setback requirements), see Chapter 7.0 - Development Regulations.

**Architectural Styles.** For design theme and architectural criteria, see Section 8.2 - Commercial Design Guidelines.

**Landscaping.** The freeway edge along the northern boundary of Planning Area 18 at I-210 will feature a landscape area that provides attractive views into the commercial center, and project identification signage (for design at freeway edge, see Section 9.6 - Edge Treatments and for commercial project sign information, see Section 7.7 - Sign Standards). An MWD right-of-way crosses the southern corner of the Planning Area. Development within the right-of-way in Planning Area 18 will comply with the MWD’s guidelines. For commercial streetscape, commercial landscape palettes, and landscape development standards, see Chapter 9.0 - Landscaping.

### 2.3.22 Planning Area 19

**Location.** At approximately 42 acres, Planning Area 19 is the largest in the Specific Plan area, and is located south of I-210 and east of Campus Avenue (Figure 2.23).

**Use.** The west half of the Planning Area could provide a commercial destination retail and entertainment area. The overall design intent for this Planning Area is to provide an atmosphere where customers can meet their shopping needs, while providing the local and regional community with an attractive, convenient, and safe place to gather and socialize. The uses could include a theater along with restaurants and specialty retail shops, stand-alone retail stores adjacent to in-line restaurants and retail stores, pad retail stores, office uses, and a discount retailer (for permitted uses, see Table 7.B in Chapter 7.0). The Planning Area is designed to facilitate pedestrian access between uses and to create areas for informal gatherings. It will be developed as a unified center with common vehicular access and parking for its uses.

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 19 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a “✓” in the Table for Planning Area 19 is to be constructed and operational prior to the first occupancy of a structure within this Planning Area.

**Vehicular and Pedestrian Access.** A primary intersection with monumentation provides vehicular access to Planning Area 19 at the intersection of 19<sup>th</sup> Street and Colonies Parkway (for primary intersection design, see Section 9.3 - Streetscapes). Vehicular access into the Planning Area via a commercial intersection is located approximately mid-way between the intersection of Campus Avenue/19th Street and the primary intersection at 19th Street/Colonies Parkway. Two right in/right out vehicular access points occur off 19th Street and one occurs off Campus Avenue.

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 Water and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards (including maximum building height, parking space requirements, signage, and setback requirements), see Chapter 7.0 - Development Regulations.

**Architectural Styles.** For design theme and architectural criteria, see Section 8.2 - Commercial Design Guidelines.

**Landscaping.** A commercial intersection with project-identification monumentation is located at Campus Avenue and 19<sup>th</sup> Street within view of Planning Area 19 (for design of commercial intersection with project-identification, see Section 9.3.3 - Commercial Intersection and Monumentation). The freeway edge along the northern boundary of Planning Area 19 at I-210 will feature a landscape area that provides attractive views into the commercial center (for design at freeway edge, see Section 9.6 - Edge Treatments). Within Planning Area 19, a landscaped buffer located at the eastern edge of the Planning Area adjacent to the Cucamonga Creek Channel shall screen commercial development within the Planning Area (for buffer design, see Section 9.6 - Edge Treatments). Landscaped project entry monumentation is located at 19<sup>th</sup> Street at the eastern boundary of the Specific Plan area (for easterly project entry monumentation design, see Section 9.3 - Streetscapes). An MWD right-of-way lies along the northwest edge of the Planning Area. Development within the right-of-way in Planning Area 19 will comply with the MWD's guidelines. For commercial streetscape, commercial landscape palettes, and landscape development standards, see Chapter 9.0 - Landscaping.

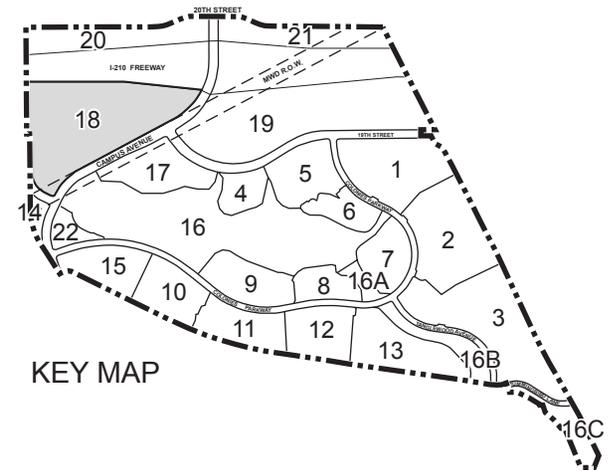
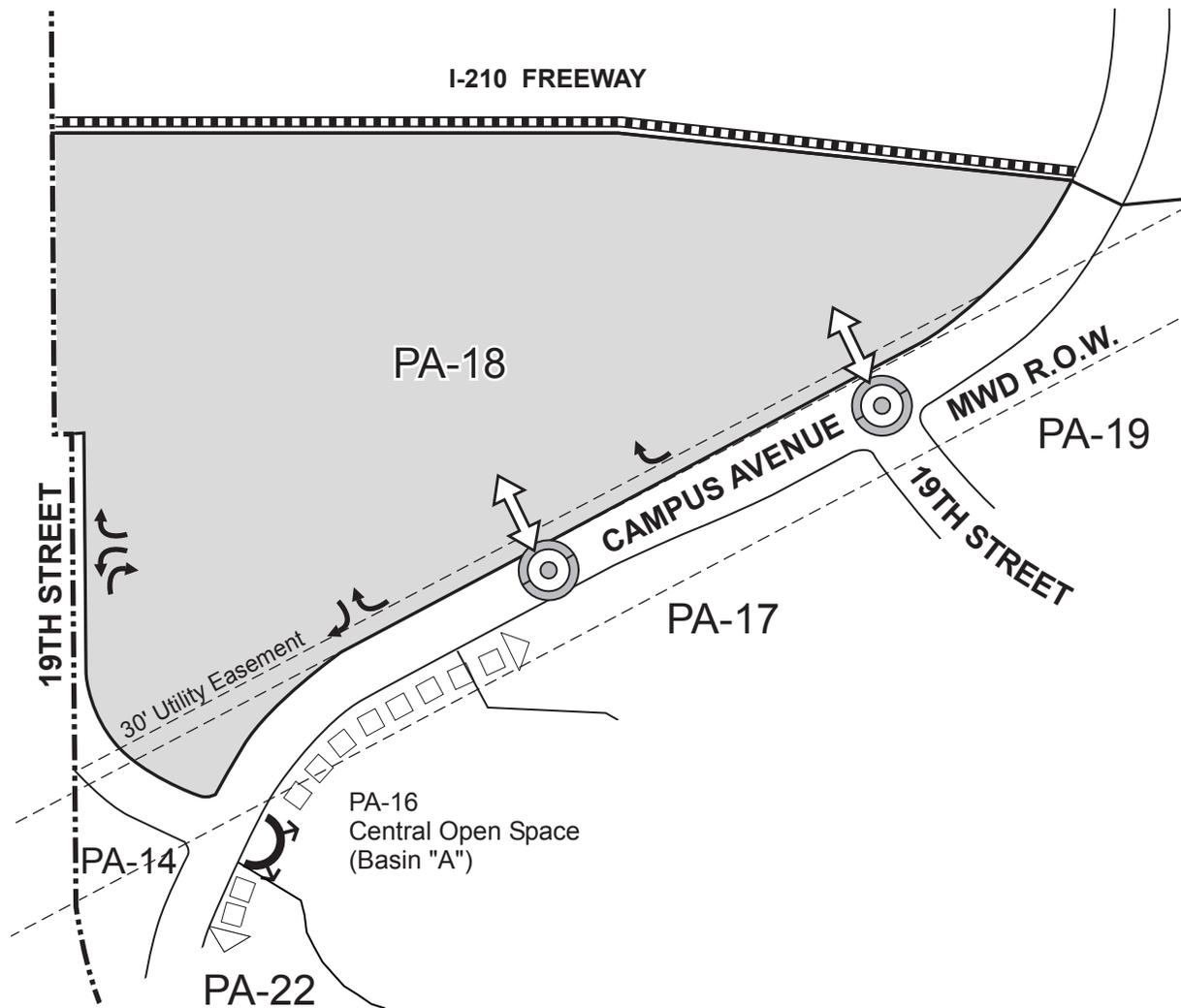
# PLANNING AREA 18

CH (Highway Commercial)  
36.53 Acres

## LEGEND

-  Project Boundary
-  Freeway Edge Condition
-  Community Open Space Hiking Trail
-  Right In / Right Out Access
-  Right In / Right Out / Left Out Access
-  Right In Access Only
-  Commercial Intersection
-  Vehicular Access (full turning movement)
-  Overlook

NOTE: See Chapter 9 for design concepts.



KEY MAP

*The Colonies at San Antonio Specific Plan*

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N Not to Scale

Source: RJM Design Group.

Figure 2.22

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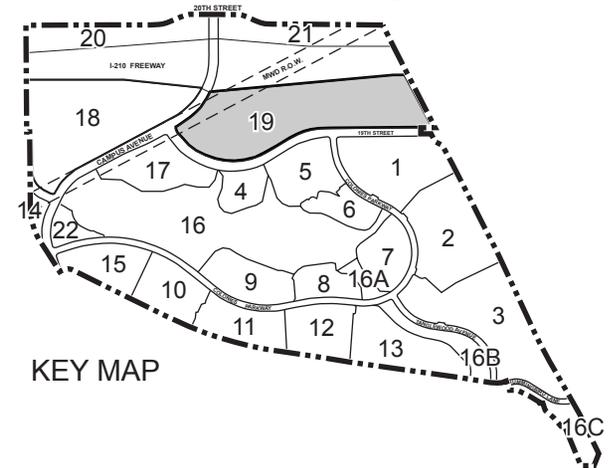
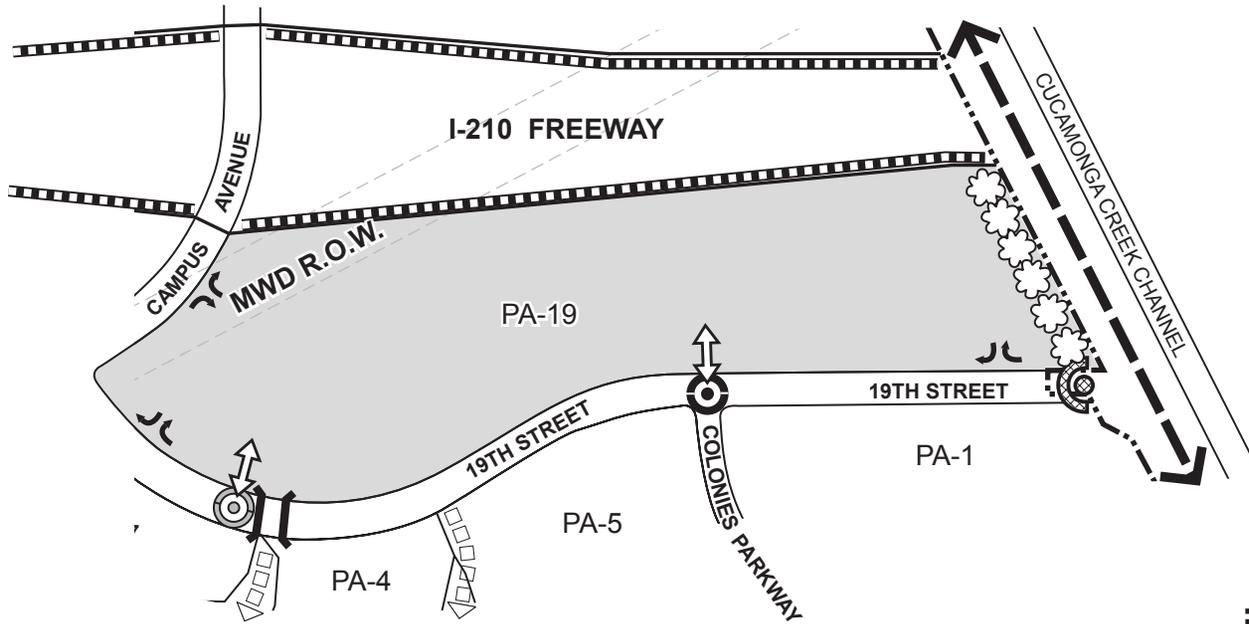
# PLANNING AREA 19

CH (Highway Commercial)  
42.31 Acres

## LEGEND

-  Project Boundary
-  Freeway Edge Condition
-  Regional Hiking and Bike Trail
-  Community Trail
-  Community Open Space Hiking Trail
-  Right In / Right Out Access
-  Major Intersection
-  Commercial Intersection
-  Vehicular Access (full turning movement)
-  Enhanced Pedestrian Crossing

NOTE: See Chapter 9 for design concepts.



KEY MAP

Figure 2.23

*The Colonies at San Antonio Specific Plan*

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N Not to Scale

Source: RJM Design Group.

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### 2.3.23 Planning Area 20

**Location.** Planning Area 20 (approximately 12 acres) is located in the northwest quadrant of the I-210/Campus Avenue interchange in the northern portion of the Specific Plan area (Figure 2.24).

**Use.** Development in this Planning Area could include automobile dealership, hotel, retail, restaurant, gas station, and other freeway-oriented commercial uses. The Planning Area's size and orientation, which is much wider than deep, increases the exposure of the permitted uses to freeway traffic (for permitted uses, see Table 7.B in Chapter 7.0).

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 20 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a "✓" in the Table for Planning Area 20 is to be constructed and operational prior to the first occupancy of a structure within the Planning Area.

**Vehicular Access.** Vehicular access into Planning Area 20 will be based on user requirements, and reasonable and generally accepted traffic engineering practices. For a typical independent tenant of approximately 1 acre, a minimum of two vehicular access points along the 20th Street frontage is expected, subject to traffic engineering review. Tenant development plans will be coordinated with existing conditions from previously approved adjacent parcels.

**Roadway Cross Sections.** See Chapter 3.0 (Transportation).

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 Water and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards (including maximum building height, parking space requirements, signage, and setback requirements), see Chapter 7.0 - Development Regulations.

**Architectural Styles.** For design theme and architectural criteria, see Section 8.2 - Commercial Design Guidelines.

**Landscaping.** A primary intersection featuring monumentation will be located to the east of Planning Area 20 at Campus Avenue north of the freeway (for primary intersection design, see Section 9.3 - Streetscapes). The southern boundary of Planning Area 20 adjacent to I-210 will feature a landscape area that provides attractive views into the Planning Area, and project identification signage (for design at freeway edge, see Section 9.6 - Edge Treatments and for commercial project sign information, see Section 7.7 - Sign Standards). For commercial streetscape, commercial landscape palettes, and landscape development standards, see Chapter 9.0 Landscaping.

### 2.3.24 Planning Area 21

**Location.** Planning Area 21 (approximately 11 acres) is located in the northeast quadrant of the I-210/Campus Avenue interchange in the northern portion of the Specific Plan area (Figure 2.25).

**Use.** Development in this Planning Area could include automobile dealership, hotel, retail, restaurant, gas station, and other freeway-oriented commercial uses. The Planning Area's size and orientation increase the exposure of the uses to freeway travelers (for permitted uses, see Table 7.B in Chapter 7.0).

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 21 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a "✓" in the Table for Planning Area 21 is to be constructed and operational prior to the first occupancy of a structure within the Planning Area.

**Vehicular Access.** Vehicular access into Planning Area 21 will be based on user requirements, and reasonable and generally accepted traffic engineering practices. For a typical independent tenant of approximately 1 acre, a minimum of two vehicular access points along the 20th Street frontage is expected, subject to traffic engineering review. Tenant development plans will be coordinated with existing conditions from previously approved adjacent parcels.

**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 Water and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards (including maximum building height, parking space requirements, signage, and setback requirements), see Chapter 7.0 - Development Regulations.

**Architectural Styles.** For design theme and architectural criteria, see Section 8.2 - Commercial Design Guidelines.

**Landscaping.** A primary intersection with monumentation will be located to the west of Planning Area 21 at Campus Avenue north of the freeway (for primary intersection design, see Section 9.3 Streetscapes). The southern boundary of Planning Area 21 adjacent to I-210 will feature a landscape area that provides attractive views into the development, and project identification signage (for design at freeway edge, see Section 9.6 - Edge Treatments, and for commercial project sign information, see Section 7.7 - Sign Standards). Just to the east of the Planning Area and Specific Plan boundary lies the Cucamonga Creek regional hiking and bike trail. A landscaped buffer located at the eastern edge of Planning Area 21 adjacent to the Cucamonga Creek Channel shall screen commercial development within the Planning Area (for buffer design, see Section 9.6 - Edge Treatments). An MWD right-of-way traverses the Planning Area; development within the right-of way in Planning Area 21 will comply with the MWD's guidelines. For commercial streetscape, commercial landscape palettes, and landscape development standards, see Chapter 9.0 -Landscaping.

### 2.3.25 Planning Area 22

**Location.** Planning Area 22 (approximately 3 acres) is located along the east side of Campus Avenue at the western edge of Planning Area 16 (Figure 2.26).

**Use.** Development in this Planning Area would include flexible residential uses, that would allow residential units to be developed at any density up to 24 du/ac (for permitted uses, see Table 7.B in Chapter 7.0), for a maximum of 72 dwelling units (RF 24.0). The residential units could be developed as either attached or detached units, depending on market demand at the time of construction.

If the available unit count for the Specific Plan is met prior to the development of this site, Planning Area 22 will be developed as a neighborhood commercial property. This will be implemented as detailed in Section 11.2, Specific Plan Administration.

**Infrastructure Phasing.** The infrastructure required to support development of Planning Area 22 is delineated in Table 10.A (see Chapter 10.0). Each backbone infrastructure component identified by a "✓" in the Table for Planning Area 22 is to be constructed and operational prior to the first occupancy of a structure within the Planning Area.

**Vehicular Access.** Vehicular access into Planning Area 22 will be based on user requirements and reasonable and generally accepted traffic engineering practices. For a typical tenant, two vehicular access points will be provided: one right in/right out vehicular access point along Campus Avenue and one along Colonies Parkway, each subject to traffic engineering review.

# PLANNING AREA 20

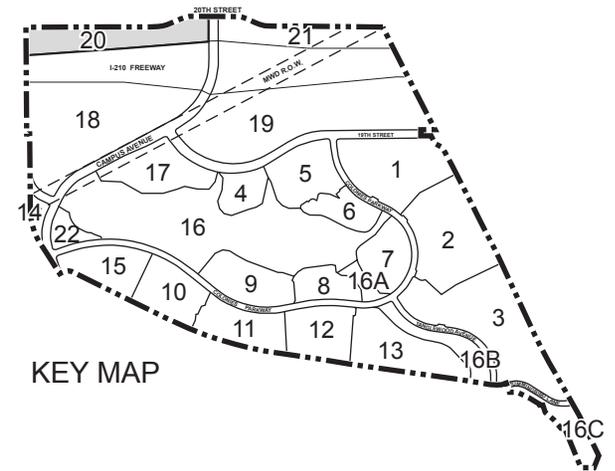
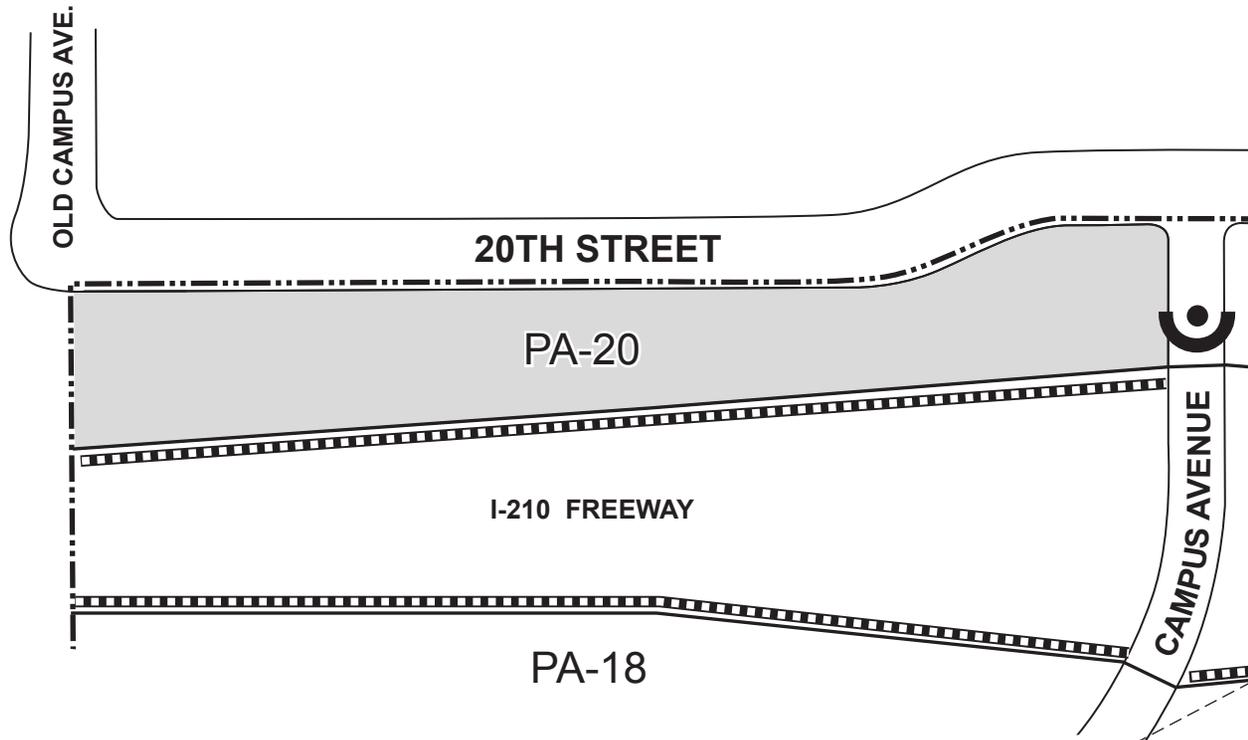
CH (Highway Commercial)

12.66 Acres

## LEGEND

-  Project Boundary
-  Freeway Edge Condition
-  Primary Intersection

NOTE: See Chapter 9 for design concepts.



KEY MAP

Figure 2.24

*The Colonies at San Antonio Specific Plan*

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N Not to Scale

Source: RJM Design Group.

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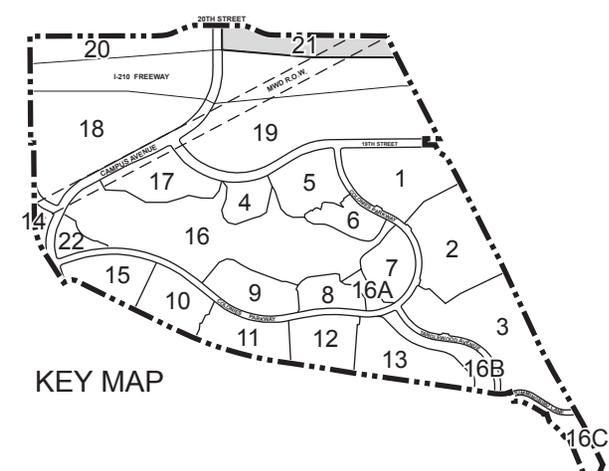
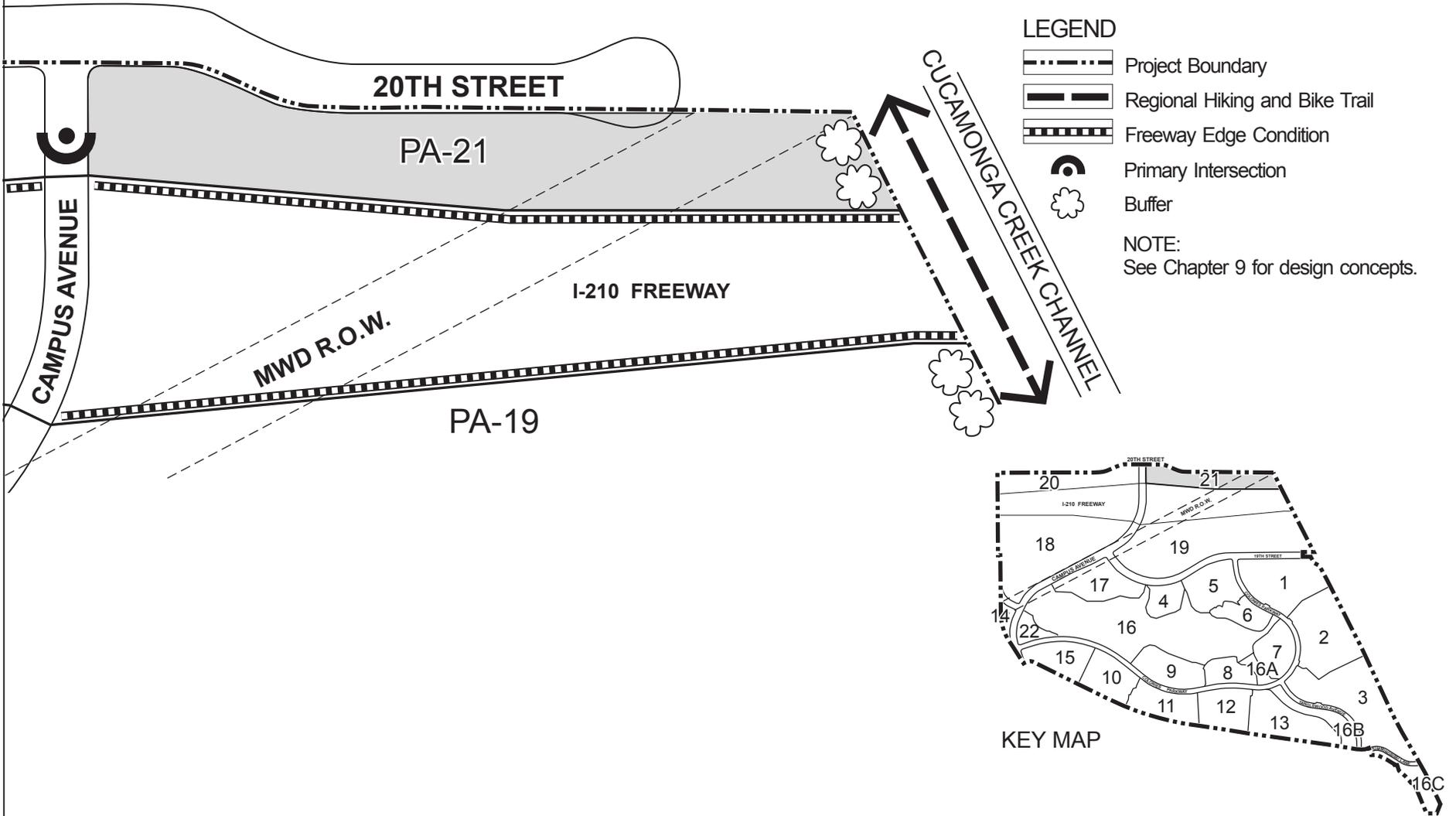
# PLANNING AREA 21

CH (Highway Commercial)  
11.02 Acres

## LEGEND

-  Project Boundary
-  Regional Hiking and Bike Trail
-  Freeway Edge Condition
-  Primary Intersection
-  Buffer

NOTE:  
See Chapter 9 for design concepts.



KEY MAP

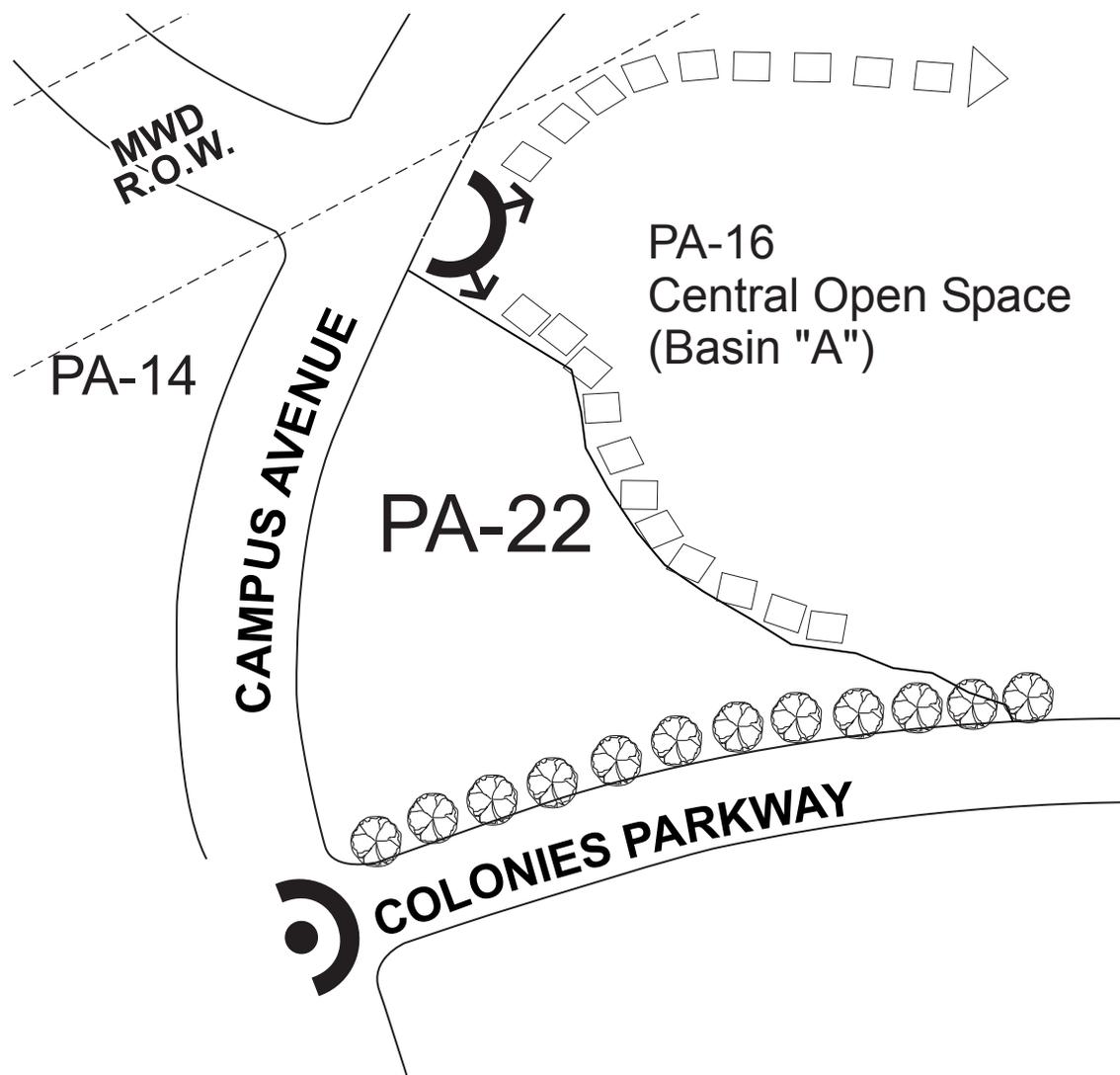
Figure 2.25

*The Colonies at San Antonio Specific Plan*

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Source: RJM Design Group.

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## PLANNING AREA 22

RF (Flexible Residential)

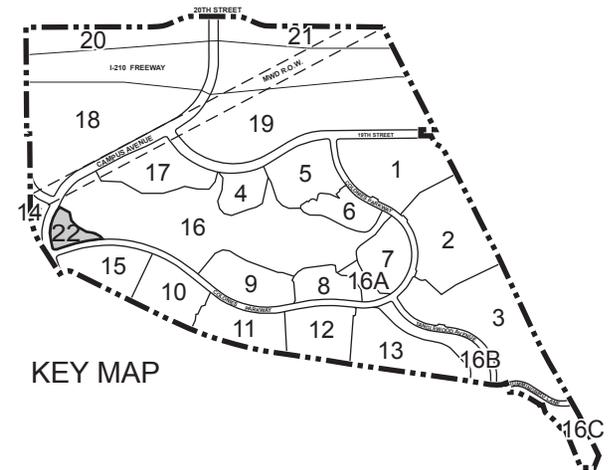
Up to 72 Units

3.0 Acres

### LEGEND

-  Community Trail and Greenbelt
-  Community Open Space Hiking Trail
-  Bio-Swale Drainage Channel
-  Service Drive
-  Overlook
-  Primary Intersection
-  Neighborhood Intersection  
*(locations of are conceptual only.)*

NOTE: See Chapter 9 for design concepts.



KEY MAP

Figure 2.26

*The Colonies at San Antonio Specific Plan*

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N Not to Scale

Source: RJM Design Group.

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**Roadway Cross Sections.** See Chapter 3.0 - Transportation.

**Drainage, Water and Sewer Facilities, and Grading.** See Chapter 4.0 - Drainage, Chapter 5.0 Water and Sewer, and Chapter 6.0 - Grading.

**Development Regulations.** For development standards for the attached and detached residential unit options (including maximum building height, parking space requirements, signage, and setback requirements), see Chapter 7.0 - Development Regulations.

**Architectural Styles.** For design theme and architectural criteria, see Section 8.2 - Commercial Design Guidelines.

**Landscaping.** For residential streetscape, residential landscape palettes, and landscape development standards, see Chapter 9.0 -Landscaping.

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## **3.0 TRANSPORTATION**

### **3.1 ROADWAYS**

The Colonies at San Antonio Specific Plan circulation concept is based on traffic impact analyses prepared by the firms of LSA Associates, Inc. (LSA) and Kimley Horn. The Colonies at San Antonio roadway concept is shown in Figure 3.1.

#### **3.1.1 Goals and Objectives**

As noted in Section 1.3.4 of the Colonies at San Antonio Specific Plan, the circulation goal of the proposed development is to “provide for safe and efficient vehicular, pedestrian, and bicycle movement within and through the Specific Plan area, while protecting the integrity of the Colonies at San Antonio planned community.” The roadway plan for the Specific Plan area recognizes that, with completion of the I-210 freeway, the majority of the through traffic now flowing along 19<sup>th</sup> Street will be diverted to the freeway, significantly changing the role of 19<sup>th</sup> Street. Whereas prior to freeway completion, 19<sup>th</sup> Street is the primary east-west connector in northern Upland and Rancho Cucamonga, following freeway completion, its primary role will be to accommodate shorter trips and distribute regional traffic from the freeway to local neighborhoods.

The Colonies at San Antonio Specific Plan also recognizes that the I-210/Campus Avenue interchange will become a major focus of traffic and thus proposes a circulation pattern to address this. Caltrans’ original design for the Campus Avenue interchange was to offset the interchange east of the existing Campus Avenue. Thus, the Caltrans design would have required traffic to make turns at two intersections in order to gain access to and from the freeway. The Colonies at San Antonio circulation plan improves upon this original freeway access design.

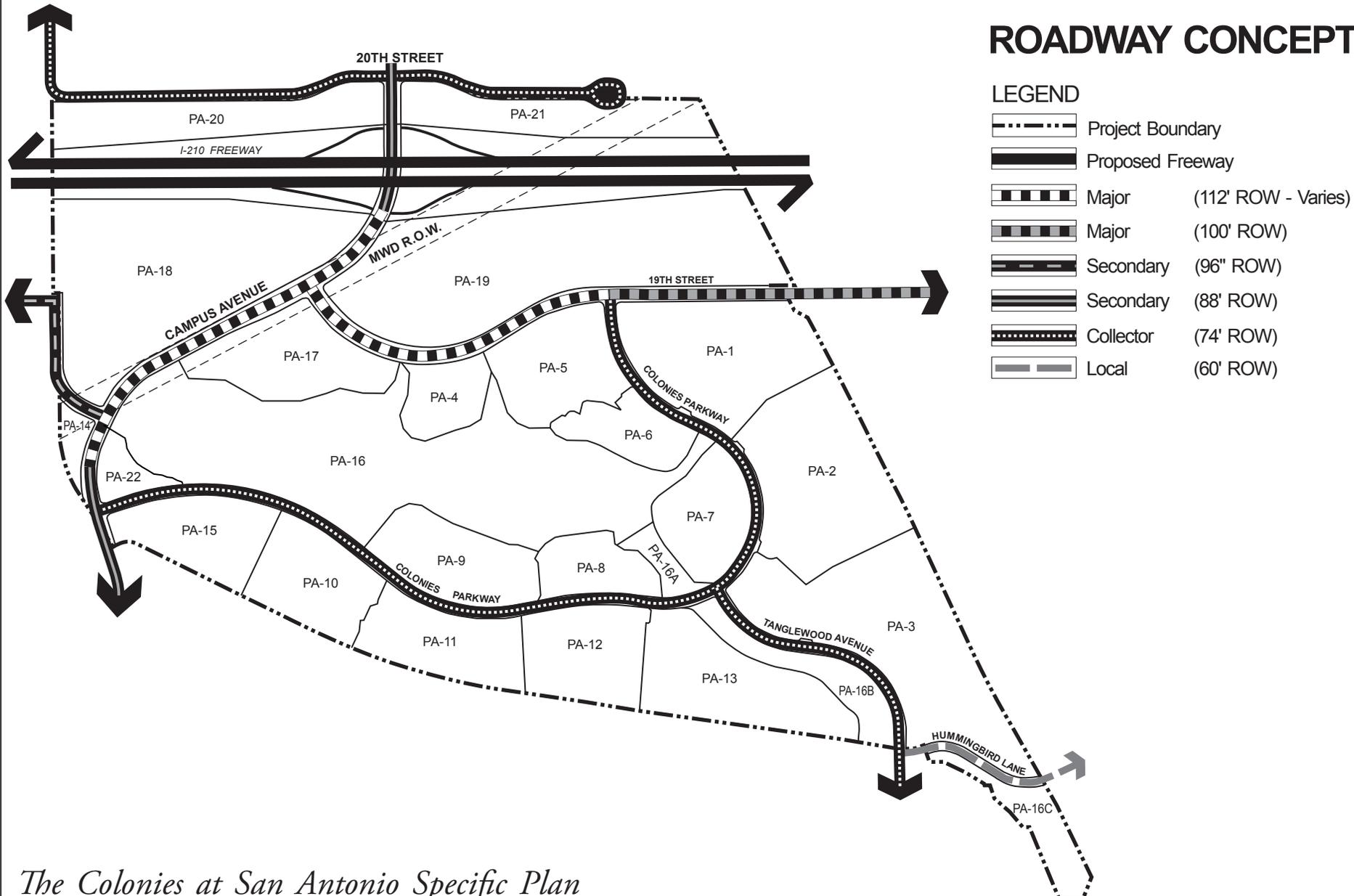
Once the freeway is available to accommodate regional through traffic, it is the objective of the Colonies at San Antonio Specific Plan to reduce traffic speeds and the noise generated by 19<sup>th</sup> Street. It is also the objective of the Specific Plan to facilitate southbound traffic from the I-210/Campus Avenue interchange. This will be accomplished by realigning both Campus Avenue and 19<sup>th</sup> Street. As shown in Figure 3.1, Campus Avenue will be realigned to provide a continuous route from the freeway to the south. An offset will be created along 19<sup>th</sup> Street. By realigning 19<sup>th</sup> Street, through traffic into residential neighborhoods to the west of the project site will be minimized, traffic speeds along 19<sup>th</sup> Street will be slowed, and the intrusion of noise from high speed through traffic into local residential neighborhoods will be minimized.

#### **3.1.2 Proposed Vehicular Circulation System**

To provide a safe and efficient street system, a hierarchy of roadways, including major highways (112' and 100' ROW), secondary highways (96' and 88' ROW), collector roads (74' ROW), and local residential streets (60' ROW) will be provided. Dedicated turning lanes will be provided at intersections along 19<sup>th</sup> Street, Campus Avenue, Tanglewood Avenue, and Colonies Parkway as

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# ROADWAY CONCEPT



*The Colonies at San Antonio Specific Plan*

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**N** Not to Scale  
 Source: LSA Associates, Inc.

Figure 3.1

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needed to provide for efficient traffic flow. In addition, turn lanes at the I-210/Campus Avenue interchange have been designed to provide adequate traffic flow during peak hour and normal operating conditions.

The Specific Plan proposes a total of about 34.76 acres of access roads including major highways, secondary highways, and collector roads. The I-210 freeway right-of-way (approximately 45.36 acres) is located east-west through the northern portion of the property. This freeway is currently under construction. An interchange will be constructed at the realigned Campus Avenue. Cross-sections for each street classification are shown in Figures 3.2A and 3.2B, Roadway Cross-Sections.

Key east-west routes include 19th Street, which is proposed to be realigned as a four-lane major highway with a raised landscaped median from Campus Avenue to Colonies Parkway, and a painted median east of Colonies Parkway. The curved realignment of 19th Street will encourage drivers who might use it as a through-street to instead use the freeway for this purpose. Thus, the curved alignment, which will intersect Campus Avenue, will help discourage all but local traffic usage.

Proposed as a two-lane collector road with dedicated left turn lanes into residential planning areas, Colonies Parkway will serve the southern and eastern portions of the site, between Campus Avenue (west) and Tanglewood Avenue (east) and 19th Street (north). Along the northern site boundary, 20th Street is proposed as a two-lane collector road to serve the commercial uses. Tanglewood Avenue, which will terminate at Colonies Parkway, will provide access to residential areas to the south.

Primary north-south routes include Campus Avenue at the western boundary. The northern portion of Campus Avenue is proposed to have a new alignment, connecting to the I-210 freeway. The realigned Campus Avenue is designed as a four-lane divided major highway to carry anticipated heavy traffic flows diagonally northeast through the site, and directly to the freeway and major commercial land uses. Another north-south route is Colonies Parkway, serving the eastern portion of the project and bringing traffic north from existing Tanglewood Avenue in the southern portion of the project site. This roadway is proposed as a two-lane collector road.

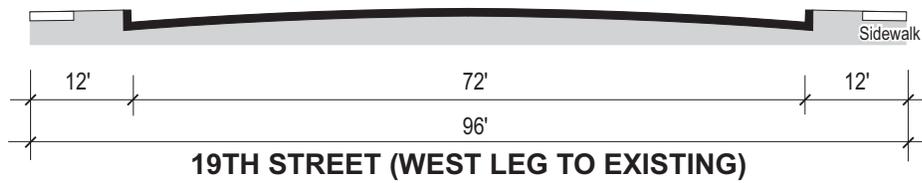
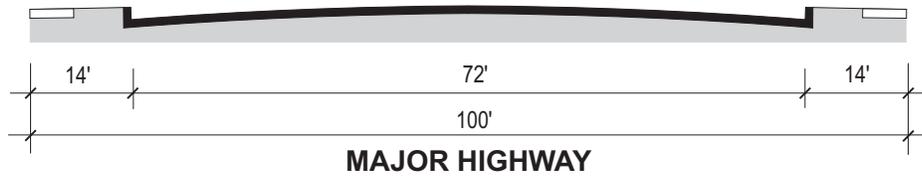
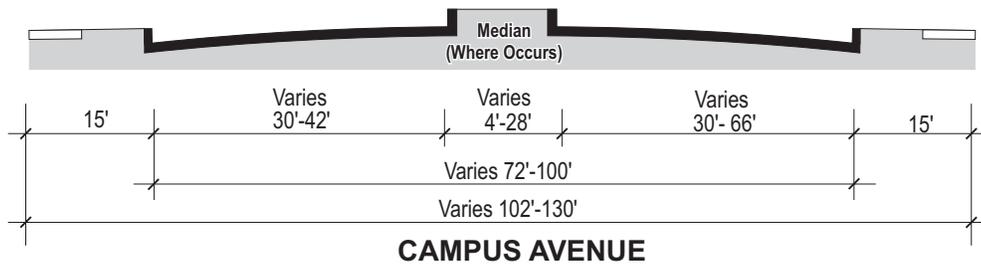
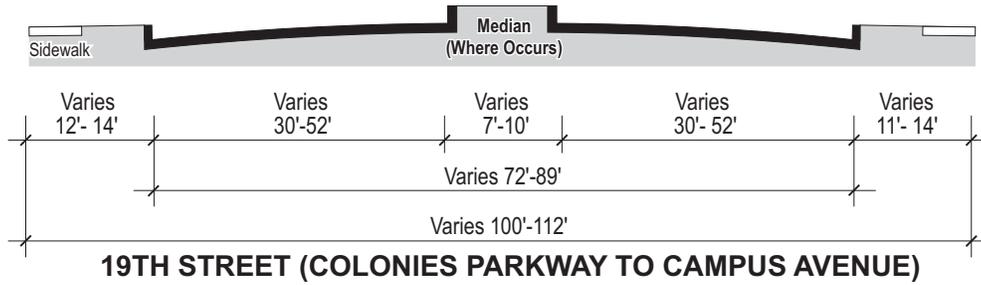
The timing of on-site roadway construction and intersection signalization in relation to development within the Colonies at San Antonio Specific Plan area is identified in Table 10.A.

Prior to the issuance of building permits, the applicant shall pay fair share project costs for the following off-site intersection improvements per the project Traffic Impact Analysis, as modified by the Colonies at San Antonio Development Agreement, as amended.

- *16th Street and Mountain Avenue:* Add an eastbound right turn lane, northbound left turn lane, and a northbound right turn lane. Modify existing traffic signal.
- *Baseline Road and Carnelian Avenue:* Add an eastbound right turn lane, westbound right turn lane, and a southbound left turn lane. Modify existing traffic signal.
- *Baseline Road and Archibald Avenue:* Add an eastbound through lane and a westbound right turn lane. Modify existing traffic signal.
- *19th Street and Sapphire Avenue:* Add a westbound through lane. Modify existing traffic signal.

In addition, traffic analyses have indicated that signalization will be required at the Campus Avenue/17th Street and Campus Avenue/View Point intersections. Campus Avenue/17th Street currently has sufficient approach volumes to warrant signalization during both the a.m. and p.m. peak

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# ROADWAY CROSS SECTIONS

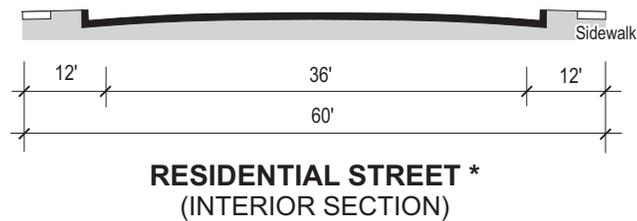
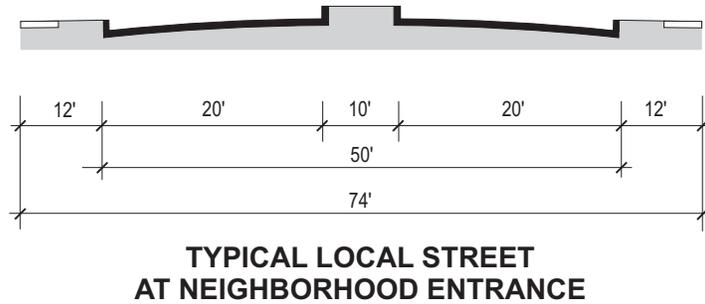
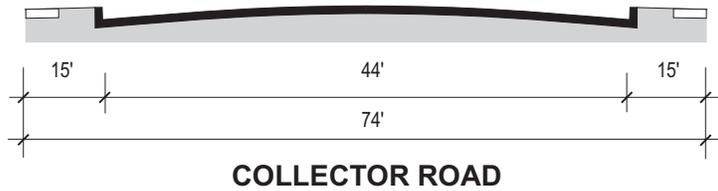
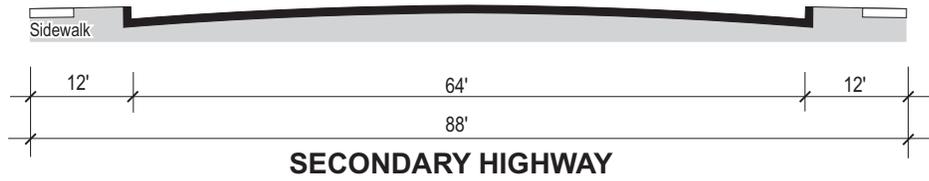
*The Colonies at San Antonio Specific Plan*

Figure 3.2A

Not to Scale

Source: LSA Associates, Inc.

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\*Private streets shall be per city's engineer's standards.

## ROADWAY CROSS SECTIONS

*The Colonies at San Antonio Specific Plan*

Figure 3.2B

Not to Scale

Source: LSA Associates, Inc.

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hours. Under 2020 conditions, Campus Avenue/View Point will have sufficient approach volumes to warrant signalization during the a.m. peak hour. However, prior to signalizing these locations, a more rigorous analysis needs to be conducted using all of the applicable 11 signal warrant criteria.

Because the Campus Avenue/17<sup>th</sup> Street currently appears to meet traffic warrants, development of the Colonies project will increase traffic volumes, but not create a need for signalizing the intersection. At the Campus Avenue/View Point intersection, project development will contribute to the need for a traffic signal prior to 2020. The Colonies at San Antonio will be required to make a fair share contribution to signalization of the Campus Avenue/View Point intersection. Campus Avenue/View Point and Campus Avenue/17<sup>th</sup> Street should be monitored by the City to determine the specific timing of when signalization at these locations may actually be warranted under future conditions.

### **3.2 WALKING AND BICYCLE TRAILS**

An extensive hiking and bicycle trail system will be developed on-site for the enjoyment of site residents and visitors (see Figure 3.3, Trails Plan and Section 9.2, Open Space/Trails). The trail network will be designed along with the central open space and restored riparian habitat area as a community focal point. All bicycle trails and circulation routes will be constructed per the City of Upland's circulation standards. In addition to this trail network, a safe and efficient system of sidewalks will be installed throughout the Specific Plan area. All sidewalks along all major, secondary, and collector roadways will be constructed per City of Upland standards with a minimum width of 5 feet.

### **3.3 CIRCULATION PLAN DEVELOPMENT STANDARDS**

The Colonies at San Antonio Specific Plan includes an efficient and safe circulation design that will accommodate traffic, public safety, security and public transportation needs. Figure 3.1, based on the Master Circulation Plan outlined in the project Traffic Impact Analysis, will serve as the composite Circulation Plan for the Colonies at San Antonio. All on-site roadway improvements illustrated will be constructed in accordance with this plan.

Heavy through traffic on site will be eliminated from residential neighborhoods, since they will be served by residential streets connecting from Colonies Parkway, and there will be no through routes provided. Financing of major roadway improvements could be accomplished through the use of assessment districts, redevelopment funds, developer financing, or a variety of other methods for financing certain public improvements to be provided as part of this project, including 19<sup>th</sup> Street, Colonies Parkway, the realignment of Campus Avenue and 20<sup>th</sup> Street.

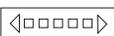
All roads shall be constructed to ultimate City standards as a requirement of implementing subdivisions for the Specific Plan, subject to approval by the City Engineering Department.

An extensive system of off-street pedestrian and bicycle trails will be developed in the southern portion of the site. In addition, on-street bicycle lanes may also be constructed on major streets. A sidewalk system will also be implemented throughout the project area.

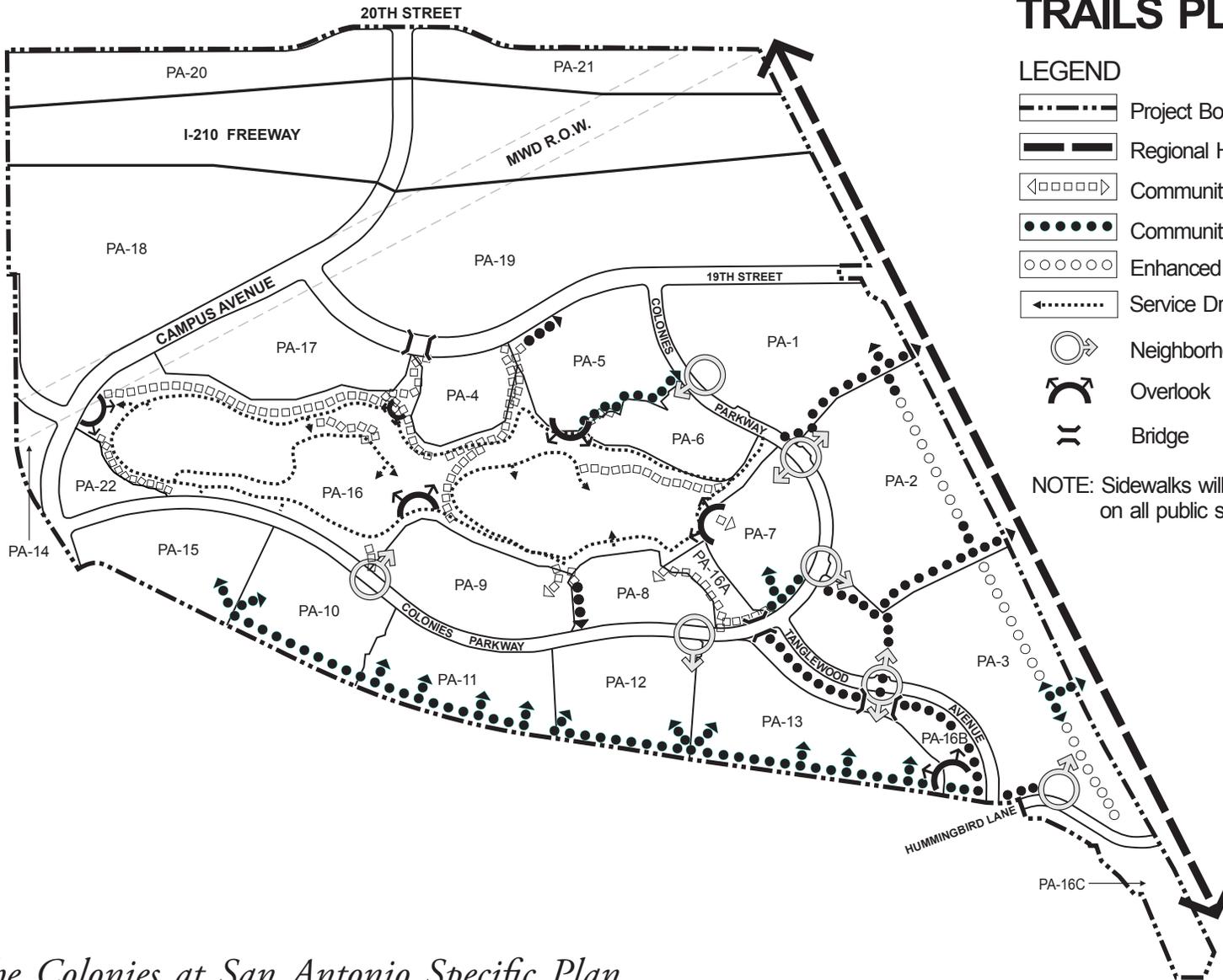
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# PRELIMINARY TRAILS PLAN

## LEGEND

-  Project Boundary
-  Regional Hiking and Bike Trail
-  Community Open Space Hiking Trail
-  Community Trail
-  Enhanced Parkway
-  Service Drive/Trail
-  Neighborhood Entry and Trail Connection
-  Overlook
-  Bridge

NOTE: Sidewalks will be constructed on all public streets.



*The Colonies at San Antonio Specific Plan*

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N Not to Scale

Source: RJM Design Group.

Figure 3.3

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## **4.0 DRAINAGE**

### **4.1 INTRODUCTION**

The watershed tributary to the Colonies at San Antonio Specific Plan area totals 2,563 acres. The majority of this watershed is located outside of the Specific Plan area; however, the Specific Plan area is required by the San Bernardino County Flood Control District to receive, detain, and outlet these local and regional flows. Within the Specific Plan area, I-210 intercepts runoff from 73.5 acres located at the northern boundary of the site. To the south, the remaining on-site watershed covers a total of 391.5 acres.

This chapter illustrates the backbone facilities and improvements, based on preliminary design flow calculations, that are necessary to provide drainage and flood protection to the Colonies at San Antonio Specific Plan area. The conceptual master plan and local facilities identified in this chapter will drain the project site to Cucamonga Creek Channel, a regional facility located along the eastern boundary of the Specific Plan. Detailed information on the hydrologic conditions affecting the Colonies at San Antonio Specific Plan and the Specific Plan's storm drain plan are included in the Storm Drain Master Plan prepared for the Colonies at San Antonio Specific Plan. This Master Plan is available under separate cover.

### **4.2 STORM DRAIN SYSTEM CONCEPT**

Using circulation and grading plan information, a storm drain layout for required local, regional, and I-210 flows was developed by the project engineer. The proposed backbone storm drain system for the Colonies at San Antonio Specific Plan is shown in Figure 4.1. Considered in this layout are the design parameters of the San Bernardino County Flood Control District, as well as drainage infrastructure constructed as part of the I-210 project. Flows from the 19<sup>th</sup> Street Storm Drain and the 20<sup>th</sup> Street Storm Drain discharge into Basin A within the Specific Plan's central open space. The Campus Avenue Storm Drain will connect to the 20<sup>th</sup> Street Storm Drain, and then flow in the central open space area. Flows from the I-210 culvert located at the east boundary of the project are conveyed to the storm drain in Colonies Parkway that discharges directly to Basin B.

Also considered was collection and/or safe direction overland of 100 year storm surface flows. Drainage systems in or crossing 19<sup>th</sup> Street, 20<sup>th</sup> Street, and Campus Avenue must be sized to collect and convey 100 year storm flows. The proposed storm drain along the southern boundary of the Specific Plan area will also need to be sized for 100 year storm flows from the adjacent tracts. All Planning Areas on the north side of Basin A will discharge to Basin A. Low points must also be similarly designed to dewater 100-year storm surface flows.

The central open space area of Planning Area 16 is designed to accommodate stormwater flows that currently run through the site, ensuring that the capacity of the adjacent Cucamonga Creek Channel will not be exceeded by on-site *or* upstream development. A controlled outlet at the downstream end of the basin, consisting of a gated valve, will detain and discharge flows up to a 100-year storm level into Planning Area 16B.

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# PROPOSED BACKBONE STORM DRAIN SYSTEM

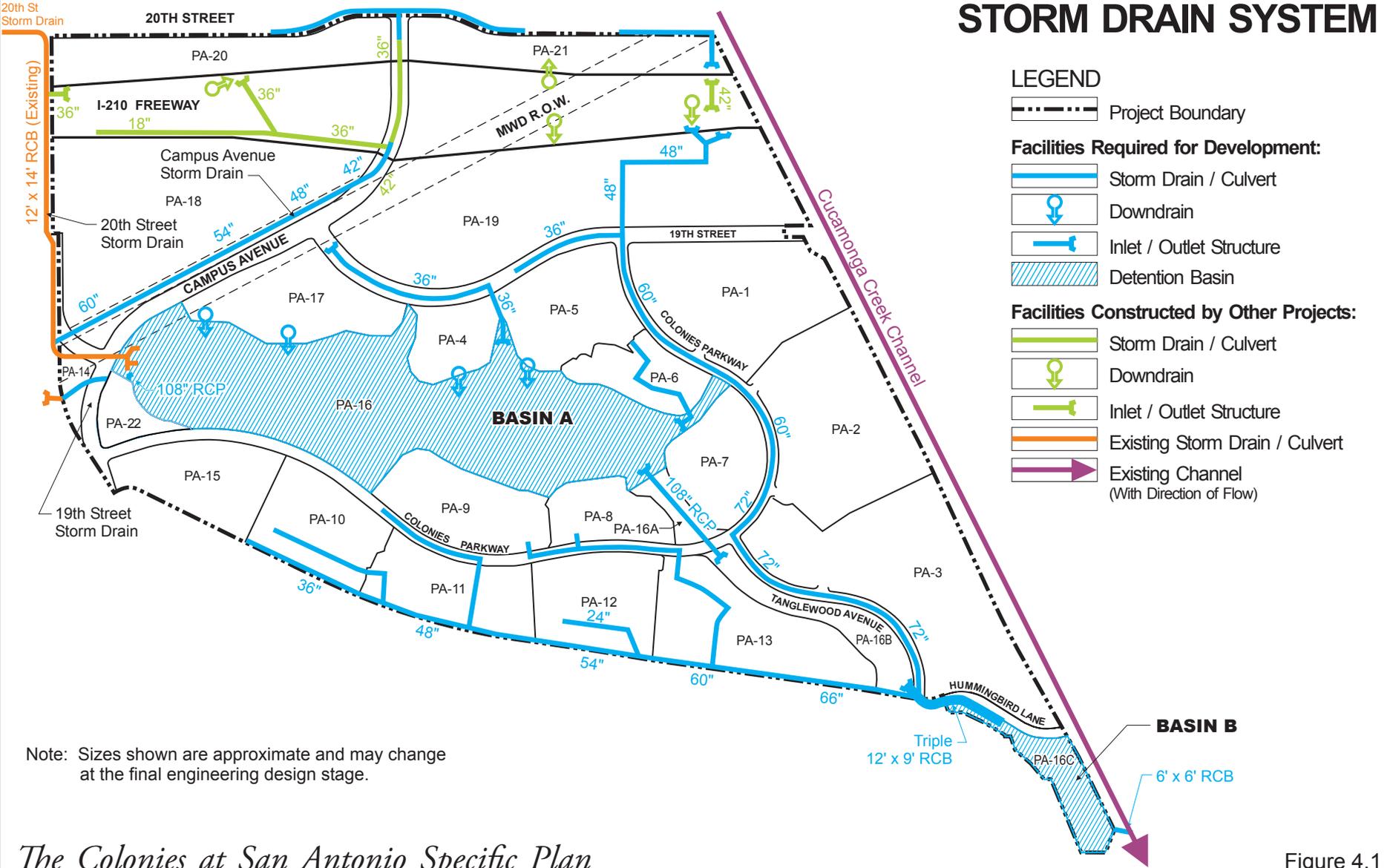


Figure 4.1

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Flows from storms greater than a 100-year level will exit Basin A over a different controlled outlet, discharging into Planning Area 16A. The main feature of Planning Area 16A is the rock outlet facility that originates in the adjacent Planning Area 16. The floodway directs water south under Colonies Parkway to Detention Basin B in Planning Area 16C, where flood waters are outlet at a controlled rate of flow into the Cucamonga Creek Channel located to the east just outside the Specific Plan boundary.

The central open space area is composed of three sub-basins designed to maintain historic levels of groundwater recharge and help sustain wetland/riparian plant habitat. Modifications to the central basin area have been necessitated by the settlement agreement entered into by the City, the San Antonio Water Company, the Cucamonga County Water District, a citizens group, and the property owner in a CEQA challenge against the Colonies at San Antonio EIR. As required by the conditions of approval for master tentative tract 16198, a Flood Emergency Action Plan was prepared. The Flood Emergency Action Plan required an interim basin with a detention capacity of 582 acre-feet and ultimate improvements which will raise the spillway and increase detention capacity to 799 acre-feet. The design of the recharge facilities will maintain historic rates of groundwater recharge in both the interim and ultimate conditions, as defined in the settlement agreement.<sup>1</sup> Interim and ultimate basin conditions are described in Table 4.A. Four sources of water flow to the basins: (1) flow from the 19<sup>th</sup> and 20<sup>th</sup> Street storm drains; (2) flow from the San Antonio Water Company; (3) nuisance flow, and (4) drainage from lands within the Colonies at San Antonio Specific Plan. The water entering the basin will have the following uses: (1) evapo-transpiration, which is the water used by plants; (2) percolation within the three sub-basins within the open space basin; and (3) outflow, in which periodic rainfall events generate a surplus of water that will leave the open space detention basin, and flow into the Cucamonga Creek channel.

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See Section 3 (Settlement and Amendment of the 1966 Spreading Easement) of the Settlement Agreement in the matters of *The Colonies Partners L.P. v. San Bernardino County Flood Control District and San Antonio Water Company* (San Bernardino County Superior Court RCV061971) and *Cucamonga County Water District v. City of Upland* (San Bernardino County Superior Court RCV067462). The settlement agreement in these matters was reached between the Colonies Partners LP (developer of the Colonies project), the City of Upland, San Antonio Water Company (SAWCO), and the Cucamonga County Water District (CCWD).

In the settlement agreement, the parties agreed on a definition of “historic recharge” (2,233 acre feet of water imported by SAWCO on an average annual basis and an annual average of 1,232 acre feet of stormwater from Upland. The settlement agreement sets forth the rights and responsibilities of parties to implement the “Basin Plan,” which revised the configuration of sub-basins “A-2” and “A-3.” This resulted in the design for Planning Area 16 set forth in the Colonies at San Antonio Specific Plan.

Information on the hydrology of the central open space basin is presented in *Hydrologic Summary of Basin Conditions at The Colonies at San Antonio*, prepared November 2003 by Associated Engineers, Inc. Information on groundwater recharge rates within Planning Area 16 is presented in the November 3, 2003 report of Daniel B. Stephens & Associated, titled *Infiltration Analysis of Proposed Colonies Spreading Grounds.*”

**Table 4.A: Comparison of Interim and Ultimate Basin Construction**

Feature	Initial Construction/ Interim Condition	Ultimate Condition
Overflow (Breach)	Elevation 1,537' AMSL. The breach will be protected by ungrouted rock (rip rap).	Elevation 1,544' AMSL. The overflow will be protected by a concrete spillway and a State-permitted concrete dam with headwalls.
Dead storage capacity	Approximately 582 acre-feet, an increase from the approximate 345 acre-feet of storage in the existing condition. The 582 acre-feet of storage is approximately the capacity to hold three inches (3") of rain in a 12-day period.	Approximately 799 acre-feet, approximately the capacity to hold four inches (4") of rain in any time period.
Outlet	Storm flows below the 1,537 elevation will only leave the basin to provide additional storage through evaporation, recharge, or pumping.	There will be a controlled (gated valve) outlet at elevation 1,516. This valve may be opened at the discretion of the operator (proposed to be the San Bernardino County Flood Control District) to drain detained stormwater to about 65 acre-feet in storage (remaining volume below elevation 1516). It would take less than 24 hours to drain the basin from full to this level. After draining the basin, the operator may either leave the drain open to allow controlled flow to reach the Cucamonga Creek channel at a maximum flow rate of 658 cfs, or close the gated valve to capture future storm flows.
Landscaping	The basin will be hydroseeded with native vegetation to provide interim groundcover consistent with the ultimate mitigation plan. The hydroseed will be monitored and, if needed supplemented with irrigation to aid in its growth and establishment. The vegetation will act as a filtration system to help cleanse stormwater prior to its reaching the primary recharge basin (A-3).	The basin will be replanted with a mix of native riparian, wetland, and Southern California upland vegetation as mitigation for project impacts on existing riparian habitat. The planting will be monitored and, if needed supplemented with irrigation to aid in its growth and reestablishment. The vegetation will act as a filtration system to help cleanse stormwater from northeast Upland prior to reaching the primary recharge basin (A-3) and the Cucamonga Creek channel.  In addition to vegetation acting as a filtration system, a meandering streambed will be constructed of local and artificial rock. The streambed will incorporate drop structures and scouring basins that will reduce velocities and provide for sediment removal.
Grading	The central basin area will be rough graded to its ultimate configuration, with the exception of the spillway.	The spillway will be raised to elevation 1544.

Feature	Initial Construction/ Interim Condition	Ultimate Condition
Hardscape Improvements	No hardscape improvements will be provided. Access roads (20' wide) and trails (10' wide) will be graded in their ultimate location, but will not be paved.	Access roads and trails graded in the interim condition will be improved. The trails and roads will provide access for maintenance, as well as hiking/jogging and passive recreation.
Fencing and Access Restrictions	Because trails are not provided in the interim condition, and there is no need for public access, the basin itself will be fenced along the perimeter to prevent unauthorized access.	A wrought iron fence will be placed along the perimeter, generally at the basin's high water mark. The fence will be provided with vehicular and pedestrian gates that will remain open in the dry season and closed (locked) in times of large storms.
Standing Water	There may be standing water in Basin A-3 through the winter and spring rainy season.	There may be standing water in Basin A-3 through the winter and spring rainy season.
Flood Control Design	Storm flows exceeding the magnitude of a 10-year event will flow over the spillway at elevation 1,537 in a controlled manner through Planning Area 16A. Water will flow along the surface adjacent to Tanglewood Avenue in a rock-lined channel through Planning Area 16B, and enter the existing triple box structure, flowing into the basin within Planning Area 16C. Storm flows will reach the Cucamonga Channel at a rate of 1,804 cfs, in excess of the approved rate of 658 cfs, but below the existing rate of 3,400 cfs, consistent with the Flood emergency Action Plan.	During the design storm (back-to-back 100-year storms over a 5-day period), the valved outlet at elevation 1,516 will be opened, and storm flows will reach the Cucamonga Channel at a rate of 625 cfs, below the approved rate of 658 cfs.

### 4.3 STORMWATER TREATMENT

California's Nonpoint Source Management Plan was approved by the State Water Resources Control Board (SWRCB) in 1988. In 1993 the United States Environmental Protection Agency (USEPA) published guidance for specifying management of nonpoint sources of pollution which was incorporated into the California Nonpoint Source/Coastal Zone Act Reauthorization Amendments (NPS/CZARA) Program. The new federal regulations for nonpoint source pollution are expected to require the State of California to issue Phase II National Pollutant Discharge Elimination System (NPDES) permits as of May 31, 2002. The permit program adopted by the State of California will require permits for stormwater discharges for:

- All municipalities within urbanized areas
- Small municipalities outside of urbanized areas of at least 10,000 and/or a population density of at least 1,000 persons/square mile
- Construction sites that disturb between 1 and 5 acres.

Under California's NPS management plan, six management measures are specified which include Wetlands, Riparian Areas and Vegetated Treatment Systems. Management Measure 6C (MM6C) "promotes the installation of vegetated treatment systems (e.g., artificial or constructed wetlands) in areas where these systems will serve a polluted run-off abatement function. Vegetated filter strips and engineered wetlands (also known as bio-filters) remove sediment and other pollutants from runoff and wastewater, and prevent pollutants from entering adjacent water bodies. Removal typically occurs through filtration, deposition, infiltration, absorption, adsorption, decomposition, and volatilization."

The open space basin proposed in Planning Area 16 is designed to provide treatment for low flow runoff and nuisance water conditions. Normal rainfall and nuisance water discharging into Planning Area 16 will receive treatment via the treatment processes specified above in MM6C. Major storm events will not receive the same level of treatment because the volume of stormwater exceeds the system's capacity for treatment. However, major storm events do not have the same pollutant concentrations as low flow events. Stormwater pollutant loads are highest at the beginning of the rainy season, after light rains, and during dry weather when nuisance water is discharging into the basin. The objective of the stormwater treatment concept for the Colonies at San Antonio is that the water quality of stormwater leaving the site be equal to that of stormwater entering the basin in Planning Area 16 from outside sources such as the 19<sup>th</sup> Street and 20<sup>th</sup> Street storm drains. Thus, mitigation will be provided for the water quality effects of drainage generated *within* the Specific Plan area.

The desire of the San Antonio Water Company (SAWCO) and Cucamonga County Water District (CCWD) to maintain historic levels of groundwater recharge has been provided for in a manner consistent with the Federal requirement to treat stormwater. In order to provide groundwater recharge to the quantities identified in the settlement agreement with SAWCO and CCWD, Basin A-3, a 4.11-acre sub-basin, will be provided, free of all vegetation. The design described for the open space basin (Planning Area 16) balances the Federal requirements for stormwater treatment with historic groundwater recharge in Planning Area 16's sub-basins.

## **5.0 WATER AND SEWER FACILITIES**

### **5.1 WATER FACILITIES**

To serve the Colonies at San Antonio, a water system will be constructed, and service will be provided by the City of Upland, within whose service area the project site is located. The Colonies development will be served by pressure Zone 3 of the City of Upland's water system, which derives its supplies from the following sources: City-owned wells, wells owned by others, existing water treatment plants, and connections with other agencies. Detailed information on water facilities is provided in the Colonies at San Antonio Technical Study, which is available under separate cover.

#### **5.1.1 Water Demand**

The land use proposed for the Colonies at San Antonio Specific Plan, along with City of Upland water consumption factors for the proposed land uses, was used to calculate the future water demand for the Colonies at San Antonio Specific Plan. The water technical study prepared for the Colonies at San Antonio Specific Plan indicates that daily water consumption at build out will be 910,000 gallons per day.

#### **5.1.2 Water Technical Study**

Domestic water will be provided to the Colonies at San Antonio Specific Plan area by the City of Upland. The backbone water system required to serve project site and surrounding properties is illustrated in Figure 5.1. The water system proposed for the Colonies at San Antonio Specific Plan will be connected to an existing Upland 12-inch water line on 19<sup>th</sup> Street at the western boundary of the Specific Plan, to the existing water interconnection between the City of Upland and the Cucamonga County Water District on 19<sup>th</sup> street at the eastern boundary of the Specific Plan, and to the existing 8-inch water line on Tanglewood Avenue south of the Specific Plan boundary. Where feasible, looped connections to existing facilities have been provided. Water lines have been sized to meet an anticipated fire flow and projected domestic water demands. Off-site water facilities, such as wells, storage facilities, and related off-site connections and improvements will be constructed at off-site locations as determined by the City Engineer.

### **5.2 SEWER FACILITIES**

#### **5.2.1 Introduction**

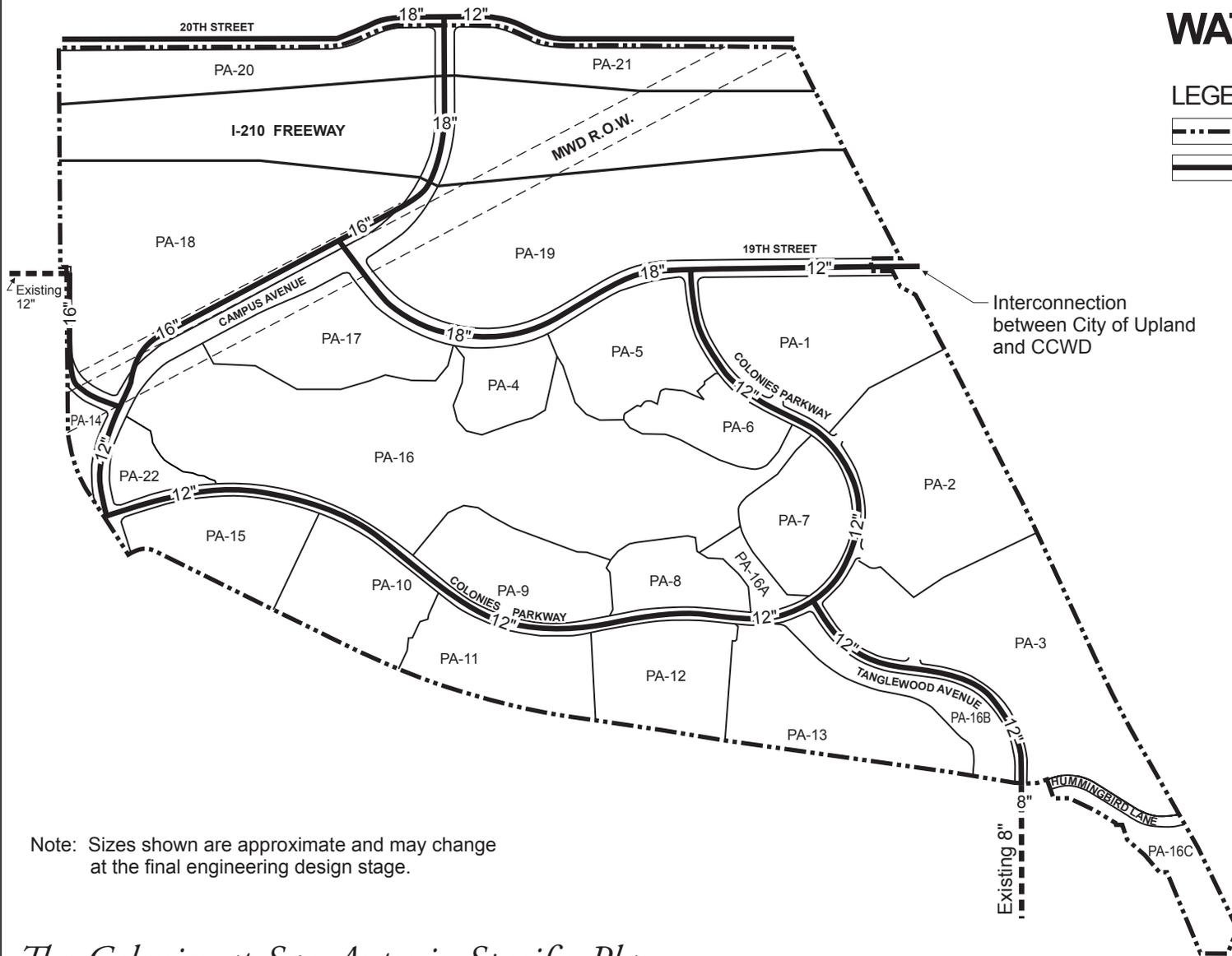
Sewage collection services for the Colonies at San Antonio Specific Plan area are provided by the City of Upland. Sewage is treated by the Inland Empire Utilities Agency (IEUA) at their Regional Treatment Plant No. 1 in Ontario (RP-1). Detailed information on sewer facilities is provided in the Colonies at San Antonio Sewer Technical Study, which is available under separate cover.

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# BACKBONE WATER SYSTEM

## LEGEND

-  Project Boundary
-  Water Line



Note: Sizes shown are approximate and may change at the final engineering design stage.

*The Colonies at San Antonio Specific Plan*

↑  
N Not to Scale

Source: Associated Engineers, Inc.

Figure 5.1

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### 5.2.2 Sewage Generation Criteria

The projected sewage generation has been calculated based on generally accepted rates used by the local sewer agencies for the different types of development. The following rates, as shown in Table 5.A, have been used:

**Table 5.A - Sewage Generation Rates**

<b>Land Use</b>	<b>Generation Rate</b>
Single Family Residential (SFR)	270 GPD/Lot
Multi-Family	190 GPD/Unit
School	1500 GPD/Acre
Commercial Highway (CH)	1500 GPD/Acre
Neighborhood Commercial (NC)	1500 GPD/Acre

Based on these generation factors, the Colonies at San Antonio Specific Plan could generate up to 0.4852 million gallons per day (MGD) non-peaked flows with a 1.4635 MGD peaked flow.

### 5.2.3 Sewer Technical Study

Figure 5.2 shows the conceptual backbone sewer layout for the Colonies at San Antonio Specific Plan. Flows generated by the initial increment of development (Planning Areas 1, 2, and 3) can be adequately handled by the majority of the existing Upland sewer system. Because the existing City sewers between Hummingbird Lane and Fifteenth Street are not adequate to accept the additional flows from Planning Areas 1, 2, and 3, a 12-inch diameter relief sewer, which will bypass the undersized reach, will be constructed with initial infrastructure improvements. It will run from the project boundary southerly in Tanglewood Avenue across Baseline Road, and then across the Upland Hills Golf Course to the 15<sup>th</sup> Street Basin. The new sewer will then run westerly along the south edge of the flood control basin to Grove Avenue, where it will join an existing Upland sewer line. Modeling shows that all existing lines south of this intersection are currently adequate for Planning Areas 1, 2, and 3 sewage flows, based on the peak flows calculated for the lines currently in use.

With completion of the balance of the Specific Plan, approximately 1,400 linear feet of existing 8-inch sewer in Grove Avenue, between Fifteenth Street and Fourteenth Street, will become deficient, and a 10-inch relief sewer must be constructed. The increased north-south gradient allows the relief sewer to be a 10-inch pipe, working in parallel with the existing 8-inch sewer. In the Memorial Park East Access Road, from Foothill Boulevard south to San Bernardino Road, there is a 1,150 linear foot reach of existing 15-inch diameter sewer that constricts the additional flows added to the system by the Colonies at San Antonio Specific Plan. New 10-inch and 12-inch relief sewers will be required to run parallel to this sewer.

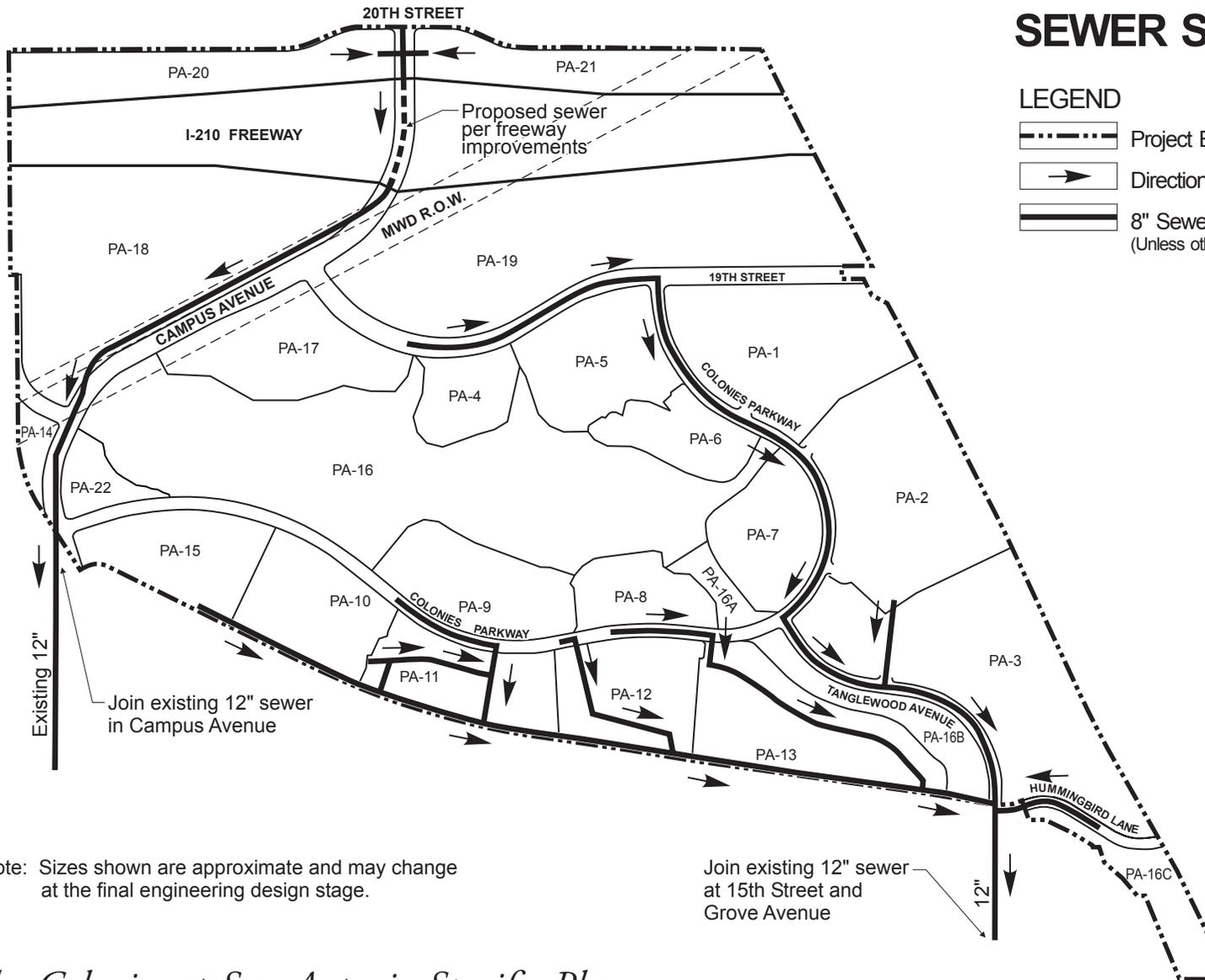
Approximately 2,900 linear feet of new 10-inch, 12-inch, and 15-inch sewer must be constructed in San Bernardino Road, Thirteenth Avenue and Arrow Highway to handle the increase in peak flows to the existing Upland sewer system. In Grove Avenue, the existing 18-inch sewer will need a parallel

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# BACKBONE SEWER SYSTEM

## LEGEND

-  Project Boundary
-  Direction of flow
-  8" Sewer  
(Unless otherwise noted)



Note: Sizes shown are approximate and may change at the final engineering design stage.

Join existing 12" sewer at 15th Street and Grove Avenue

*The Colonies at San Antonio Specific Plan*

↑  
N Not to Scale

Source: Associated Engineers, Inc.

Figure 5.2

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8-inch relief sewer between the alley south of Ninth Street and the manhole at the AT&SF Railroad. The completion of these sewer improvements will allow The Colonies at San Antonio project to construct up to the 1,150 residential units approved under the Specific Plan. Once these relief sewers are constructed down to the intersection of Arrow Highway and Grove Avenue, the system can handle all of the flows generated by the Colonies at San Antonio. The additional relief sewers discussed are the minimum improvements required for development of Planning Areas 4 through 21. The City of Upland wishes to construct the relief sewers as replacement sewers in order to avoid having two sewer mains in a street. The City intends to participate financially for any additional expense caused by combining existing and new relief sewers into one line and making all new lateral connections. All sizes shown in the Sewer System Map of the Sewer Technical Study are preliminary. Actual sizes to be constructed as replacement sewers will be determined prior to design. The City has also requested that since the depth to diameter ratio exceeds 1:1 for the existing 18-inch sewer in Grove Avenue between Arrow Highway and Ninth Street, it could be replaced.

The City-owned sewer joins the Upland/Ontario Interceptor owned by the IEUA at Grove Avenue and Eighth Street. The IEUA is currently working on future relief sewers for the Upland/Ontario Interceptor in anticipation of future growth in the Upland and Ontario area. They have anticipated accepting flows from the Colonies project and have stated that if the City's sewer flows do not exceed projected flows, then the capacity is allocated. Since the current flow projections have decreased from prior estimates, the reserved capacity is adequate at Regional Plant 1 for the phased construction of the Colonies development, especially once the proposed new IEUA relief sewers are constructed.

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## **6.0 GRADING**

### **6.1 GRADING PROGRAM**

The proposed grading plan for the Colonies at San Antonio will result in reclamation of past mining activities, and return the site to a productive state (Figure 6.1). Earthwork quantities are estimated to be approximately 5,034,000 cubic yards of excavation (cut) and approximately 4,721,700 cubic yards of embankment (fill). Balanced grading of cut and fill has been achieved using fill from the construction of I-210. The intent is to place excavated material in engineered fills designed to allow surface drainage to flow generally toward the flood control facilities.

### **6.2 SPECIAL TECHNIQUES/DEVELOPMENT STANDARDS**

All grading activities shall be in substantial conformance with the overall grading concept exhibit, as well as with any grading-related mitigation measures outlined in the EIR prepared for the Colonies at San Antonio Specific Plan Amendment (date to be determined). Additionally, all grading and earthwork necessary for implementation of the Colonies at San Antonio Specific Plan shall adhere to the Uniform Building Code and all applicable City of Upland codes, requirements and standard drawings. Prior to the commencement of grading operations, the City of Upland shall have first approved grading permits pursuant to City ordinance. Project grading shall meet the following requirements.

- All grading will occur in conformance with City engineering guidelines and shall, whenever possible, be balanced on site.
- Prior to any development within any area of the Specific Plan, an overall conceptual grading plan for the portion in process shall be submitted to the Engineering Department of the City of Upland for approval. The grading plan for each such area shall be used as a guideline for subsequent detailed grading plans for individual stages of development within that area, and shall include techniques utilized to prevent erosion and sedimentation during and after the grading process; approximate time frames for grading; and preliminary pad and roadway elevations.
- Detailed grading plans shall be prepared prior to any on-site grading for each individual project within the Specific Plan area, or group of projects.
- Prior to initial grading activities, a soils report and geotechnical study shall be completed that further analyzes on-site soil conditions and includes appropriate measures to be implemented to control erosion and dust.
- Prior to issuance of a grading permit, a General Construction Activity Storm Water Permit shall be obtained from the State Water Resources Control Board. A Storm Water Pollution Prevention Plan, which cites the water quality control measures to be implemented with project grading, shall be submitted concurrent with grading permit applications to the City Engineer.

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# CUT/FILL EXHIBIT



*The Colonies at San Antonio Specific Plan*

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N Not to Scale

Source: Associated Engineers, Inc.

Figure 6.1

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- All streets shall have a gradient not to exceed City of Upland engineering standards. Where cut and fill slopes are created higher than 10 feet in vertical height and exceeding a 5:1 slope, slope erosion control plans shall be submitted to the Community Development Department when grading plans are submitted for approval.
- The overall shape, height and grade of any cut and fill slope shall be developed in concert with the existing natural contours and scale of the natural terrain of a particular site. The graded form shall reflect the natural, rounded terrain, where possible. All cut and fill slopes shall be constructed at inclinations of no steeper than 2:1. Exceptions for steeper slopes can be included, if supported by technical analysis and approved by the City Engineer. The grading plan will reflect a contouring and landscaping program intended to control erosion.
- The toes and tops of all slopes higher than 10 feet in vertical height (except freeway slopes) shall be rounded with curves with radii designed in proportion to the total height of the slope, where drainage and stability permit such rounding.
- Potential brow ditches, terrace drains or other minor swales determined necessary at future stages of project review shall be lined with natural erosion control materials or concrete.
- The applicant shall be responsible for maintenance and upkeep of all planting and irrigation systems until those operations become the responsibility of other parties.
- MWD guidelines are to be followed for grading within the existing 200-foot-wide MWD right-of-way running northeast-southwest across the project site.
- Any additional cut and fill needed for Planning Area 22 will be balanced on site.

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## 7.0 DEVELOPMENT REGULATIONS

### 7.1 INTRODUCTION

The Colonies at San Antonio Specific Plan provides for development of a wide range of uses. The purpose of this Chapter is to ensure quality development by setting forth detailed development regulations. The Colonies at San Antonio Specific Plan establishes zoning districts and permitted uses for residential, commercial, community facility, and open space uses; sets forth development standards applicable to all buildings, structures, and uses within the Specific Plan area; and defines specific regulations for specific uses within the Specific Plan area. In addition, this Chapter establishes parking and signage regulations.

### 7.2 IDENTIFICATION OF COLONIES AT SAN ANTONIO ZONING CLASSIFICATIONS

Zoning classifications within the Colonies at San Antonio Specific Plan site are set forth in Table 7.A.

**Table 7.A - Colonies at San Antonio Zoning Classifications**

Zone	Intent and Purpose
<b><i>Residential Zones</i></b>	
“SFR-4.0” Single-Family Residential (4,000-square foot minimum lot size) “SFR-4.5” Single-Family Residential (4,500-square foot minimum lot size) “SFR-5.0” Single-Family Residential (5,000-square foot minimum lot size) “SFR-6.0” Single-Family Residential (6,000-square foot minimum lot size)	The purpose of these single family zoning classifications is to provide opportunities for a variety of quality housing products to meet a range of lifestyle and housing needs.
“RF” (Flexible Residential)	The purpose of this residential classification is to provide areas where attached or detached structures are available to allow opportunities for a variety of quality housing products to meet a range of lifestyle and housing needs.
“MU” Mixed Use	The purpose of this mixed-use designation is to provide a high quality, higher density housing product incorporating neighborhood commercial and restaurant uses to appeal to those who prefer this style of living.
<b><i>Commercial Zones</i></b>	
“CH” Highway Commercial	The purpose of this commercial designation is to serve regional and freeway users, as well as local residents by providing for large-scale commercial, retail, and entertainment needs.
“NC” Neighborhood Commercial	The purpose of this commercial designation is to serve local and neighborhood retail and service needs by providing for uses that are conveniently located and easy to access.

**Table 7.A - Colonies at San Antonio Zoning Classifications (Continued)**

Zone	Intent and Purpose
<i>Other Zoning Classifications</i>	
“CF” Community Facility	The purpose of this designation is to provide for a site for a fire station, to be built and maintained by the City of Upland Fire Department to serve the Colonies at San Antonio and the surrounding community.
“OS” Open Space	The purpose of this classification is to preserve a multi-use open space area to serve as a visual amenity, a recreational area for passive use via a trail system, and a drainage system that handles storm flows in a manner that supports restored wetlands and wildlife habitat.

### 7.3 PERMITTED USES

The purpose of this section is to define the uses that are allowable within each of the zoning districts within the Specific Plan area. These permitted uses are set forth in Table 7.B. Each use type set forth in Table 7.B is designated as being permitted, conditionally permitted or prohibited within each of the Colonies at San Antonio zoning classifications as follows.

- **P Permitted Use P(N).** Use is permitted only north of the I-210 Freeway A Permitted only as an ancillary use to an otherwise permitted use A(N) Permitted only as an ancillary use to an otherwise permitted use north of the I-210 Freeway A/CUP Permitted only as an ancillary use to an otherwise permitted use, subject to the approval of a Conditional Use Permit (CUP) in compliance with the Upland Municipal Code, Chapter 17.16.020
- **P/CUP.** Permitted if use is determined by the Community Development Director to be most similar to a permitted use within the zone such use is proposed; permitted subject to the approval of a Conditional Use Permit (CUP) in compliance with the Upland Municipal Code, Chapter 17.16.020 if use is determined by the Community Development Director to be most similar to a conditionally permitted use within the zone such use is proposed
- **CUP.** Permitted subject to the approval of a Conditional Use Permit (CUP) in compliance with the Upland Municipal Code, Chapter 17.16.020
- **CUP(N).** Permitted north of the I-210 Freeway only subject to the approval of a Conditional Use Permit (CUP) in compliance with the Upland Municipal Code, Chapter 17.16.020
- **CUP(S).** Permitted south of the I-210 Freeway only subject to the approval of a Conditional Use Permit (CUP) in compliance with the Upland Municipal Code, Chapter 17.16.020
- **SP EV.** Permitted subject to the approval of a Special Events Permit in compliance with the Upland Municipal Code, including Section 17.16.160. LEP Permitted subject to the approval of a Live Entertainment Permit in compliance with the Upland Municipal Code, Public Peace and Welfare, Chapter 9.28.020.
- **LEP.** Permitted subject to the approval of a Live Entertainment permit in compliance with the Upland Municipal Code, Public Peace and Welfare, Chapter 9.28.020.
- **x.** Prohibited use (not permitted).

**Table 7.B - Permitted Uses**

Land Use Types	Use List	Residential						Commercial			Other	
		SFR 4.0	SFR 4.5	SFR 5.0	SFR 6.0	RF	MU	CH	NC	CF	OS	SS
<b>GENERAL COMMERCIAL USES</b>												
<i>Administrative and Professional Offices.</i> Activities typically include, but are not limited to, executive management, administrative, or clerical uses of private firms and public utilities. Additional activities typically include the provision of advice, design, information or consultation of a professional nature.	Administrative, business, and professional offices	x	x	x	x	x	P	P	P	x	x	x
	Data storage, financial records, auditing centers	x	x	x	x	x	x	A	x	x	x	x
	Medical, dental, and chiropractic offices	x	x	x	x	x	P	P	P	x	x	x
	Other administrative and professional office uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
<i>Adult Businesses.</i> Establishments as defined in Chapter 9490.1 (Adult Businesses) of the City of Upland Municipal Code.		x	x	x	x	x	x	x	x	x	x	x
<i>Alcohol Sales, On Sale.</i> Activities typically include the sale, subject to required license, of alcoholic beverages for consumption on the premises. See development standards in Section 7.8.1.	Bars, taverns cocktail lounges	x	x	x	x	x	CUP	CUP	CUP	x	x	x
	Restaurants serving alcohol as part of their menu	x	x	x	x	x	P	P	P	x	x	x
	Other on-sale alcohol uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than the on-sale alcohol uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
<i>Alcohol Sales, Off Sale.</i> Activities typically include the sale, subject to required license, of alcoholic beverages for consumption off the premises. See development standards in Section 7.8.1.	Drug stores	x	x	x	x	x	P	P	P	x	x	x
	General merchandise	x	x	x	x	x	x	P	P	x	x	x
	Convenience markets (less than fifteen thousand square feet)	x	x	x	x	x	P/CUP	P	P	x	x	x
	Grocery stores	x	x	x	x	x	X	P	P	x	x	x
	Liquor stores	x	x	x	x	x	X	P	P	x	x	x
	Wine merchants	x	x	x	x	x	P	P	P	x	x	x
	Other off-sale alcohol uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than the off-sale alcohol uses listed as permitted or conditionally permitted.	x	x	x	x	x	X	P/CUP	P/CUP	x	x	x
<i>Animal Care Facilities.</i> Activities typically include, but are not limited to, the care and treatment of domesticated, non-farm animals. Not included in this land use are breeding facilities and long-term kennels. See development standards in Section 7.8.3.	Animal grooming	x	x	x	x	x	X	P	P	x	x	x
	Animal hospitals	x	x	x	x	x	X	P	P	x	x	x
	Pet clinics	x	x	x	x	x	X	P	P	x	x	x
	Pet stores	x	x	x	x	x	x	P	P	x	x	x
	Veterinary services	x	x	x	x	x	x	P	P	x	x	x
	Kennels (Indoor Facilities)	x	x	x	x	x	x	P	P	x	x	x
	Kennels (Outdoor Facilities)	x	x	x	x	x	x	CUP	CUP	x	x	x
	Other animal care facility uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
<i>Automotive Uses:</i> Activities typically include, but are not limited to the, sales and servicing of motor vehicles, recreational vehicles, boats, and trailers.	Car washes, automated and self-service; automobile detailing (see development standards in Section 7.8.4)	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
	Parts stores (no installation or repair)	x	x	x	x	x	x	P	P	x	x	x
	Parts stores (with installation or repair)	x	x	x	x	x	x	P(N), CUP(S)	CUP	x	x	x
	Rental agencies (without ancillary car wash and maintenance facilities)	x	x	x	x	x	x	P	CUP	x	x	x
	Rental agencies (without ancillary car wash and maintenance facilities)	x	x	x	x	x	x	P(N), CUP(S)	x	x	x	x
	Repair services - Minor (see development standards in Section 7.8.6)	x	x	x	x	x	x	P(N), CUP(S)	CUP	x	x	x



**Table 7.B - Permitted Uses (Continued)**

Land Use Types	Use List	Residential						Commercial			Other	
		SFR 4.0	SFR 4.5	SFR 5.0	SFR 6.0	RF	MU	CH	NC	CF	OS	SS
<i>Automotive Uses (Continued):</i>	Repair services - Major, including body work, painting, automobile customizing, towing facilities (see development standards in Section 7.8.6)	x	x	x	x	x	x	P(N)	x	x	x	x
	Service stations, with or without mini-mart (see development standards in Section 7.8.5)	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
	Vehicle sales, including automobiles, boats, RVs, and trailers (see development standards in Sections 7.8.4 and 7.8.7)	x	x		x	x	x	P(N)	x	x	x	x
	Other automotive uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
<i>Banks and Financial Services.</i>	Banks and credit unions	x	x	x	x	x	P	P	P	x	x	x
	Check cashing	x	x	x	x	x	x	P	P	x	x	x
	Home mortgage	x	x	x	x	x	P	P	P	x	x	x
	Other financial services	x	x	x	x	x	P	P	P	x	x	x
	Other banks and financial service uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
<i>Business Support Services.</i> Activities typically include, but are not limited to, services and goods generally provided to support other businesses.	Building maintenance services	x	x	x	x	x	x	P(N)	x	x	x	x
	Business machines sales and service	x	x	x	x	x	x	P	P	x	x	x
	Employment agencies	x	x	x	x	x	x	P	P	x	x	x
	Office supply stores	x	x	x	x	x	x	P	P	x	x	x
	Printing and photocopy shops	x	x	x	x	x	P	P	P	x	x	x
	Stationery stores	x	x	x	x	x	P	P	P	x	x	x
	Other business support service uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
<i>Drive-through Sales and Service.</i> Activity typically include, but are not limited to, kiosks and service windows of businesses designed to enable customers to drop off goods or receive goods or services while remaining in their automobiles. See development standards in Section 7.8.10.	Convenience drop-off	x	x	x	x	x	P	P	P	x	x	x
	Kiosks and ATMs (see development standards in Section 7.8.15)	x	x	x	x	x	P	P	P	x	x	x
	Remote tellers and ATMs at permitted financial institutions	x	x	x	x	x	P	P	P	x	x	x
	Food services	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
	Other drive-through sales and service uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
<i>Eating and Drinking Establishments.</i> Activities typically include, but are not limited to, the retail sale from the premises of food or beverages prepared for on-premises or off-premises consumption. See development standards in Section 7.8.11.	Brew pubs	x	x	x	x	x	CUP	P	P	x	x	x
	Cafés, including sidewalk seating	x	x	x	x	x	P	P	P	x	x	x
	Coffee shops, coffee houses, tea rooms	x	x	x	x	x	P	P	P	x	x	x
	Delicatessens, sandwich shops	x	x	x	x	x	P	P	P	x	x	x
	Donut shops	x	x	x	x	x	P	P	P	x	x	x
	Fast food	x	x	x	x	x	x	P	P	x	x	x
	Night clubs	x	x	x	x	x	CUP <sup>4</sup>	CUP	CUP	x	x	x
	Restaurants	x	x	x	x	x	P	P	P	x	x	x
Other eating and drinking establishment uses not specifically listed that the Community Development Director determines are similar to, and no more	x	x	x	x	x	P	P	P	x	x	x	



**Table 7.B - Permitted Uses (Continued)**

Land Use Types	Use List	Residential						Commercial			Other	
		SFR 4.0	SFR 4.5	SFR 5.0	SFR 6.0	RF	MU	CH	NC	CF	OS	SS
	objectionable than uses listed as permitted.											
<i>Food and Beverage Sales.</i> Activities typically include, but are not limited to retail sale from the premises of food and beverages for off-premises final preparation and consumption.	Bakeries	x	x	x	x	x	P	P	P	x	x	x
	Candy stores, including fountain	x	x	x	x	x	P	P	P	x	x	x
	Convenience markets	x	x	x	x	x	P	P	P	x	x	x
	Dairy products stores	x	x	x	x	x	x	P	P	x	x	x
	Egg and poultry stores (no slaughtering, plucking or dressing)	x	x	x	x	x	x	P	P	x	x	x
	Ice cream and frozen yogurt shops	x	x	x	x	x	P	P	P	x	x	x
	Meat, fish markets	x	x	x	x	x	P/CUP	P	P	x	x	x
	Produce markets	x	x	x	x	x	P	P	P	x	x	x
	Supermarkets	x	x	x	x	x	P	P	P	x	x	x
	Other food and beverage sale uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
<i>General Merchandise.</i> Activities typically include, but are not limited to, the retail sales from premises, including incidental rental and repair services.	Apparel/clothing stores	x	x	x	x	x	P	P	P	x	x	x
	Antique shops	x	x	x	x	x	x	P	P	x	x	x
	Art and art supply stores	x	x	x	x	x	P	P	P	x	x	x
	Art galleries	x	x	x	x	x	P	P	P	x	x	x
	Bicycle stores	x	x	x	x	x	x	P	P	x	x	x
	Bookstores	x	x	x	x	x	P	P	P	x	x	x
	Card shops	x	x	x	x	x	P	P	P	x	x	x
	Department stores	x	x	x	x	x	x	P	x	x	x	x
	Discount stores	x	x	x	x	x	x	P	x	x	x	x
	Dry good or notions stores	x	x	x	x	x	x	P	P	x	x	x
	Electrical appliance stores	x	x	x	x	x	x	P	P	x	x	x
	Electronics, computer, television, stereo stores	x	x	x	x	x	x	P	P	x	x	x
	Fabric stores	x	x	x	x	x	P	P	P	x	x	x
	Firearms sales and service (other than within sporting goods or general merchandise store)	x	x	x	x	x	x	CUP	x	x	x	x
	Florist shops	x	x	x	x	x	P	P	P	x	x	x
	Frozen food lockers (retail only)	x	x	x	x	x	x	P	P	x	x	x
	Furniture stores	x	x	x	x	x	x	P	P	x	x	x
	Gift shops	x	x	x	x	x	P	P	P	x	x	x
	Hobby and craft stores	x	x	x	x	x	P	P	P	x	x	x
	Household appliance stores	x	x	x	x	x	x	P	P	x	x	x
	Interior design	x	x	x	x	x	P	P	P	x	x	x
	Jewelry stores	x	x	x	x	x	P	P	P	x	x	x
	Linens and domestics	x	x	x	x	x	P	P	P	x	x	x
	Luggage and leather goods stores	x	x	x	x	x	P	P	P	x	x	x
Music and video stores	x	x	x	x	x	P	P	P	x	x	x	
Musical instrument sales and service	x	x	x	x	x	P	P	P	x	x	x	



**Table 7.B - Permitted Uses (Continued)**

Land Use Types	Use List	Residential						Commercial			Other	
		SFR 4.0	SFR 4.5	SFR 5.0	SFR 6.0	RF	MU	CH	NC	CF	OS	SS
	Newsstands	x	x	x	x	x	P	P	P	x	x	x
<i>General Merchandise (Continued).</i>	Pharmacies and drug stores	x	x	x	x	x	P	P	P	x	x	x
	Photography and camera shops	x	x	x	x	x	P	P	P	x	x	x
	Sewing machine sales and service shops	x	x	x	x	x	P	P	P	x	x	x
	Shoe stores	x	x	x	x	x	x	P	P	x	x	x
	Sign stores	x	x	x	x	x	x	P	P	x	x	x
	Spa and pool table stores	x	x	x	x	x	x	P	P	x	x	x
	Sporting goods stores	x	x	x	x	x	x	P	P	x	x	x
	Stamp and coin stores	x	x	x	x	x	P	P	P	x	x	x
	Swimming pool supplies stores	x	x	x	x	x	x	P	P	x	x	x
	Telephone stores	x	x	x	x	x	P	P	P	x	x	x
	Toy stores	x	x	x	x	x	P	P	P	x	x	x
	Vacuum cleaner sales and service	x	x	x	x	x	x	P	P	x	x	x
	Variety stores	x	x	x	x	x	x	P	P	x	x	x
	Other general merchandise uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
<i>Health Clubs and Spas.</i> Activities typically include, but are not limited to, sport and health-related activities performed either indoors or outdoors. See development standards in Section 7.8.12.	Day spas	x	x	x	x	x	P	P	P	x	x	x
	Health and fitness centers	x	x	x	x	x	P	P	P/CUP	x	x	x
	Other health club and spa uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	P/CUP	P/CUP	P/CUP	x	x	x
<i>Home Improvements:</i> Activities typically include, but are not limited to, the retail sales or rental from premises of household goods and building materials.	Building materials and supplies	x	x	x	x	x	x	P	A	x	x	x
	Carpet stores	x	x	x	x	x	x	P	P	x	x	x
	Drapery and blinds stores	x	x	x	x	x	x	P	P	x	x	x
	Electrical supplies	x	x	x	x	x	x	P	P	x	x	x
	Hardware stores	x	x	x	x	x	x	P	P	x	x	x
	Home decor stores and decorating services	x	x	x	x	x	x	P	P	x	x	x
	Home improvement centers and stores	x	x	x	x	x	x	P	P/CUP	x	x	x
	Household and yard tools and equipment rentals	x	x	x	x	x	x	P	x	x	x	x
	Lighting stores	x	x	x	x	x	x	P	P	x	x	x
	Linen and domestics stores	x	x	x	x	x	x	P	P	x	x	x
	Lumber yards	x	x	x	x	x	x	P	P	x	x	x
	Paint and wallpaper stores	x	x	x	x	x	x	P	P	x	x	x
	Plant nurseries (fertilizer stored within bags in an enclosed building)	x	x	x	x	x	x	P	P	x	x	x
	Plant nurseries - wholesale	x	x	x	x	x	x	P(N)	x	x	x	x
	Plumbing supplies	x	x	x	x	x	x	P(N)	x	x	x	x
	Swimming pool supplies	x	x	x	x	x	x	P	P	x	x	x
Tool and equipment sales and services	x	x	x	x	x	x	P	P/CUP	x	x	x	
Upholstery shops	x	x	x	x	x	x	P(N)	x	x	x	x	



**Table 7.B - Permitted Uses (Continued)**

Land Use Types	Use List	Residential						Commercial			Other	
		SFR 4.0	SFR 4.5	SFR 5.0	SFR 6.0	RF	MU	CH	NC	CF	OS	SS
<i>Home Improvement (Continued)s:</i>	Warehouse club stores	x	x	x	x	x	x	P	CUP	x	x	x
	Yard and garden supplies, sales	x	x	x	x	x	x	P	x	x	x	x
	Yard and garden supplies, service	x	x	x	x	x	x	P	x	x	x	x
	Other home improvement uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
<i>Hotel and Motels, Visitor Services Establishments.</i> Activities typically include, but are not limited to, the provision of overnight accommodations and related ` and conference facilities.	Catering establishments	x	x	x	x	x	x	P	P	x	x	x
	Banquet facilities	x	x	x	x	x	CUP	C/CUP	P/CUP	x	x	x
	Conference facilities	x	x	x	x	x	CUP	P(N)	x	x	x	x
	Hotels, motels (see development standards in Section 7.8.13)	x	x	x	x	x	CUP <sup>4</sup>	CUP	x	x	x	x
	Wedding chapels	x	x	x	x	x	CUP <sup>4</sup>	P/CUP	x	x	x	x
	Other hotels, motels, and visitor service establishment uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
<i>Medical Support.</i> Activities typically include the provision of diagnostic, therapeutic and emergency transport services, along with the sale and rental of medical appliances and equipment. See development standards in Section 7.8.17.	Ambulance and medical transport services	x	x	x	x	x	x	P(N)	x	x	x	x
	Biological, medical, dental laboratories	x	x	x	x	x	x	P	x	x	x	x
	Clinics	x	x	x	x	x	x	P	P	x	x	x
	Medical appliances and equipment sales and rental	x	x	x	x	x	x	P	P	x	x	x
	Physical therapy centers	x	x	x	x	x	x	P	P	x	x	x
	Weight loss services	x	x	x	x	x	x	P	P	x	x	x
	Other medical support uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
<i>Personal Services and Sales.</i> Activities typically include, but are not limited to retail sales of personal convenience items and professional services that are used frequently by the community.	Art framing	x	x	x	x	x	P	P	P	x	x	x
	Barber and beauty shops, hair stylists, nail salons	x	x	x	x	x	P	P	P	x	x	x
	Check cashing	x	x	x	x	x	x	P	P	x	x	x
	Child and adult day care (see development standards in Section 7.8.9)	x	x	x	x	x	P/CUP	P	P	x	x	x
	Children’s indoor play centers	x	x	x	x	x	P/CUP	P	P	x	x	x
	Dressmaking, tailors	x	x	x	x	x	x	P	P	x	x	x
	Dry cleaners	x	x	x	x	x	P	P	P	x	x	x
	Formal wear rental, sales, and tailoring	x	x	x	x	x	P	P	P	x	x	x
	Interior decorating	x	x	x	x	x	P	P	P	x	x	x
	Laundromats, self-service laundries	x	x	x	x	x	P	P	P	x	x	x
	Locksmiths	x	x	x	x	x	x	P	P	x	x	x
	Mortuaries, funeral parlors	x	x	x	x	x	x	P(N) P/CUP(S)	x	x	x	x
	Photographic studios	x	x	x	x	x	x	P	P	x	x	x
	Photographic processing	x	x	x	x	x	x	P	P	x	x	x
	Police Storefront	x	x	x	x	x	x	P	P	P	x	x



**Table 7.B - Permitted Uses (Continued)**

Land Use Types	Use List	Residential						Commercial			Other	
		SFR 4.0	SFR 4.5	SFR 5.0	SFR 6.0	RF	MU	CH	NC	CF	OS	SS
	Postal services	x	x	x	x	x	P	P	P	x	x	x
<i>Personal Services and Sales (Continued).</i>	Real estate offices	x	x	x	x	x	P	P	P	x	x	x
	Shoe repair	x	x	x	x	x	P	P	P	x	x	x
	Tanning salons	x	x	x	x	x	P	P	P	x	x	x
	Telephone stores	x	x	x	x	x	P	P	P	x	x	x
	Travel agencies	x	x	x	x	x	P	P	P	x	x	x
	Video rental stores	x	x	x	x	x	P	P	P	x	x	x
	Weight loss centers	x	x	x	x	x	x	P	P	x	x	x
	Other personal service and sale uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
<i>Personal Instruction.</i> Activities typically include instruction in artistic, academic, athletic or recreational pursuits within an enclosed structure.	Athletic instruction, personal training	x	x	x	x	x	x	P	P	x	x	x
	Dance studios	x	x	x	x	x	x	P	P	x	x	x
	Music schools	x	x	x	x	x	x	P	P	x	x	x
	Self-defense schools	x	x	x	x	x	x	P	P	x	x	x
	Tutoring centers	x	x	x	x	x	x	P	P	x	x	P
	Vocational and trade schools	x	x	x	x	x	x	P	P/CUP <sup>4</sup>	P	x	P/CUP
	Private and Charter Schools	x	x	x	x	P/CUP P	P/CUP	P/CUP <sup>5</sup>	x	x	x	x
	Other personal instruction uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
<i>Senior Housing and Care Continuum.</i> Activities typically include, but are not limited to senior housing and supportive services such as meals, housekeeping, social activities, and transportation, depending on level of need, allowing residents to remain living in the community as their care needs change. The intent is to provide housing and services for seniors, ranging from adult day care to assisted living facilities.	Adult Day Care. Daily structured programs in a community setting with activities and health-related and rehabilitation services to elderly who are physically or emotionally disabled and need a protective environment. This care is provided for during the day, the individual returning home for the evening.	x	x	x	x	P/CUP P	P/CUP	P	x	x	x	x
	Alternate Care Facility. A licensed residence other than a skilled nursing facility where care services are delivered, such as hospice, assisted care or Alzheimer's facilities.	x	x	x	x	P	P	P/CUP <sup>6</sup>	x	x	x	x
	Assisted Living Facilities. In general, a state-licensed program with services that include meals, laundry, housekeeping, medication reminders, and assistance with Activities of Daily Living (ADLs) and Instrumental Activities of Daily Living (IADLs).	x	x	x	x	P	P/CUP	P/CUP <sup>6</sup>	x	x	x	x
	Other senior residential/care uses not specifically listed that are similar to, and no more objectionable than the residential uses listed as permitted or conditionally permitted.	x	x	x	x	P	P/CUP	P/CUP <sup>6</sup>	x	x	x	x
<i>Temporary/Seasonal Outdoor Sales.</i> This use consists of the <i>outdoor</i> retail sale of materials on a temporary and/or seasonal basis and/or <i>outdoor</i> performances. These uses are subject to the issuance of a Special Events Permit. See development standards in Section 7.8.23.		x	x	x	x	x	x	SP EV	SP EV	x	x	x
<b>ENTERTAINMENT/RECREATIONAL USES</b>												
<i>Amusement Centers/Arcades.</i> Any structure (or portion thereof) in which four or more amusement devices (either coin- or card-operated) are installed, such as photography machines, video games, muscle testers, fortune telling machines, laser tag, "midway" style		x	x	x	x	x	x	P	P	x	x	x



Table 7.B - Permitted Uses (Continued)

Land Use Types	Use List	Residential						Commercial			Other	
		SFR 4.0	SFR 4.5	SFR 5.0	SFR 6.0	RF	MU	CH	NC	CF	OS	SS
games, rides or similar uses, and other games of skill or science, but not including games of chance or other similar devices. Included is any place open to the public, whether or not the primary use of the premises is devoted to operation of such devices. Sales of prepared foods and beverages is also included as ancillary to the primary use. Establishments defined in Chapter 9490.1 (Adult Businesses) of the City of Upland Municipal Code are not included. See development standards in Section 7.8.2.												
<i>Indoor Recreational Facilities.</i> See development standards in Section 7.8.14.	Batting cages	x	x	x	x	x	x	P (N)	x	x	x	x
	Billiard and pool halls	x	x	x	x	x	x	P	x	x	x	x
	Bowling alleys	x	x	x	x	x	x	P	x	x	x	x
	Dance halls	x	x	x	x	x	CUP <sup>4</sup>	CUP	x	x	x	x
	Ice and roller rinks	x	x	x	x	x	x	P	x	x	x	x
	Indoor skate parks	x	x	x	x	x	x	P	x	x	x	x
	Racquet clubs	x	x	x	x	x	P	P	x	x	x	x
	Tennis clubs	x	x	x	x	x	P	P	x	x	x	x
Other indoor recreational facility uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x	
<i>Live Entertainment.</i> Activities consist of ancillary uses typically including, but not limited to, live music; amplified vocal or instrumental sounds; recorded music played by a disc jockey; karaoke; and song, dance, or comedic acts (excluding adult business uses), which occur more than three times per calendar year within an enclosed building. See development standards in Section 7.8.16.		x	x	x	x	x	CUP	LEP	LEP	x	x	x
<i>Outdoor Recreational Facilities.</i> See development standards in Section 7.8.20.	Miniature golf	x	x	x	x	x	x	CUP	x	x	x	x
	Batting cages	x	x	x	x	x	x	CUP	x	x	x	x
	Go-cart tracks	x	x	x	x	x	x	CUP	x	x	x	x
	Water parks	x	x	x	x	x	x	CUP	x	x	x	x
	Other outdoor recreational facility uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
<i>Theaters.</i> Includes structures where the primary use is devoted to the exhibition of live or prerecorded theatrical, musical, comedic or other performances. (See Live Entertainment if use is an ancillary, rather than primary, use.) Uses defined in Chapter 9490.1 (Adult Businesses) of the City of Upland Municipal Code are not included. Sale of prepared foods and beverages is permitted ancillary to the primary use. See development standards in Section 7.8.24.	Movie theaters	x	x	x	x	x	x	P	x	x	x	x
	Concert, theatrical, comedy venues	x	x	x	x	x	x	P/CUP	x	x	x	x
	Other theater uses not specifically listed that the Community Development Director determines are similar to, and no more objectionable than uses listed as permitted or conditionally permitted.	x	x	x	x	x	x	P/CUP	P/CUP	x	x	x
<b>OTHER USES</b>												
<i>Communication Facilities.</i> Activities typically include, but are not limited to, broadcasting and other information relay services for commercial and personal uses. Typical uses include but are not limited to monopoles, antennas, and rooftop dishes. Rooftop displays of satellite dishes for sale or rent are not permitted. See development standards in Section 7.8.8.		x	x	x	x	x	x	P	P	A	x	A
<i>Civic Administration.</i> Activities typically include, but are not limited to, management, administration, clerical, and other services performed by public and quasi-public agencies.		x	x	x	x	x	x	P	P	P	x	P
<i>Cultural Facilities.</i> Activities typically include, but are not limited to, those performed by public and private museums and art galleries, public and private libraries and observatories.		x	x	x	x	x	x	P	x	P	x	P
<i>Religious Assembly.</i> Activities typically include religious services and assembly such as		x	x	x	x	x	x	P(N)	x	P/CU	x	P/CUP



**Table 7.B - Permitted Uses (Continued)**

Land Use Types	Use List	Residential						Commercial			Other	
		SFR 4.0	SFR 4.5	SFR 5.0	SFR 6.0	RF	MU	CH	NC	CF	OS	SS
customarily occurs in churches, synagogues, and temples. See development standards in Section 7.8.21.										P		
<i>Schools, public and private.</i> Typical activities include educational facilities for K-8 students provided by public agencies or private institutions.		x	x	x	x	x	x	x	x	x	x	P
<i>Utility/Roadway Facilities.</i> Equipment, facilities, or features required for the operation or maintenance of public utilities, roadways, or conveyance of flood waters. Includes parking areas, bus stops, and taxi stands. See development standards in Section 7.8.25.		x	x	x	x	x	x	P	P	P	P	P
<b>RESIDENTIAL USES</b>												
<i>Single Family Residential</i>	Detached single family residential dwelling units with public streets	P	P	P	P	P	x	x	x	x	x	P
	Detached single family residential dwelling units with gated private streets											
<i>Multiple Family Residential</i>		x	x	x	x	P	P	x	x	x	x	x
<i>Residential Accessory Uses and Structures.</i>		P	P	P	P	P	P	x	x	x	x	x
<i>Home Occupations.</i> <sup>2</sup>		P	P	P	P	P	P	x	x	x	x	x
<i>Residential Care Homes.</i>		x	x	x	x	x	P	x	x	x	x	x
<i>Other residential uses not specifically listed that the Community Development Department Director determines are similar to, and no more objectionable than the residential uses listed as permitted or conditionally permitted.</i>		P/ CUP	P/ CUP	P/ CUP	P/ CUP	P/ CUP	P/CUP	x	x	x	x	x

**Notes:**

- 2 In compliance with Chapter 17 of the Upland Municipal Code.
- 3 In compliance with the Upland Municipal Code and state requirements.
- 4 CUP requires parking analysis.
- 5 Permitted with parking analysis north of Interstate 210; CUP and parking analysis required south of Interstate 210.
- 6 CUP allowed in PA 20 and 21 only.



## 7.4 PROHIBITED USES

In addition to the uses identified as being prohibited in Table 7.B, the following uses are prohibited in commercial zones within the Colonies at San Antonio Specific Plan.

- Mobile home parks.
- Residential uses within the CH, NC, OC and CF zones.
- Any other use not listed as permitted or determined to be a similar use to an otherwise permitted use in compliance with the provisions of this Specific Plan.

In addition to the uses identified as being prohibited in Table 7.B, the following uses are prohibited in residential zones within the Colonies at San Antonio Specific Plan.

- Mobile home parks.
- Commercial uses.
- Industrial uses.
- Sales stands offering products for sale.
- The keeping of horses, colts, mules, ponies, goats, sheep, cows, calves, or animals of general like character.
- The keeping of poultry, rabbits, three or more dogs, or three or more cats at least four months of age.
- The keeping of wild, exotic or non-domestic animals without first applying for and receiving a “wild animal” permit, as provided in section 4130.2 of Article IV, Chapter 1, Part 3, “Permit for Keeping Wild or Vicious Animals or Reptiles–In General.”

## 7.5 GENERAL DEVELOPMENT STANDARDS

### 7.5.1 General Standards

Structures shall not be erected or located on a lot unless the building or enlargement conforms with the provisions outlined in Table 7.C - “General Development Standards.”

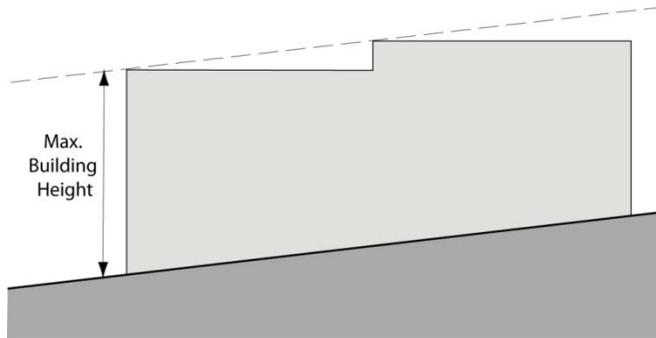
### 7.5.2 Measuring Maximum Height

**a. General.** The building envelope to be used for determining the height of all structures, except those in the MU zone, shall be measured as the vertical distance in a horizontal plane from the finished grade to the top of the structure, exclusive of architectural elements (i.e., towers) in commercial zones, chimneys, and ventilators. The maximum height established in Table 7.C shall not be exceeded anywhere along the horizontal plane that runs perpendicular to the finished grade.

**b. MU Zone.** Since the MU zone is the only developed area within the Specific Plan were there is a distinct grade differential, occurring as the land slopes from 19<sup>th</sup> Street down to the central open space, the measurement of height in the MU zone requires a method that accommodates this grade change and its effect on the architectural design of residential units in this area. Therefore, the maximum height of structures within the MU zone, as established in Table 7.C, shall be measured as follows, according to the direction of slope of the structure:

**Building height measurement on sloped sites:**

The building height (measured from grade), including all architectural features, shall not project above a plane parallel to the slope of the site.



**Table 7.C - General Development Standards**

Zone	Use	Min. Lot Area (square feet)	Minimum Dimensions		Maximum Height		Max. Lot Coverage	Parking Space Requirements <sup>1</sup>
			Width (feet)	Depth (feet)	Stories	Feet		
CH	Highway Commercial	NR (No Requirement)	NR	NR	3 NR for office or hotel	552, <sup>5</sup> 145 for office or hotel	NR	Parking within commercial sites shall be provided at a rate of 4.0 spaces per 1,000 square feet of adjusted gross floor area. Determination of the required number of parking spaces shall be made on a Planning Area-wide basis. See also Table 7.E for required parking adjustments.
NC	Neighborhood Commercial	NR	NR	NR	2	50 <sup>2</sup>	NR	Parking within commercial sites shall be provided at a rate of 4.0 spaces per 1,000 square feet of adjusted gross floor area. <sup>7</sup> Determination of the required number of parking spaces shall be made on a Planning Area-wide basis. See also Table 7.E for required parking adjustments.
CF	Community Facility	NR	NR	NR	NR	NR	NR	NR
OS	Open Space	NR	NR	NR	NR	NR	NR	NR

**Table 7.C - General Development Standards (Continued)**

Zone	Use	Min. Lot Area (square feet)	Minimum Dimensions		Maximum Height		Max. Lot Coverage	Parking Space Requirements <sup>1</sup>
			Width (feet)	Depth (feet)	Stories	Feet		
MU	Mixed Use	NR	NR	NR	8 plus parking	- 85 <sup>8</sup>	NR	One bedroom units (850 sf min): 1.25 off-street parking spaces per unit of which 1 space shall be in a garage. Two bedroom units (1,000 sf min): 1.75 off-street parking spaces per unit of which 1 space shall be in a garage. Three (1,200 sf min) or more bedroom units: 2 off-street parking spaces per unit of which 1 space shall be in a garage. In addition to the required number of parking spaces for each unit, 1 off-street uncovered parking space shall be provided for each four units for visitor parking. Commercial: 4 spaces per every 1,000 square feet of adjusted gross floor area. <sup>6</sup>
SFR 4.0	Single Family Residential	4,000	45	90	NR	35	55%	2 parking spaces within an enclosed garage per unit.
SFR 4.5	Single Family Residential	4,500	50	90	NR	35	50%	2 parking spaces within an enclosed garage per unit.
SFR 5.0	Single Family Residential	5,000	55	91	NR	35	45% <sup>4</sup>	2 parking spaces within an enclosed garage per unit.
SFR 6.0	Single Family Residential	6,000	60	100	NR	35	45% <sup>4</sup>	2 parking spaces within an enclosed garage per unit.
RF	Residential	NR	NR	NR	3 plus parking	45	NR	2 parking spaces, 1 within an enclosed garage per unit. <sup>6</sup>

- Notes:**
- 1 See Section 7.6 for requirements regarding location of parking, calculating adjusted gross floor area (AGFA), adjustments to required parking ratios, general standards, and loading provisions.
  - 2 The maximum height of buildings, canopies, and other structures (excepting signs) for gas stations shall be 35 feet.
  - 4 The maximum lot coverage for a single story dwelling shall be 50 percent.
  - 5 For theaters, the maximum height shall be 65 feet for the front elevation and 85 feet for the rear elevation to accommodate a change in grade. Theater marquees shall have a maximum height of 105 feet.
  - 6 A senior care project could have different requirements, subject to a parking analysis and as reviewed under the Conditional Use Permit Process
  - 7 Table 7.E contains Commercial Parking adjustments.
  - 8 Building height shall include all architectural features.

### 7.5.3 Required Setback Areas

Required setback areas shall be open and unobstructed from the ground to the sky. Electrical transformers, utility meters; street furniture; landscaping; and telephone, cable television, gas and water appurtenances shall be exempt from this provision. In addition, ground-mounted signs may be approved within required building and parking setback areas. Table 7.D specifies required setbacks for each zoning

district, except MU, and the setbacks required when intrusions are permitted. The Mixed Use zoning district setback requirements contain more detail, given the unique location of the Mixed Use zone. Therefore, they are included as a separate section, following the table.

a. **Measuring Setback Area.** Setbacks for all parcels shall be measured from property lines.

**Table 7.D - Setbacks and Permitted Intrusions**

<b>Zone</b>	<b>Building and Parking Setbacks (Minimum)</b>	<b>Bldg.</b>	<b>Pkg.</b>	<b>Setbacks for Architectural and Other Features (Minimum)</b>
CH <sup>9</sup>	Interstate 210	20'	10'	Electrical transformers; utility meters; ground-mounted signs; street furniture; and telephone, cable television, gas, and water appurtenances shall be permitted to intrude into required setbacks as necessary.
	Property Lines abutting Cucamonga Creek Channel	25'	25'	
	Public Streets (for buildings)	25'	15'	
	Public Streets (for parking and display of vehicles for sale)	15'	15'	
	Private streets and drives	15'	10'	
	Interior Property Lines	NR	NR	
	Special Setbacks for gas stations:			
	Any structure or building	20'		
	Pump islands	20'		
	Pump island canopies	10'		
Fuel storage fill pipes	15'			
NC	Interstate 210	20'	10'	Electrical transformers; utility meters; ground-mounted signs; street furniture; and telephone, cable television, gas, and water appurtenances shall be permitted to intrude into required setbacks as necessary.
	Public streets (for buildings)	25'	15'	
	Public streets (for parking and display of vehicles for sale)	15'	10'	
	Private streets and drives	15'	10'	
	Interior property lines other than adjacent to residential planning areas	NR	NR	
	Interior property lines adjacent to residential planning areas	20'	20'	
	Property lines adjacent to Open Space	20'	15'	
	Special setbacks for gas stations:			
	Any structure or building	20'		
	Pump islands	20'		
	Pump island canopies	10'		
	Fuel storage fill pipes	15'		
CF	Required setbacks shall be determined through Administrative Committee Review			
OS	No setbacks are required within areas zoned OS.			
MU <sup>9, 10</sup>	See Section 7.5.3.b for Mixed Use Regulations			
SFR 4.0	FRONT			ARCHITECTURAL FEATURES <sup>1</sup>
	One Story Porch	12'		Front – 10' minimum
	Swing-in garage	10'		Side <sup>2</sup> – 3'4" minimum
	One or two story home element	12'		Rear – 12' minimum
	Front-in garage	18'		FIRST STORY BAY WINDOWS (at grade with a maximum width of 12')

**Table 7.D - Setbacks and Permitted Intrusions (Continued)**

<b>Zone</b>	<b>Building and Parking Setbacks (Minimum)</b>	<b>Bldg.</b>	<b>Pkg.</b>	<b>Setbacks for Architectural and Other Features (Minimum)</b>
SFR 4.0	SIDE			
	One or two story home element	5'		Front – 10' minimum Side <sup>2</sup> – 3'4" minimum
	One or two story home element on corner lot	10'		Rear – 12' minimum
	One story porch	5'		COVERED PATIO, GAZEBO, ARBOR, OR SIMILAR <sup>3</sup>
	One story porch on corner lot	8'		Side – 3' one side, 5' other side, minimum
	REAR			Rear – 6' minimum
	Adjacent to Open Space			
	Average	15'		OPEN SECOND FLOOR BALCONY (at rear)
	Minimum	12'		Side – 10' minimum
	Adjacent to Other than Open Space			Rear – 10' minimum
	One story on typical lot	15' <sup>4</sup>		
	Two story on typical lot	15' <sup>4</sup>		
	Rear garage	5'		
	SFR 4.5	FRONT		
One Story Porch		12'		Front – 10' minimum
Swing-in garage		10'		Side <sup>2</sup> – 3'4" minimum
One or two story home element		12'		Rear – 12' minimum
Front-in garage		18'		FIRST STORY BAY WINDOWS (at grade with a maximum width of 12')
SIDE				
One or two story home element		5'		Front – 10' minimum Side <sup>2</sup> – 3'4" minimum
One or two story home element on corner lot		10'		Rear – 12' minimum
One story porch		5'		COVERED PATIO, GAZEBO, ARBOR, OR SIMILAR <sup>3</sup>
One story porch on corner lot		8'		Side – 3' one side, 5' other side, minimum
REAR				Rear – 6' minimum
Adjacent to Open Space				
Average		15'		OPEN SECOND FLOOR BALCONY (at rear)
Minimum		12'		Side – 10' minimum
Adjacent to Other than Open Space				Rear – 10' minimum
One story on typical lot		15' <sup>4</sup>		
Two story on typical lot		15' <sup>4</sup>		
Rear garage	5'			
SFR 5.0	FRONT			ARCHITECTURAL FEATURES <sup>1</sup>
	One Story Porch	12'		Front – 10' minimum
	Swing-in garage	10'		Side <sup>2</sup> – 3'4" minimum
	One or two story home element	12'		Rear – 8' minimum
SFR 5.0	Front-in garage	18'		FIRST STORY BAY WINDOWS (at grade with a maximum width of 12')
	SIDE			

**Table 7.D - Setbacks and Permitted Intrusions (Continued)**

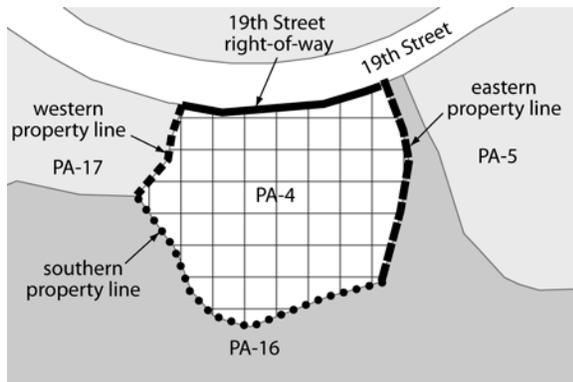
<b>Zone</b>	<b>Building and Parking Setbacks (Minimum)</b>	<b>Bldg.</b>	<b>Pkg.</b>	<b>Setbacks for Architectural and Other Features (Minimum)</b>
	One or two story home element	5'		Front – 10' minimum Side <sup>2</sup> – 3'4" minimum
	One or two story home element on corner lot	10'		Rear – 15' minimum
	One story porch	5'		COVERED PATIO, GAZEBO, ARBOR, OR SIMILAR <sup>3</sup>
	One story porch on corner lot	8'		Side – 3' one side, 5' other side, minimum
	REAR			Rear – 6' minimum
	One story on typical lot	18' <sup>4</sup>		
	Two story on typical lot	20' <sup>4</sup>		OPEN SECOND FLOOR BALCONY (at rear)
	Rear garage	5'		Side – 10' minimum
	Lot rear on Open Space (average)	15'		Rear – 10' minimum
SFR 6.0	FRONT			ARCHITECTURAL FEATURES 1
	One Story Porch	12'		Front – 10' minimum
	Swing-in garage	10'		Side <sup>2</sup> – 3'4" minimum
	One or two story home element	15'		Rear – 8' minimum
	Front-in garage	18'		FIRST STORY BAY WINDOWS (at grade with a maximum width of
	SIDE			12'
	One or two story home element	5'		Front – 10' minimum Side <sup>2</sup> – 3'4" minimum
	One or two story home element on corner lot	10'		Rear – 15' minimum
	One story porch	5'		COVERED PATIO, GAZEBO, ARBOR, OR SIMILAR <sup>3</sup>
	One story porch on corner lot	8'		Side – 3' one side, 5' other side, minimum
	REAR			Rear – 6' minimum
	One story on typical lot	18' <sup>4</sup>		
	Two story on typical lot	20' <sup>4</sup>		OPEN SECOND FLOOR BALCONY (at rear)
	Rear garage	5'		Side – 10' minimum
	Lot rear on Open Space (average)	15'		Rear – 10' minimum
RF	FRONT			ARCHITECTURAL FEATURES <sup>1,2</sup> – 30"
	Building face to property line	25'		EXTERIOR STAIRS <sup>11</sup> – 10'
	To edge of slope	15'		FIRST STORY BAY WINDOWS <sup>2, 12, 13</sup> – no limit on maximum intrusion, except may not intrude into driveway, pedestrian path, or recreation area.
	SIDE			If bay window is at grade with a maximum width of 36"
	Side to side	15'		GARAGES UNDER BUILDING – no limit on
	To adjacent residential development	30'		COURTYARDS <sup>6</sup> – minimum dimension 55' (typical 63') of common area; maximum cross slope of 2%.

**Table 7.D - Setbacks and Permitted Intrusions (Continued)**

Zone	Building and Parking Setbacks (Minimum)	Bldg.	Pkg.	Setbacks for Architectural and Other Features (Minimum)
	Balcony to balcony	15'		INTERNAL ROADS – no limit on maximum intrusion if curb-separated sidewalks provided
SS	FRONT	20'	20'	NR
	SIDE	5'	10' <sup>5</sup>	
	REAR	20'	10' <sup>5</sup>	

- Notes:**
- 1 Fireplaces, eaves, cornices, canopies, belt courses, sills, media niches, buttresses, wing walls, utility meters, etc., which do not create additional floor area.
  - 2 Projecting chimneys and bays may not directly oppose similar projections on adjacent homes.
  - 3 The ground floor square footage for main and accessory structures, including covered patios, gazebos, arbors, and similar structures, shall not cover more than 60 percent of the lot.
  - 4 Required 30' minimum setback to opposing building elevation.
  - 5 Six foot high masonry wall required at corresponding property line.
  - 6 "Courtyard" shall be defined as the outdoor space framed by buildings, wholly or in part, on at least three sides.
  - 7 For SFR 4.0 and 5.0, a "zero lot line" detached residential development pattern may be used, where one side of the dwelling unit is located on the side property line of the lot (and a reciprocal easement is granted to allow this dwelling owner access to the side of the dwelling located on the property line for maintenance), as long as the required side setback for the other side of the dwelling that is not located on the property line is doubled (i.e., for two adjacent dwelling units the separation distance between them is maintained as it would be if the side setbacks stated in Table 7.D were used in a conventional development pattern).
  - 8 For SFR 4.0, 5.0, and 6.0, a "z-lot" pattern may be used in which a side lot line may be offset in a "z" configuration rather than perpendicular to the street, when accompanied by a use easement. In "z-lot" patterns, the lot area and width dimension may be less than the minimum required as long as the two adjacent lots forming the "z" configuration average out to the minimum requirements. In addition, in "z-lot" patterns in SFR 5.0 and 6.0, the minimum rear setbacks required by Table 7.D may be considered the required *average* rear setback applicable to each dwelling unit, with the minimum rear setback to be not less than 15 feet. (For an example of a "z-lot" configuration, see Section 8.3.3 - Massing and Plotting in the Residential Design Guidelines.)
  - 9 Senior Care Facilities with a CUP will be subject to different standards, as detailed in section 7.8.23
  - 10 In the event that residential and commercial uses are mixed on site, NC commercial standards shall apply to the commercial buildings, and mixed use standards shall apply to the residential buildings. In the event that residential and commercial uses are mixed within a building, mixed use standards shall be used, and a parking study will be required.
  - 11 Stairs may intrude 10 feet into the required 25' setback.
  - 12 Building may face basin.
  - 13 An eight foot easement for a trail may be included within the required setback within Planning Area 22 on the boundary edge facing the basin.

**b. Building and Parking Setbacks for Mixed Use Zone**



Setbacks from the 19<sup>th</sup> Street right-of-way:

- 3 ft min. for surface parking, underground parking, internal streets, and alleys.
- 25 ft min. for buildings, including above-grade parking structures

Setbacks from the western property line facing the private street in PA-17:

- 3 ft min. for surface parking and underground parking structures

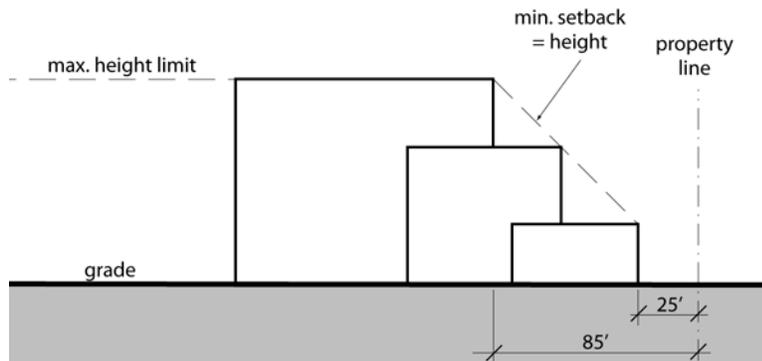
- 15 ft min. for buildings, including above-grade parking structures

Setbacks from the southern property line facing PA-16:

- 3 ft min. for internal streets, alleys, surface parking, and underground parking structures
- 25 ft min. for buildings, including above-grade parking structures

Setbacks from the eastern property line facing PA-5:

- 3 ft min. for alleys, internal streets, and surface parking
- 25 ft min. for buildings up to 25 ft in height. Additional building height shall be permitted up to a limit of 85 feet in overall building height if additional setback distance is provided at a ratio of 1 to 1. In no case shall the overall building height be greater than the building setback. See diagram below.
- 85 ft min. for structures up to 85 ft in height.



Setbacks from internal private street right-of-way:

- 0 ft min. for underground parking structures
- 0 ft min. for buildings or portions of a building with ground floor commercial uses
- 10 ft min. for buildings or portions of a building with ground floor residential uses
- 3 ft min. for surface parking
- 15 ft min. for above-grade parking structures

Setbacks from alleys:

- 5 ft min. for garage entrances

Setbacks between buildings in PA-4:

- 0 ft for attached structures, including structured parking
- 20 ft min. between the side elevations of detached structures
- 30 ft min. between the side elevation of a building and the front or facade that includes entrances on one side
- 40 ft min. between building fronts or facades that include entrances on both sides

Setback encroachments:

- 3 ft max. for architectural features, including but not limited to bay windows, chimneys, cornices
- 10 ft max. for stairs, porches, stoops, covered entries, balconies or decks

**c. Setbacks for Accessory Buildings.** All accessory buildings shall have the same minimum side and rear setback requirements as for the main building, regardless of whether the accessory building is

attached to or detached from the main building. However, when a garage or accessory building (except those occupied as a dwelling unit) abuts and has garage doors opening onto an existing or proposed alley, the garage or accessory building shall be located not less than 25 feet from the side of the alley adjacent to the subject property, provided, however, that the setback of detached accessory buildings or garages that are located 85 feet or more from the front property line, may be reduced to a zero (0) distance from the side or rear property line if a fire wall is provided and approved by the Fire Department. Detached accessory buildings or garages without a fire wall shall be a minimum of 3 feet from the property line.

## **7.6 PARKING AND LOADING**

### **7.6.1 Parking**

**a. Location of Parking.** Unless off-site parking is provided for by a reciprocal parking agreement, parking spaces shall be provided either:

- On the premises of the building which requires the parking,
- On a site not more than 500 feet from the external boundaries of the lot upon which the building that requires the parking is located, or within an integrated commercial center (which will not differentiate between individual lots).

**b. Number of Required Parking Spaces.** See Table 7.C - “General Development Standards” for the number of parking spaces required for uses in each zone. For the purposes of calculating the adjusted gross floor area (AGFA) for a project, the following items may be excluded from the overall dimensions of the building project.

- Interior courtyards
- Interior atriums
- Thickness of exterior walls of the structure
- Elevator shafts
- Stair wells
- Equipment/maintenance rooms
- Basements proposed exclusively for storage of records or similar uses
- Restrooms
- Exterior sales and seating areas other than those that are part of the main business of a commercial use Interior and exterior corridors, hallways, or balconies
- Janitor and storage rooms
- Other unusable floor area (as defined by the Administrative Committee).

For example, seating provided in a plaza area adjacent to a building would not be counted for purposes of calculating required parking, whereas outdoor dining areas for individual restaurants or groups of restaurants that are an integral part of the restaurant use would be used in determining required parking. Similarly, the “patio area” in front of home improvement stores that is often used to display merchandise would not be used in calculating required parking since that area does not generate traffic to the store in addition to the interior space.

**c. Adjustments to Required Parking Ratios.** The parking ratios set forth in Table 7.C are for typical commercial centers. As a result, there exist uses such as health clubs and spas, garden centers associated with home improvement uses, theaters and vehicles sales lots that have substantially different parking

demand (either higher or lower) than the average parking rates set forth in Table 7.C. In addition, the provision of stacking areas for drive-through facilities tends to reduce parking demands for the uses they serve. Adjustments to the required parking ratios given in Table 7.C, above, are presented for such uses in the following Table 7.E.

**Table 7.E - Commercial Parking Adjustments**

Land Use	Required Parking Spaces
Health Clubs and Spas	1 space for every 150 square feet of AGFA for facilities with playing courts, and 1 space for every 100 square feet of AGFA for facilities without playing courts <sup>1</sup>
Home Improvement, area devoted to permanent outdoor display which is characterized by an open roof and/or is open on one or more sides, and is not an air-conditioned space (i.e., garden centers)	1 space for every 1,000 square feet of permanent outdoor display area (the required parking ratios for the remainder of the home improvement use shall be as established in Table 7.C)
Theaters In Planning Areas with no office development: Fixed seats Temporary or movable seats In Planning Areas with office development (minimum 40,000 square feet AGFA): Fixed seats Temporary or movable seats	1 per 5 seats, or 6 spaces per 1,000 square feet AGFA, whichever is greater 1 space per every 40 square feet of area within the main structure 4.33 spaces per 1,000 square feet AGFA 1 space per every 80 square feet of area within the main structure
Vehicle Sales	1 space for every 250 square feet of office and showroom.
Drive-through Stacking Areas	Required parking shall be reduced by 1.0 parking spaces per 20 feet of drive-through stacking area that is provided.
Planning Areas 19A and 21	4 spaces for every 1,000 square feet of commercial. This is to allow for the parking of the 200' Metropolitan Water District easement, which will not be counted in the requirements, but which will nevertheless be developed as parking stalls.

**Notes:** 1. For this use, swimming pool area shall be counted as playing court area.  
 2. May be reduced to 4.33 spaces per 1,000 AGFA if the theater is located within a Planning Area containing office development.

**d. General Parking Standards.** The following guidelines refer to general standards for parking requirements.

- Parking stall size for perpendicular parking shall be at least 9 feet in width and 18.5 feet in length for standard parking spaces and 9 feet in width and 16 feet in length for compact parking spaces.
- Minimum driveway aisle widths and stall dimensions shall be as follows in Table 7.F for the indicated parking layout angle using standard parking spaces or compact parking spaces.
- Parallel parking stalls shall be at least 10 feet in width and 23 feet in length.
- Within parking structures, aisle widths may be reduced to 24 feet. Vehicle height clearance shall be as required by the Uniform Building Code for the parking structure entry and throughout the interior of the structure.
- Parking stalls adjacent to walls or raised planters over eight inches in height shall be at least 12 feet in width.
- For residential uses, each garage parking space shall be a minimum of 10 feet in width and 20 feet in length.
- The handicapped parking requirements established by the State of California shall be considered and adhered to when designing and constructing parking lots, parking garages, or other any parking facility.

**Table 7.F - Parking Layout Dimensions**

Dimension	Letter Indicated on Diagram	Standard Parking Stall Angle				Compact Parking Stall Angle			
		45E	60E	75E	90E	45E	60E	75E	90E
Stall width, parallel to aisle (feet)	A	12.7	10.4	9.3	9.0	12.7	10.4	9.3	9.0
Stall length of line (feet)	B	27.5	23.7	20.9	18.5	25.0	21.2	18.4	16.0
Stall depth to wall (feet)	C	19.5	20.5	20.0	18.5	17.7	18.4	17.8	16
Aisle width between stall lines (feet)	D	12.0	19.0	23.0	26.0	12.0	19.0	23.0	26.0
Stall depth, interlock (feet)	E	16.5	18.5	19.0	18.5	14.5	16.2	16.6	16.0
Module, wall to interlock (feet)	F	48.0	58.0	62.0	63.0	44.2	53.5	57.4	58.0
Module, interlocking (feet)	G	45.0	56.0	61.0	63.0	41.0	51.4	56.2	58.0
Module, interlock to curb face (feet)	H	46.0	55.5	59.5	60.5	42.4	51.2	54.9	55.5
Bumper overhang (typical) (feet)	I	2.0	2.3	2.5	2.5	2.0	2.3	2.5	2.5
Offset (feet)	J	6.4	2.6	0.6	0.0	6.4	2.6	0.6	0.0
Setback (feet)	K	13.1	9.3	4.8	0.0	11.3	8.0	4.1	0.0

- Where parking areas adjoin residential zones, they shall be separated by a solid masonry wall constructed per applicable standards stated in this Specific Plan. Where no wall is required along a boundary of an area, there shall be a concrete curb not less than 6 inches in height abutting property on public right-of-way.
- Adequate vehicular access from a dedicated and improved street, service road or alley, to any commercial use shall be constructed to withstand anticipated commercial usage. Such access shall be reviewed and approved by the City Engineer.
- All areas shall be surfaced or paved with asphaltic concrete, concrete or other bitulithic surfacing acceptable to and in accordance with the engineering standards of the City. All parking areas shall be maintained in good condition.
- Parking stalls shall be marked and access lanes shall be clearly defined, and include directional arrows.
- A maximum of 25 percent of the total number of non-residential parking stalls may be provided in compact car stalls. Such compact car spaces shall comply with all applicable City standards.
- Compact parking stalls shall be clearly marked “COMPACT CARS ONLY” in 6-inch high letters.
- No non-residential parking stall shall be located where it would be necessary for the driver to back up across a public sidewalk.
- Curbs shall be required on all parking stalls abutting a property line or in other cases deemed necessary by City staff. In no case shall vehicles extend over the public sidewalk or property line.
- A double loaded field parking aisle serving 90E parking stalls may be reduced from 26 feet to 24 feet in width if the aisle serves 25 or fewer parking stalls, subject to approval of the Upland Community Development Director.
- Lighting of parking lots shall comply with Section 8.2.9, Outdoor Lighting.
- Parking or storage of recreational vehicles (RVs), boats, trailers, personal watercraft, and similar motorized or non-motorized vehicles shall be within a fully enclosed garage that is architecturally compatible with the main building.

**Loading.** Commercial and other uses within the CH and NC zones shall provide vehicle loading spaces not less than 10 feet in width, 20 feet in length and 14 feet vertical clearance in accordance with the following requirements.

- Loading areas shall be designed to provide for backing and maneuvering on site and not from or within a public street. Direct loading from a public street shall not be permitted.
- Loading and maneuvering areas shall not encroach into required setback areas.
- Screen walls and wing walls shall be provided adjacent to loading doors and loading areas, shall be of a material compatible with adjacent buildings, and shall be of sufficient height to provide adequate visual screening from adjacent public street rights-of-way and the center of any commercial center.
- Loading areas may be permitted adjacent to a public street provided they are screened by a combination of screen walls, ornamental landscaping, and/or portions of on-site buildings to the extent that the loading area is adequately shielded, to the satisfaction of the Community Development Director. All such loading areas shall be maintained in a clean and orderly condition.
- Truck maneuvering areas associated with on-site loading areas and vehicle drop-off areas for the proposed automobile dealerships shall not encroach into required parking stalls.
- On-site truck maneuvering to and from loading areas shall be provided to accommodate the turning radius of a 66-foot extra large semi-trailer (WB-62). With the approval of the Community Development Department, a greater or lesser turning radius may be required based upon review of a site specific analysis.
- When the lot upon which a loading area is located abuts upon an alley, such loading space shall adjoin or have access from the alley. The length of the loading space shall be measured perpendicular to or parallel with the center line of the alley. Where such loading area is parallel with the alley, the loading space shall extend across the full width of the lot, except that if only two spaces are required, the length of the loading area need not exceed 50 feet. Loading space required by this section may occupy a required rear yard, but in no case shall any part of an alley or street be used for loading.

Table 7.G presents the minimum number of loading spaces that are required.

**Table 7.G - Required Loading Spaces**

<b>Use (Adjusted Gross Floor Area)</b>	<b>Required Loading Spaces</b>
Commercial Buildings up to 100,000 square feet	1
100,001 square feet or more	2
Hotels and Office Buildings up to 100,000 square feet	1
100,001 square feet or more	2

## **7.7 SIGN STANDARDS**

### **7.7.1 Purpose and Intent**

It is the intent of the sign development standards, sign guidelines, and conceptual placement locations set forth in this section to maximize identification of the Colonies at San Antonio as a distinct planned community, and to maximize the visibility of individual commercial tenants within the Specific Plan area in a manner that compliments the overall image of the Colonies at San Antonio. The provisions of this Section set forth the requirements that apply to signage throughout the Colonies at San Antonio Specific Plan area.

### 7.7.2 Applicability

- a. All signs for project identification, residential subdivisions, commercial developments, and the central open space area shall require approval of a Sign Program by the Community Development Director, unless otherwise specified herein. Alteration or replacement of signs approved as part of a Sign Program shall also require approval of the Community Development Director, unless otherwise specified herein.
- b. Each of the commercial centers intended for Planning Areas 17, 18, and 19 shall have a comprehensive Sign Program approved by the City. Individual development sites within Planning Areas 20 and 21 may have individual Sign Programs. Individual Sign Programs may also be prepared for individual residential subdivisions. A single, comprehensive sign program shall be prepared for project identification signage, and a single sign program shall be prepared for Planning Area 16 open space signage.

### 7.7.3 Sign Review Process

- a. Sign Programs or amendments to Sign Programs shall be submitted to the Community Development Director on forms as prescribed by the Community Development Director. The application shall be accompanied by any fees or bonds as specified by City Council resolution.
- b. After receipt of an application for a Sign Program or an amendment thereto, the Community Development Director or a designated representative shall review the application for consistency with the provisions of this Section. The Community Development Director or a designated representative shall render a decision to approve, approve with modifications, or deny the sign request within fifteen working days of receipt of a complete application.

### 7.7.4 Submittal Requirements

Sign Program submittals shall include the drawings and details required for review as specified on applications forms provided by the Community Development Director, including, as appropriate:

- a. Elevation(s) of buildings for which signs are being requested, showing design, location, size, and layout of wall signs. Elevations shall be drawn to scale indicating dimensions, attachment devices, and construction details.
- b. Site plan of the site for which signs are being requested showing building and perimeter with location of proposed and existing ground mounted and pole mounted signs, and elevations showing proposed design and dimensions of signs.
- c. Materials board showing actual sample materials and “PMS” or locally manufactured paint color(s).
- d. Section through letter and/or sign panel showing the dimensioned projection of the letter face and/or sign panel. The method of illumination shall also be identified.

### 7.7.5 General Provisions

- a. **Sign Guidelines.** The guidelines below establish positive criteria for the design and implementation of desirable signs that complement the landscape and architectural elements in the Specific Plan area. The following guidelines should be considered prior to developing any individual sign programs in the Specific Plan area.

- Use a brief message.
  - Avoid faddish and complicated typefaces.
  - Select sign colors and materials to contribute to legibility and design integrity.
  - Harmonize sign size with scale of project environment.
  - Design wall signs to be appropriate to the buildings on which they are placed.
  - Place signs consistent with the proportions of scale of building elements within the facade.
  - Place wall signs to establish a facade rhythm.
  - The use of tenant corporate signs/logos is encouraged.
- b. Fabrication and Installation.** All signs and installations of the signs shall conform to appropriate building and electrical codes, as well as sign approval requirements of the City of Upland. Any illuminated sign or lighting device shall employ only lamps emitting a light of constant intensity. All signs located within the Metropolitan Water District (MWD) right-of way shall be installed in such a manner so as to provide for relocation (i.e., j-bolt mounting).
- c. Sign Maintenance.** All signs shall be maintained in good order and repair, including replacement of damaged Plexiglass faces, letters, and/or burned out lighting.
- d. Color.** Consistent application of a project color palette will serve to reinforce project identity and shall be identified in sign submittals. Selection of sign color shall be compatible with adjacent tenant signs.
- e. Letter Style.** Dimensional/channel letters and other sign elements shall be affixed without visible means of attachment, unless attachments make an architectural statement. Typography on all project-oriented signs, including tenant directional information, shall be restricted to the type style identified in the applicable approved Sign Program, unless otherwise approved by the Community Development Director.
- f. Sign Area.** The area of a sign shall be measured by a rectangular box around the outside of each group of lettering and/or graphics. Typographical ascenders and descenders shall not be included in the calculation of the sign face area.
- g. Protection from Graffiti.** Anti-graffiti finish shall be provided and maintained on all signs. Graffiti shall be removed immediately by the party responsible for sign maintenance.

#### **7.7.6 Size and Number of Signs**

The size and number of signs shall be consistent with the provisions of Table 7.H.

**Table 7.H - Commercial Signage Standards**

<b>Sign Type</b>	<b>Maximum Number</b>	<b>Maximum Sign Area or Allowed Ratio</b>	<b>Maximum Sign Structure Dimensions (From Grade)</b>
Primary Freeway Pylon	Four double sided primary pylons (“PF1” through “PF4” in Figure 7.1) at locations shown in the Figure to be located a minimum of 600 feet from residential uses east of the Specific Plan area and 500 feet from residential areas west of the Specific Plan area.	<p>Primary Pylons FP1, FP2 (east face), FP3, FP4 (west face)</p> <p>Pylon signs shall incorporate up to 4 sign cabinets per face to identify tenants or project components. (Each cabinet may hold up to 2 tenant names, for a total of 8 tenants per pylon side.) Maximum tenant letter height shall be 6 feet.</p> <p>Primary Pylons FP2 (west face), FP4 (east face)</p> <p>Pylons signs shall incorporate full color LED electronic message centers with a maximum size of 480 square feet (maximum width 18' feet, maximum height 26'6").</p>	Primary freeway pylon maximum height of 65 feet above the elevation of the adjacent freeway roadway.
Secondary Freeway Signage	Four double sided secondary pylons (“SF1 through SF4” in Figure 7.1) at locations shown in the figure to be located near the Campus Avenue interchange.	<p>Secondary Pylons SP1 (east face), SP2, SP3 (west face), SP4</p> <p>Secondary freeway signs shall incorporate up to 4 sign cabinets per face to identify tenants or project components. (Each cabinet may hold up to 2 tenant names, for a total of 8 tenants per pylon side. Signs may be double sided.) Maximum tenant letter height shall be 4 feet.</p> <p>Secondary Pylons SP1 (west face), SP3 (east face)</p> <p>Pylons signs shall incorporate full color LED electronic message centers with a maximum size of 480 square feet (maximum width 18' feet, maximum height 26'6").</p>	Secondary freeway sign maximum height of 55 feet above the elevation of the adjacent freeway roadway.
Monument, Primary, Secondary, and Tertiary	Per locations shown in this Section (“P”, “S”, and “T” in Figure 7.1).	<p>Primary monuments shall incorporate up to 5 tenant identifications per face. Secondary monuments shall incorporate up to 4 tenant identifications. Tertiary monuments shall identify up to two tenants per sign.</p>	Primary monument maximum height of 10 feet and width of 50 feet. Secondary corner monuments maximum width of 40 feet and a maximum height of 9 feet. Tertiary monument maximum width of 12 feet and a maximum height of 11 feet from average grade.
Fuel Price	No more than one price information sign shall be permitted for any facility.	Fuel price signs shall be provided in accordance with the provisions of State law.	Maximum width of 8 feet and a maximum height of 4 feet.

<b>Sign Type</b>	<b>Maximum Number</b>	<b>Maximum Sign Area or Allowed Ratio</b>	<b>Maximum Sign Structure Dimensions (From Grade)</b>
Pedestrian Directional	Per approved sign program.	Pedestrian directional monument signs shall have a sign area with a maximum width of 6 feet and a maximum height of 7 feet. Pedestrian directional pole mounted signs shall have a sign area with a maximum width of 2.2 feet and a maximum height of 4 feet. The pole mounted sign area shall be located at least 7.5 feet from grade.	Maximum height for pedestrian directional monument signs of 8 feet.
Vehicular Directional	Per approved sign program.	Vehicular directional signs shall have a sign area with a maximum width of 3.5 feet and a maximum height of 4.5 feet.	Maximum width of 4.5 feet and maximum height of 7 feet.
Drive Thru Directional	Drive thru directional signs per approved sign program. One menu board per drive thru restaurant tenant.	Drive thru directional signs shall have a sign area with a maximum width of 4 feet and a maximum height of 4 feet. Menu board structures shall have a maximum area of 60 square feet.	Maximum width of 5.5 feet and maximum height of 6 feet for drive thru directional sign.
Wall	One per building (or tenant space) elevation.	Major tenants: On building frontage, 1.5 square feet of sign area (including any logo) per lineal foot of building frontage. On freeway elevation, 1.0 square feet of sign area (including any logo) per lineal foot of building elevation. For elevations backing up to 19th Street, 1.5 square feet of sign area (including any logo) per lineal foot of building elevation. (In addition, theater buildings may have panels to allow for display of movie posters or other promotional materials along the rear building elevation, with a combined maximum of 1,500 square feet for poster display areas.) In-line shop tenants: On building frontage, 1.5 square feet of sign area (including any logo) per lineal foot of building frontage. On freeway elevation, 1.0 square feet of sign area (including any logo) per lineal foot of building elevation. All tenants are permitted a wall sign at least 40 square feet; however, the maximum allowable wall sign area per tenant shall be 500 square feet.	Not applicable.
Marquee	One per theater use.	On building frontage, 1.5 square feet of sign area (including any logo) per lineal foot of building frontage.	See Table 7.C - General Development Standards
Kiosk Signs	Signage allowed on no more than 3 elevations per kiosk.	On elevations where signs are permitted, 1.0 square feet of sign area shall be allowed per lineal foot of the signed building elevation. All tenant identification signs, logos, menu boards, service lists, and similar signage shall be included in this limit.	Not applicable.

Sign Type	Maximum Number	Maximum Sign Area or Allowed Ratio	Maximum Sign Structure Dimensions (From Grade)
		All kiosks are permitted at least 15 square feet of signage per signed elevation; however, the maximum allowable signage area per signed elevation shall be 40 square feet.	
Office Signage	No requirement for primary identification signs. Four per elevation for eyebrow identification signs.	For primary identification signs, 1.5 square feet of sign area (including any logo) per lineal foot of building frontage. Maximum allowable sign area shall be 120 square feet. For eyebrow identification signs, the maximum allowable sign area shall be 30 square feet.	Not applicable.
Wall Graphics	No requirement.	On freeway elevations, illuminated graphic “lifestyle” posters within permanent display frames shall be permitted, with a maximum square footage of 120 square feet per poster display area.	Not applicable.
Banner (pole mounted) or flag	No requirement.	Banners and flags shall have a sign area with a maximum width of 4 feet and a maximum length of 12 feet.	Banner signs and flags shall be located 3 feet below light fixture on parking lot light standard.
Temporary (future tenant)	No requirement.	Temporary (future tenant) signs shall have a sign area with a maximum width of 6 feet and a maximum height of 7.8 feet.	No requirement.
Temporary (property for lease or sale)	No requirement.	Temporary (property for lease or sale) signs shall have a sign area with a maximum width of 8 feet and a maximum height of 10 feet.	No requirement.

<sup>1</sup> LED electronic message centers will be required to be consistent with all Caltrans, State and Federal Regulations regarding on-site and off-site advertising. <sup>2</sup> Standards for electronic display signs can be found in Section 7.8.27 Electronic Signage Standards.

### 7.7.7 Commercial Signage

Conceptual locations of the commercial project identification signs are shown in Figure 7.1. Following are the development provisions for these commercial project identification signs as well as provisions for individual commercial signs on private property, listed according to sign type.

**a. Primary Freeway Pylon and Secondary Freeway Signs.** In addition to the standards of Table 7.H, freeway pylon signs shall:

- (1) incorporate the logo and project identity of the Colonies at San Antonio or Colonies Crossroads, as well as the City of Upland,
- (2) include business and/or tenant identification where appropriate,
- (3) be double faced and sited perpendicular to the I-210 freeway,
- (4) allow a backlit graphic image to be located on the pylon signs, to be changed periodically according to various events or seasons,
- (5) not exceed a width of ½ of the sign’s total height above the freeway roadway grade,

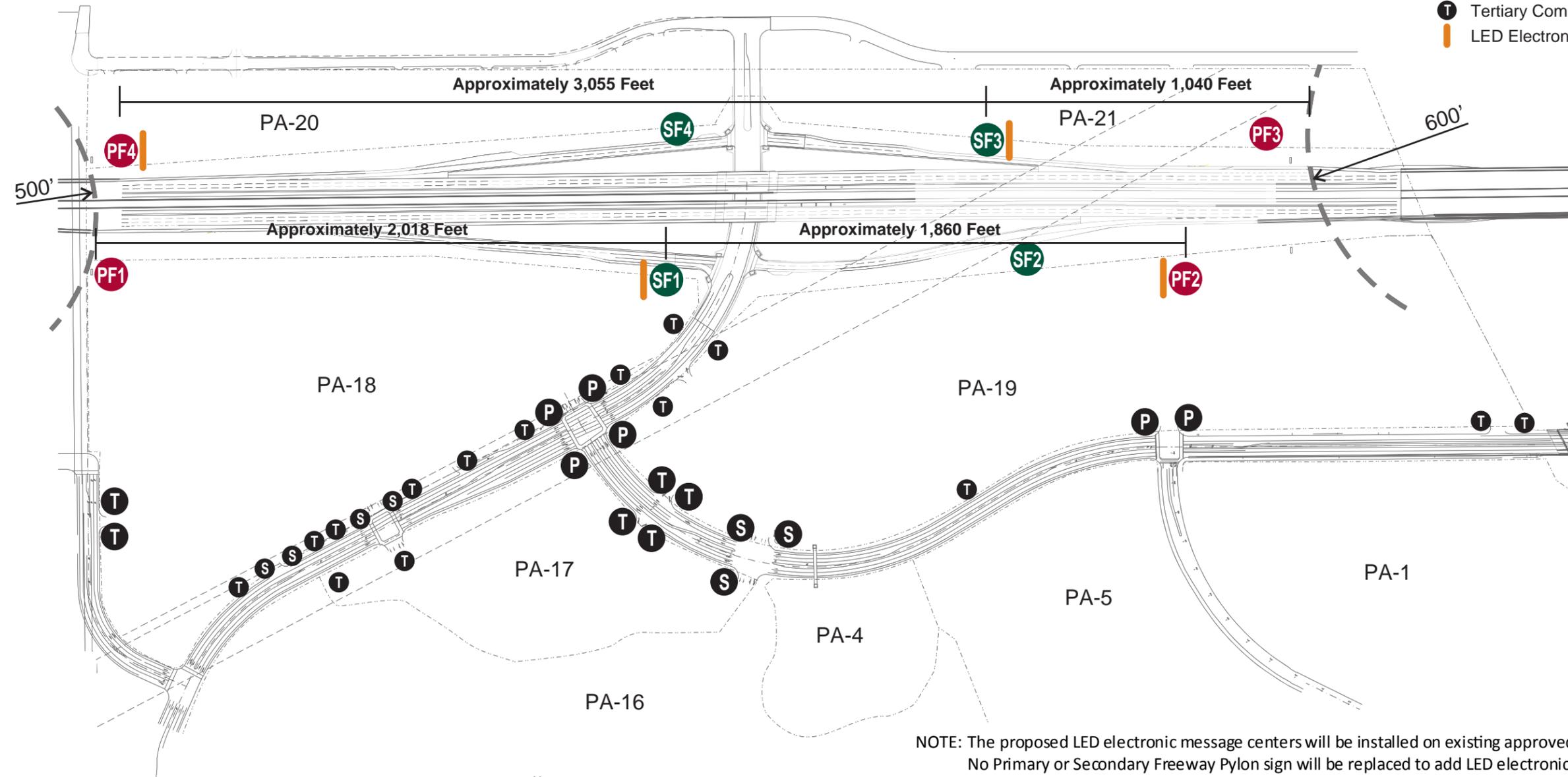
- (6) have a tenant sign area that is proportional and appropriate to the overall sign size,
- (7) not have any one tenant sign exceeding 50 percent of the overall allowable tenant sign area,
- (8) allow full color LED electronic message centers on Primary Freeway Pylons FP2 (west face), FP4 (east face) and Secondary Pylons SP1 (west face), SP3 (east face) subject to the following requirements. LED electronic message centers shall:
  - a. be mounted on signs of the same design as those not including LED electronic message centers.
  - b. be limited to a maximum size of 480 square feet, including a maximum width of 18' and a maximum height of 26' 6".
  - c. have transitions of images occurring no more than once every eight (8) seconds, with transitions not creating the appearance of movement, flashing, or animation,
  - d. be oriented in the direction of commercial activity to avoid glare on residential properties with electronic message signs only allowed on the faces of the primary pylons and secondary pylons as indicated on Figure 7.1 for a total of four (4) electronic message displays,
  - e. have electronic message displays that include ambient light sensing mechanisms and automatic dimming capabilities in order to adjust brightness levels between daytime and nighttime conditions:
  - f. not exceed brightness levels of 0.3 foot candles above ambient light conditions, as measured using a foot candle meter in conformance with the following process:
    - (1) Light measurements shall be taken with the meter aimed directly at the electronic message display sign face, or at the area of the sign emitting the brightest light if that area is not the sign face, at the following distances:
      - A sign that is 0 to 100 square feet in area shall be measured at a distance of 100 feet from the sign area being measured;
      - A sign that is 101 to 350 square feet in area shall be measured at a distance of 150 feet from the sign area being measured; and
      - A sign that is greater than 350 square feet in area shall be measured at a distance of 200 feet from the sign area being measured.
    - (2) An ambient light measurement shall be taken using a foot candle meter sometime between thirty (30) minutes past sunset or thirty (30) minutes before sunrise with the sign turned off to a black screen.
    - (3) An operating sign light measurement shall be taken with the sign turned on to full white copy immediately following the ambient light measurement.
    - (4) The brightness level of the electronic message display conforms with the brightness requirements of this subsection if the difference between the ambient light measurements and the operating sign light measurement is 0.3 foot candles or less.
  - g. devote a minimum of 10 percent of display time per hour spread evenly over the day to community events, community service announcements, and public safety announcements (e.g., Amber Alert),
  - h. not contain illegal, immoral, or inflammatory language,

# COMMERCIAL SIGNAGE CONCEPTUAL LOCATION

## LEGEND

### COMMERCIAL SIGNAGE

- PF Primary Freeway Pylon
- SF Secondary Freeway Signage
- P Primary Commercial Monument
- S Secondary Commercial Monument
- T Tertiary Commercial Monument
- | LED Electronic Message Center



NOTE: The proposed LED electronic message centers will be installed on existing approved signs.  
No Primary or Secondary Freeway Pylon sign will be replaced to add LED electronic message centers.



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- i. follow all applicable State and Federal Regulations regarding on-site and off-site advertising set forth in the Business and Professions Code, as well as the provisions of California Vehicle Code Section 21466.5 that prohibits placing, maintaining, or displaying in view of any state highway “any light of any color of such brilliance as to impair the vision of drivers upon the highway.”
  - j. All signs shall contain a default mechanism that will cause the sign to revert immediately to a black screen if the sign malfunctions,
  - k. Except as specifically set forth in this Specific Plan, electronic message displays shall comply with the provisions of the Section 17.20.210 Highway commercial zone – Electronic changeable-copy signs of the Upland Municipal Code, and
- (9) be programmed so as to have LED electronic message centers facing in the same direction transition between images simultaneously.
- b. Monument Signs.** In addition to the standards of Table 7.H, monument signs shall:
- (1) be located and installed in a manner consistent with sight-distance criteria established by the City of Upland.
- c. Fuel Price Signs.** In addition to the standards of Table 7.H, fuel price signs shall:
- (1) identify supplier logo and pricing information only,
  - (2) be installed within required landscape setback areas,
  - (3) be placed perpendicular to the adjacent roadway right-of-way or if the facility is located on a corner, be placed diagonally at the intersection so that it can clearly be seen from both roadway right-of-ways, and
  - (4) meet State requirements.
- d. Pedestrian and Vehicular Directional Signs.** In addition to the standards of Table 7.H, pedestrian and vehicular directional signs shall meet the following requirements.
- (1) Pedestrian and vehicular directional signs shall not be included in calculating the building sign area.
  - (2) Pedestrian and vehicular directional signs shall identify the business and incorporate a directional arrow.
  - (3) Pedestrian and vehicular directional signs shall not be oriented to the public street.
  - (4) Pedestrian directional signs may be freestanding monument signs that provide direction through a location map and list of names, or pole-mounted signs with the direction of facilities and tenants indicated using arrows.
  - (5) Vehicular directional signs may be freestanding or wall mounted.
  - (6) Vehicular directional signs located in the median can be used to direct vehicles to significant features of the development and to parking entrance/exit locations.
  - (7) Traffic regulatory signs may be reduced in scale where viewing distances are diminished.
  - (8) Ground-mounted vehicular directional signs for drive-thru businesses (i.e., restaurants, financial services, gas stations) may be strategically located to direct vehicular traffic; however, their placement shall not interfere with sight lines or traffic.
  - (9) Menu signs shall be single sided, internally illuminated, and oriented towards drive thru traffic (not in a manner that serves as an advertising device, not oriented off property towards center of property).

- e. Wall Signs.** In addition to the standards of Table 7.H, wall signs shall meet the following requirements.
- (1) Tenants shall only display their established trade name or their basic product name. The name shall be consistent if multiple signs are permitted (see Table 7.H).
  - (2) Signs shall maintain a minimum distance of the average letter height from architectural features, parapets, and building corners.
  - (3) Logos are encouraged if they meet the following criteria and shall be reviewed on a case-by-case basis. Logos shall be included in the calculation of permitted total sign area. Logos shall not be larger in height than 1.5 times the height of an upper-case letter in the business name. Logos independent of the main business identification may be permitted as a separate sign.
  - (4) Graphic emblems (e.g., icons, images consistent with the nature of the business other than corporate logos) are permitted and are included in the maximum sign size requirements set forth in Table 7.H.
- f. Kiosk Signs.** In addition to the standards of Table 7.H, signage on kiosks shall meet the following requirement.
- (1) Kiosk signage shall be mounted or applied to the kiosk structure. Freestanding or monument signs shall not be allowed.
- g. Wall Graphics.** In addition to the standards of Table 7.H, wall graphic posters located on freeway elevations shall meet the following requirements.
- (1) Wall graphic posters shall be approved by the property manager.
  - (2) Wall graphic posters may contain advertising for a specific product or business.
  - (3) Wall graphic posters may be changed periodically. Posters shall be changed if they become faded or in disrepair.
- h. Banners (Pole-Mounted) and Flags.** In addition to the standards of Table 7.H, pole-mounted banners and flags shall meet the following requirements.
- (1) Banners or flags may be used for community events, grand openings, sales, or seasonal decoration.
  - (2) Community event banners or flags may be permitted subject to prior approval by the Community Development Director for a time period not to exceed 14 days.
  - (3) Grand opening and sale banners or flags may be permitted subject to prior approval by the Community Development Director for a time period not to exceed 60 days.
  - (4) Seasonal banners or flags shall be permitted on parking lot light standards during applicable seasonal periods.
  - (5) Banner or flag design, colors, and materials shall be compatible with the architectural design, landscape architecture, and graphic design themes of the project.
  - (6) Banners or flags shall be properly maintained. Torn, frayed, faded, or otherwise damaged banners and flags shall not be permitted and shall be promptly removed by the owner.
- i. Wall Murals.** Wall murals consistent with the architectural design, landscape architecture, and graphic design theme of the Colonies at San Antonio may be allowed, subject to approval by the City of Upland and Colonies Crossroads, Inc., or their designee.

**j. Temporary Signs.** In addition to the standards of Table 7.H, temporary (future tenant or property for lease/sale) signs shall meet the following requirements.

- (1) Temporary signs may be allowed on an individual basis, subject to Community Development Director approval.
- (2) Temporary signs for commercial projects shall include the Colonies Crossroads logo.
- (3) Temporary signs may be double faced, perpendicular to the street; single faced, diagonally facing the intersection; or panels within retail or non-retail monuments.

### **7.7.8 Open Space Signage**

Location and design concepts for Open Space signage, including trail head entry monuments, trail markers, and interpretive signage, are illustrated in Chapter 9.0.

### **7.7.9 Public Facilities Signage**

The type and number of permitted signs and signage development standards shall be as typical for the City of Upland Fire Department. Signs shall be compatible with the architectural style, landscape architecture, and graphic design theme of the Specific Plan area.

### **7.7.10 Residential Signage**

Design concepts for residential entry signs, which are a component of the residential landscape entry concepts, are illustrated in Chapter 9 (Landscaping). The sign text shall be limited to the project name and housing type (e.g., attached luxury housing).

Other residential signs within residential zoning districts of this Specific Plan shall comply with the sign provisions of the Upland Municipal Code Chapter 17.20 (Sign Regulations).

### **7.7.11 Other Signage**

Real estate signs not addressed in Table 7.H, above, shall comply with the City of Upland Municipal Code Section 17.20.100 (Temporary Real Estate Signs).

Political campaign signs shall comply with the City of Upland Municipal Code Section 17.20.120 (Political Campaign Signs - General).

Construction signs shall comply with the City of Upland Municipal Code Section 17.20.130 (Construction Signs).

### **7.7.12 Prohibited or Restricted Signs**

- a. Immoral or Unlawful Advertising.** Exhibiting, posting, displaying or causing to be exhibited, upon any sign, anything of obscene, indecent, or immoral nature or unlawful activity shall be prohibited.
- b. Animated, Audible or Moving Signs.** Signs incorporating moving, swinging, rotating, noise making, flashing, blinking, scintillating, fluctuating, or otherwise animated parts or lights, used to attract attention for the purpose of promoting (either directly or indirectly) the sale of products or identifying a tenant, shall be considered subject to the approval of the Community Development Director.
- c. Outdoor Advertising Structure or Sign.** Signs placed for the purpose of advertising products or services that are not produced, stored, or sold on the property upon which the signs are located shall be prohibited.

- d. Common Area Signs.** No signs are permitted in the common area except as indicated within this Specific Plan and approved per the Sign Program.
- e. Vehicle Signs.** No vehicle, including trailers, shall be parked for the principal purpose of advertising or display (except temporary real estate directional and kiosk signs as provided in Section 9405.071 and 9405.072). Vehicles or other signs or devices on private property or in the public right-of-way, when used as advertising devices or displays, shall be prohibited.
- f. Light Bulb Strings and Exposed Tubing.** External displays that consist of unshielded light bulbs shall be prohibited. Exposed neon or gaseous light tubing and tivoli lighting, when the display is an integral part of the design character of the activity to which it relates, may be approved.
- g. Interior Signs.** Interior signs visible from the exterior, or designed or installed in an externally oriented manner, shall be prohibited.
- h. Roof Signs.** No roof signs or roof-mounted signs shall be permitted unless they are incorporated as an integral part of the design of the building's architecture.
- i. Advertising Devices.** No advertising displays promoting commercial tenants other than those in the Specific Plan area shall be permitted.
- j. Temporary Signs.** "A" frame signs, attraction boards, "human" signs, posters (other than on theaters or freeway elevations as permitted in Table 7.H), balloons, banners (other than pole-mounted), and sandwich board signs (including free standing signs such as "Lotto" signs) shall be prohibited at all times.
- k. Religious Assembly.** Signs for religious assembly uses may only indicate facility or tenant name(s).

## **7.8 DEVELOPMENT STANDARDS FOR SPECIFIC USES**

The following development standards shall govern the construction and operation of specific uses within the Colonies at San Antonio Specific Plan. These specific uses possess distinctive characteristics that require special development standards in order to prevent impacts on other nearby uses. By meeting the following standards, the development approval process can also be streamlined by incorporated detailed regulations into the standard development and operations requirements, thereby eliminating the need for conditional use permits. The following standards shall not replace or reduce any minimum standard established by local, state or other authority; however, where these specific development standards are more restrictive, they shall control.

### **7.8.1 Alcohol Sales**

This section sets forth the development and operational standards for the sale of alcoholic beverages.

- The following conditions shall apply to any commercial/retail operation selling alcoholic beverages of any kind:
  - The sale of any manner of alcoholic beverage shall comply with all applicable regulations of the State of California Department of Alcohol Beverage Control (ABC).
  - The operator of the use shall prevent loitering in any parking area serving the use.
  - The parking lot of the premises shall be lighted to a minimum level of one footcandle at ground level.
  - The premises shall be kept free of graffiti and litter. Any graffiti occurring on the premises shall be removed within 48 hours.
  - The rear doors of the establishment shall be kept closed at all times except to permit deliveries.

- The establishment shall have a security management plan that details the means by which on-site security measures shall be conducted and that identified responsible person(s) to be contacted in case of an emergency.
- In the event security problems occur, at the reasonable request of the Police Chief, the establishment shall provide California licensed, uniformed security guard(s) on the premises during such hours as requested by the Police Chief.
- For off-sale establishments, a silent robbery alarm shall be required.
- For off-sale establishments, the cash register shall be bolted to a stationary counter, visible from the outside of the premises, and located in close proximity to the public entrance.
- For off-sale establishments, alcoholic beverages shall be displayed in one general location and merchandise shall be arranged within the establishment to permit eye level visibility throughout the public areas of the establishment.
- Employees selling alcoholic beverages between the hours of 10:00 p.m. and 2:00 a.m. shall be at least 21 years of age.
- Such facilities shall not be situated in a manner that vehicle traffic from the facility may reasonably be believed to be a potential hazard to a school, church, public park or playground.
- The owner and management at each location shall educate the public regarding driving under the influence of intoxicating beverages, minimum age for purchase and consumption of alcoholic beverages, driving with open containers and the penalty associated with violations of these laws. In addition, the owner and management shall provide health warning about the consumption of alcoholic beverages. This requirement may be met by posting prominent signs, decals, or brochures at the point of purchase. The owner and management shall provide adequate training for all employees at the location as to these matters.
- Cold beer or wine shall be sold from or displayed in permanently affixed electrical coolers only.
- For on-sale establishments, the sale of alcoholic beverages shall be incidental to the sale and consumption of food.
- No sale of alcoholic beverages shall be made from a drive-through window.
- No lighted advertising for beer, wine or other alcoholic beverages shall be located on the exterior of buildings or within window areas.
- Any window signs shall be placed and maintained in a manner so that there is a clear and unobstructed view of the interior of the premises from the entrance.
- Public telephones shall not be allowed on the exterior of the establishment. Public telephones inside the establishment shall be equipped to prevent incoming calls.

### **7.8.2 Amusement Centers and Arcades**

This section sets forth the development and operational standards for amusement centers and arcades. “Amusement Center” is defined as any structure (or portion thereof) that houses an arcade, laser tag, “midway” style games, rides or similar uses. Sales of prepared foods and beverages is permitted ancillary to the primary use.

“Arcade” is defined as any structure (or portion thereof) in which four or more amusement devices (either coin- or card-operated) are installed, such as photography machines, video games, muscle testers, fortune telling machines, and other games of skill or science, but not including games of chance or other similar devices. Included is any place open to the public, whether or not the primary use of the premises is devoted to operation of such devices. Sales of prepared foods and beverages is permitted ancillary to the primary use.

- a. General Development Standards.** The following general standards shall apply to all amusement center and arcades within the Colonies at San Antonio Specific Plan.
- Where they share a common space with other uses, an arcade and/or amusement center shall be located within a space separate from other on-site uses and designed to prevent excessive noise, glare or other offensive factors from affecting other uses in the immediate vicinity.
  - The arcade/amusement center shall be designed and arranged so that a management attendant or designated representative can properly supervise and the arcade/amusement center at all times.
  - Adequate space shall be provided to allow the use of each machine and unimpaired access throughout the arcade without overcrowding. Emergency entry and egress points shall be maintained in accordance to applicable fire and safety guidelines.
- b. Maintenance and Operational Standards.** Amusement center and arcades within the Colonies at San Antonio Specific Plan shall be maintained and operated in compliance with the following minimum standards.
- The owner/operator of any structure where an arcade/amusement center is located shall provide sufficient security measures to effectively regulate interior and exterior loitering, parking congestion, disturbing noise and light, and criminal activity.
  - No person, shall operate, play or utilize any amusement machine game or device for the purpose of gambling, betting, wagering or pledging in any manner any money, thing or considerations of value, upon the outcome, score or result of the playing or operation of said amusement machine, game or device.
  - Arcades and/or amusement centers which cater to minors shall install security equipment or implement security procedures which provide adequate safeguards to patrons. Such safeguards may include, but will not be limited to: security cameras and “kid checks” at entrances/exits (to ensure children do not leave unattended or with strangers).
- c. Food and Beverage Sales.** Arcades and amusement centers which provide food and beverage service shall conform to all applicable City and State standards as well as applicable provisions of this Specific Plan.
- d. Alcohol Sales.** In addition to adhering to applicable local and/or State regulations governing the sale of alcohol, arcades and amusement centers which serve alcoholic beverages (e.g., Gameworks, Dave & Buster’s or similar establishments) shall adhere to the following standards.
- Minors (persons younger than 21) shall be permitted in an arcade/amusement center which serves alcoholic beverages only when they are accompanied by a parent or guardian who is 25 or older. No more than three (3) minors shall accompany each parent/guardian.
  - No minor shall be permitted within an arcade/amusement center which sells alcoholic beverages after 11:00 P.M.
  - Parents/guardians shall closely supervise minors at all times. The operator/staff of any such arcade/amusement center shall police the premises to ensure minors are not left unattended.
  - Unattended minors shall be removed from the arcade/amusement center and/or detained by staff until notification of parents/guardians. Each such arcade/amusement center shall install an indoor public address system to notify parents/guardians of unattended minors.

### 7.8.3 Animal Care Facilities

This section sets forth the development and operational standards for animal care facilities. Activities typically include the care, treatment, and interim sheltering of domesticated animals. Typical uses may include veterinarian offices, animal hospitals, and animal grooming salons. This use includes immunization clinics, grooming services, adoption services, and the keeping of domesticated animals for retail sale. Not included in this land use are breeding facilities and long-term kennels.

In addition to provisions of this Specific Plan, animal care facilities shall adhere to all applicable local and State regulations. Provisions of this Specific Plan shall not supersede local or State standards.

- Animal care facilities shall not directly adjoin or abut a residential use.
- The maximum hours of operation for non-emergency animal care facilities shall occur between 6:00 A.M. and 10:00 P.M. Twenty-four hour animal medical services shall not be permitted within the Specific Plan area, unless a Conditional Use Permit is approved.
- With the exception of temporary outdoor events, animals permitted within this use shall be limited to domesticated dogs and cats, birds (excluding adult domesticated fowl), reptiles (snakes, lizards, turtles and tortoises), amphibians, rodents (rats, mice, hamsters, guinea pigs, gerbils, and similar species), rabbits and hares, fish, crustaceans, arachnids and insects, and feed animals (vertebrate and invertebrate animals which may be purchased as live pet food). Animals not permitted within this use include (but are not limited to): horses or related species, cows or related species, goats, sheep, swine, or animals that are common to zoos.
- No animal shall be allowed to roam freely. Animals shall remain caged, penned, leashed, under the direct control of their owner or care giver, or otherwise restrained at all times.
- All temporary or pre-sale animal lodging areas shall be located within an enclosed structure. An adequate (species specific) amount of light, ventilation, heat (or cold), water, food, or other physical necessity shall be provided to each cage, pen, tank or enclosure.
- Outdoor exercise areas (if required) shall be surrounded by a wall, fence, or other barrier of a design, size and composition adequate to prevent the escape of animals from or the unauthorized entry of persons into such area.
- All animal enclosures including, but not limited to cages, pens, and tanks, shall be maintained free from litter, garbage and the accumulations of animal waste. All animal enclosures shall be maintained in a manner which limits the proliferation of disease vectors, and offensive odors.
- Animal feed shall be stored and utilized in a manner that will not encourage the proliferation of vertebrate and invertebrate pests. The operator of any animal care facility shall take adequate and appropriate measures to control the proliferation of vertebrate and invertebrate pests and/or parasites including but not limited to: fleas, ticks, ants, roaches, flies, mosquitoes, rats, and mice. The storage, application, and disposal of pesticides, insecticides, rodenticides and other commonly used pest control substances shall be subject to all applicable federal, state and local standards.
- Structures housing animal care facilities shall be constructed so that animal noise is not audible beyond the limits of structure and/or property line.
- Animal care facilities shall be maintained in a manner which odors are not detectable beyond the limits of the structure and/or property line.
- Animal waste shall not be disposed in municipal waste collection receptacles.
- Dead animals shall not be disposed in municipal waste collection receptacles. Dead animals shall be stored in a secure and odor-resistant receptacle until such time as it is collected for off-site incineration and/or disposal.

#### **7.8.4 Automotive Facilities: Car Washes**

Activities typically include the washing and polishing of automobiles. Uses typically include automobile laundries; car washes, self service washes; and automotive detailing. The following standards shall apply:

- Car washes located within commercial centers shall have an architectural style consistent with the theme established for the center.
- Car cleaning facilities shall only be located in commercial areas of the Specific Plan, and may be stand-alone facilities or co-located with automotive gas stations, light vehicle maintenance facilities, and/or vehicle sales outlets.
- Facilities shall be designed, constructed and maintained in a manner which minimizes the use of water.
- All vehicle wash facilities shall include devices, equipment, or structures which intercept wash water which may contain contaminants. Such a system shall collect water and separate contaminants for proper disposal prior to the conveyance of water into the local drainage systems.
- Except for automated vehicle washing facilities, such as those commonly provided at automotive service stations, queuing lanes shall provide at least 120 feet of reservoir space for the stacking of vehicles, as measured from the car wash structure to the entry point into the car wash facility.
- The design and locations of the car wash and necessary queuing lanes and/or detailing areas shall not block and/or interfere with traffic or increase congestion on public streets or private property adjacent to the facility.
- For automated vehicle washes, the number of vehicle drying areas, each minimally sized 12 feet by 20 feet, shall equal two times the maximum capacity of the vehicle wash. For non-automated vehicle washes, two drying areas, minimally sized 12 by 20 feet, shall be provided for each washing bay. Drying areas shall not interfere with the normal flow of traffic on-site or on adjacent roadways.
- An adequate number of waste receptacles shall be provided at self-service car auto washes. Trash receptacles shall be located adjacent to “vacuum stations” and shall be adequately screened. Areas around trash receptacles shall be maintained in a neat and sanitary manner.
- No on-site storage of inoperable vehicles shall be permitted.
- Vehicle wash facilities may include retail sales within an enclosed building, including but not limited to the sale of precooked or prepared food and beverages, packaged food and beverages, convenience items, automobile accessories, and other sundries.
- Outdoor public address systems are not permitted in Planning Areas 17, 18, or 19.
- Adult magazines shall be kept in racks providing for the shielding of cover pages and be individually wrapped in plastic shrink-wrap or bags. Adult magazines shall only be accessible to store employees.
- Loitering shall not be permitted on site.

#### **7.8.5 Automotive Facilities: Gas Stations**

Activities typically include, but are not limited to the sale from the premises of goods and the provision of services normally required in the daily operation of motor vehicles, including the principal sale of petroleum products, the incidental sale of replacement items, and the performance of minor repairs. Also included is the washing of automobiles and light trucks. The sale of prepared foods and beverages and sundries is a permitted ancillary use. The following standards shall apply.

- Automotive gas stations within consolidated commercial centers shall have an architectural style consistent with the theme established for the center in which they are located.

- In order to provide for adequate vehicular circulation on-site, pump islands shall have at least a 25 foot separation from any building or car wash facility, as measured from the pump island curb to the building or building curb, if provided.
- Automotive gas stations shall have a minimum of 150 feet of separation between on-site driveways. Driveways shall be a minimum width of 35 feet and a maximum width of 48 feet. Driveways shall not be located closer than 50 feet from a street intersection (measured from the beginning and end of the curb return), or 10 feet from an interior side property line. Under no circumstance shall a driveway be permitted to interfere with the movement and safety of vehicular and pedestrian traffic. Gas stations may be accessed from a shared driveway, if the above standards are met.
- Landscaped areas shall be provided in the following locations:
  - 10 feet adjacent to street side lot lines
  - -Adjacent to street corners and corner cut off areas as defined in Upland Municipal Code Section 17.18.060-
  - 5 feet adjacent to the interior perimeter of required parking areas.
- All landscaped areas shall be planted with living plant materials, provided with permanent and automatic irrigation systems, and maintained in a healthy and thriving condition. Landscaping shall be enclosed by a raised curb of not less than 6 inches.
- All repair or maintenance areas, washing areas, or other service uses shall be entirely enclosed within a building. Vehicle service bays shall either be oriented away from public view, or screened from public view by a 6-foot high decorative wall and landscape screen.
- Detached buildings housing accessory uses, such as car wash facilities, shall have a separation distance as required by the Uniform Building Code.
- All outdoor storage areas shall be enclosed on at least three sides by a solid 6-foot high decorative wall and landscape screen. Any opening in such an enclosure shall be provided with a decorative steel view obscuring gate (chain link prohibited). No storage shall be elevated above the top of the required wall.
- Each pump island may include computerized payment stations. Such stations shall be situated in a manner that will not cause interference with circulation or the sale of motor fuels.
- For each 20 lineal feet of pump island fascia that is served by a nozzle, one parking stall shall be deducted from the required parking for a convenience store.
- Automotive gas stations shall be provided with adequate evening/night lighting to provide a safe and secure environment for employees and patrons. All on-site lighting shall be designed, arranged and controlled to avoid excessive light and glare on adjacent properties and/or public rights-of-way.
- Personnel security measures satisfactory to the City of Upland Police Department including, but not limited to: bulletproof windows and cashier booths, surveillance cameras, and alarm systems shall be installed at automotive gas stations.
- Where operations encourage or require pedestrian traffic, the circulation system shall be designed so as to minimize conflict between pedestrian and vehicle traffic.
- All vehicle parking and loading areas shall conform to applicable provisions of this Specific Plan. All parking spaces shall be located on-site and adjacent to landscaped planters. No parking shall impede or interfere with vehicle flow or visibility, nor be located in any corner cutoff area. No parking space shall overhang more than 2.5 feet into any required planter area.
- All on-site loading and/or truck maneuvering areas shall be designed so as to safely accommodate fuel supply vehicles, other delivery vehicles, and emergency vehicles.
- Merchandise, wares and crates, in the form of storage or displays, shall remain within the confines of the building.

- The premises shall be maintained in a neat and orderly condition at all times. Structures shall be maintained in a condition of good repair and appearance.
- Storage of used or discarded automotive or truck parts or equipment or permanently disabled, junked, or wrecked vehicles shall not be visible from a public street.
- Vehicles that are in the process of being serviced and/or repaired shall be parked in an enclosed structure or within a clearly marked staging area. Outside staging areas shall be sufficiently screened from public view and be located in a manner that does not interfere with normal flow of on-site traffic. Parking spaces within any such staging area shall not count towards the number of spaces required by this Specific Plan. Staging area(s) shall be maintained in a neat and orderly manner.
- All repair and service activities shall be conducted entirely within an enclosed building, except as follows:
  - The dispensing of petroleum products, water, oil, and air from pump islands.
  - Trash areas, enclosed by walls, to be integrated with the design of the gas station.
  - Public telephones, provided that they are well lighted, are in a location that is visible from a public street, and are accessible on a 24-hour basis. Public telephones shall permit outgoing calls only.
- Automotive gas stations shall provide men's and women's public rest rooms that are accessible to the general public, including physically disabled persons, during all hours that the station is open to the public.
- At least one air pump and water supply area shall be provided at each automotive gas station, and shall be located such that its use will not conflict with on-site vehicular movement. Charges, if any, for air and water shall be governed by the provisions of State law.
- Automotive gas stations may include ancillary food services and retail sales within an enclosed building, including but not limited to the sale of precooked or prepared food and beverages, the sale of packaged food and beverages, convenience items and other sundries. The incidental sale of beer and wine from automotive gas stations for off-site consumption shall be allowed for sales areas at least 2,500 square feet in size, as conditioned by the Police Department.
- Outdoor public address systems shall be prohibited, except for communication devices as required by the Americans with Disabilities Act or other State building codes.

#### **7.8.6 Automotive Facilities: Repair and Customization Services**

Activities include, but are not limited to, minor vehicle repair and maintenance, and the retail sales of goods and services for vehicles. Uses typically include, but are not limited to, brake and muffler shops, tire shops, tune and lube shops, the installation of accessories and electronics, and the installation of major accessories and automobile customizing. Vehicle services shall conform to the following standards:

- Vehicle service facilities located within commercial centers shall have an architectural style consistent with the theme established for the center.
- All vehicle service facilities shall include devices, equipment, or structures which intercept storm water which may contain contaminants. Such a system shall collect water and separate contaminants prior to the conveyance of water into the local drainage systems.
- Buildings housing vehicle service bays or facilities shall either be oriented so that openings do not face public streets or be screened by a solid opaque wall and/or adequate landscaping. Any such screening treatment shall adhere to applicable provisions of this Specific Plan.
- The premises shall be kept in a neat and orderly condition at all times. Structures shall be maintained in a condition of good repair and appearance. Landscaping shall be maintained in a healthy condition.

- No used or discarded vehicle parts or equipment or permanently disabled, junked, or wrecked vehicles may be stored outside of an enclosed building or stored within an enclosed building so as to be visible from public view.
- Vehicles retained overnight for service shall be parked in an enclosed structure or within a clearly marked staging area. Outside staging areas shall be sufficiently screened from public view and be located in a manner which does not interfere with normal flow of on-site traffic. Parking spaces within any such staging area shall not count towards the number of spaces required by Upland Municipal Code or this Specific Plan. Staging area(s) shall be maintained in a neat and orderly manner.
- Public telephones may be provided in an exterior location, provided that they are well lighted, are in a location that is visible from a public street, and are accessible on a 24-hour basis. Public telephones shall permit outgoing calls only.

### **7.8.7 Automotive Facilities: Vehicle Sales**

Activities typically include, but are not limited to, the retail sale of predominantly new, used, or vintage vehicles together with their maintenance. Typical uses include, but are not limited to, automobile, motorcycle, watercraft, travel trailer, and recreational vehicles sales agencies. Typical accessory uses include, but are not necessarily limited to, storage, washing, detailing, preparation, body work, painting, installation of major accessories and repair of vehicles; administrative and finance offices; retail sales of parts and accessories; and automobile rental. Vehicle sales uses shall conform to the following standards:

- Except for structures and landscaping, the site shall be entirely paved, so that vehicles are not parked in a dirt or otherwise unimproved area.
- All landscaping shall be installed and maintained pursuant to applicable provisions of this Specific Plan. Auto display areas shall not be required to meet the landscaping requirements applicable to parking lots.
- Service areas and vehicle storage areas shall be screened from adjoining properties and public right-of-ways.
- Vehicle painting and repair facilities and parking areas shall include devices, equipment, or structures which intercept storm water which may contain contaminants. Such a system shall collect water and separate contaminants prior to the conveyance of water into the local drainage systems.
- Loading and unloading of vehicles shall not occur within the travel lanes of adjoining streets.
- All vehicles associated with the use shall be parked or stored on-site and not on adjoining streets.
- An adequate on-site queuing area for service customers shall be provided. Parking spaces required pursuant to provisions of this Specific Plan shall not be counted as queuing spaces, and vice-versa.
- No vehicle service or repair work shall occur except within a fully enclosed structure. Service bays shall not directly face or front on a public right-of-way.
- Vehicles retained on-site for painting and/or repair shall be parked in an enclosed structure or within a clearly marked staging area. Outside staging areas shall be sufficiently screened from public view and be located in a manner that does not interfere with normal flow of on-site traffic. Parking spaces within a staging area shall not count towards the number of spaces required by the Upland Municipal Code or this Specific Plan.
- Public telephones may be provided in an exterior location, provided that they are well lighted, are in a location that is visible from a public street, and are accessible on a 24-hour basis. Public telephones shall permit outgoing calls only.

### **7.8.8 Communication Services**

Activities typically include, but are not limited to, broadcasting and other information relay services for commercial and personal uses. Typical uses include but are not limited to monopoles, antennas, rooftop dishes, amateur radio communication facilities, and citizen band (CB) radio antennas. The following provisions shall apply:

#### **a. Communication Facilities for Commercial Uses**

- Communication equipment (i.e., monopoles, antennas, dishes) shall comply with the provisions of the City of Upland Municipal Code Section 17.30.060 (Antenna Permit Applications), and shall require environmental and design review.
- Communication equipment shall not be located within required front or side setback areas or within 5 feet of any property line. Communication equipment mounted within freeway pylon signs shall be permitted and shall be exempt from setback requirements.
- Commercial building mounted communication equipment shall be located and designed so as to limit its visibility from the ground or from nearby buildings.
- Ground mounted communication equipment shall not exceed 25 feet in height.
- Communication equipment and supporting structures shall be painted a single, neutral, non-glossy color (e.g., earth tones, black, gray, etc.). (A freeway pylon sign mounted with communication equipment shall not be considered a supporting structure.)
- Wiring shall be placed underground and obscured from view whenever possible.
- Whenever possible, proposed antenna sites should be located away from surrounding sensitive uses such as residential and schools. Numerous antenna sites exist throughout the city, including artificial palm and pine trees, flag poles, a fake 4th floor and a three-story building. The Community Development staff can assist you in locating these existing sites.
- The antenna should include a proposal for concealment through use of an architectural quality structure, artificial pine or palm tree, or other appropriate “stealth” approach. Alternatives for the appearance should be provided through photographs of similar sites, drawings, and photo simulations.
- Co-location of commercial communication towers is encouraged.

#### **b. Communication Facilities for Residential Uses**

- Communication equipment shall not be located within required front or side setback areas or within 5 feet of any property line.
- Residential building mounted communication equipment shall not exceed 10 feet in height above the highest point of the roof. Residential ground mounted communication equipment shall not exceed the height of the peak roof line of the closest building or residential structure. Antennas and dishes are not to be strapped onto chimneys, but are to be secured in the most inconspicuous location feasible.

### **7.8.9 Day Care Facilities**

This section sets forth the development and operational standards for day care centers and childcare facilities. These uses shall be defined as facilities which provide child care services in nonresidential areas. The provision of in-home child care services within residential areas shall be permitted subject to applicable local and state regulations. Activities typically include, but are not limited to the day time, temporary care of pre-school children and the daytime, after-school care of elementary school aged children. Uses typically include, but are not limited to, pre-schools and day care facilities.

- The facility shall conform to all property development standards of the planning area in which it is located.
- Childcare facilities shall operate between the hours of 6:00 A.M. to 10:00 P.M. No overnight care of children shall be permitted.
- An outdoor play area of no less than 75 square feet per child, but in no case less than 450 square feet in area shall be provided. At least 50 percent of this area shall be covered with turf, sand or other natural materials.
- A 6-foot high wall or decorative steel fence shall be constructed around any outdoor play area or other area where children may gather. In addition to conforming to applicable provisions of this Specific Plan, any such fence shall be constructed to limit the passage of materials through or over the fence/wall.
- Play areas shall be located so as to be shielded from freeway noise.
- Parking shall be provided pursuant to applicable sections of this Specific Plan. If deemed necessary by the City of Upland Community Development Department, on-site vehicle turnaround, separate entrance-exit points, or passenger loading areas shall be provided.
- The design of any childcare facility shall be reviewed and approved by the City of Upland Fire Department to ensure adequate emergency access. The childcare facility shall install and maintain smoke detectors, carbon monoxide detectors, fire sprinklers, fire extinguishers, and/or other equipment required by the Upland Fire Department.

#### **7.8.10 Drive-Through Sales and Service**

Activities typically include, but are not limited to, the retail sale from the premises of food or beverages for off-site consumption and the provision of services. Uses typically include, but are not limited to, drive-through restaurants, drive-through dairies, drive-through food windows at gas stations, drive-through windows at drug stores, drive-through video rental, drive-up remote tellers and drive-up ATM machines at financial institutions, and drive-up photographic processing. Drive-through facilities and drive-up windows within the project site shall conform to the following standards:

##### **a. General Standards**

- Drive-through facilities within consolidated commercial centers shall have an architectural style consistent with the theme established for the center and its corporate identity.
- The design and locations of the facility and queuing lane shall not contribute to increased congestion on public streets or private property adjacent to the facility.
- The design and location of the facility and queuing lane shall not impede access to or exit from project area parking facilities.

##### **b. Specific Design Requirements**

- Drive-through windows and drive-up remote tellers/ATMs shall provide at least 120 feet of reservoir space for the stacking of vehicles as measured from the service window or unit to the entry point into the drive-up lane.
- Drive-through windows and drive-up remote tellers/ATMs shall be separated from commercial drive aisles which provide direct access to a public street. A minimum 25 feet of travel distance shall be provided for drivers after leaving a drive-through window or remote teller/ATM before they enter a drive aisle that provides direct access to a public street.
- Pedestrian paths shall be maintained in such manner to allow the safe and convenient passage of persons across drive aisles. Pedestrian pathways shall be clearly marked with paint and/or enhanced paving materials and designated with adequate and appropriate signage. Pedestrian

pathways adjacent to drive aisles shall be separated by a raised curb, planter, berm, or other device to ensure a safe and adequate separation between pedestrians and motor vehicles.

- Order board speakers or intercom systems shall be installed and maintained in a manner which diminishes and/or directs noise away from adjacent uses and public rights-of-way.
- Landscaping shall be installed to screen drive-through aisles from primary circulation routes and shall be used to minimize the visual impact of order boards and directional signs.

#### **c. Operating Standards**

- Drive aisles, landscaped areas, play areas, and parking facilities associated with drive through facilities shall be maintained in a neat and orderly manner.
- Drive-through facilities shall not constitute a nuisance to the Specific Plan area and/or adjacent uses due to noise, litter, loitering, smoke or odor.
- Order board speakers and/or intercom systems shall be operated in a manner which does not exceed standards stated in the City's Noise ordinance or applicable sections of this Specific Plan.

#### **7.8.11 Eating and Drinking Establishments, Outdoor Dining**

Activities typically include, but are not limited to, the retail sale from the premises of food or beverages prepared for on-premises consumption. Uses typically include, but are not limited to, restaurants, fast food outlets (including drive-throughs), delicatessens, ice cream and frozen yogurt shops, cafes, candy and confectionaries shops (including soda fountains), delicatessens, donut shops, and coffee sales. Eating and drinking establishments shall conform to the following standards:

- The operator shall take adequate and appropriate measures to prevent loitering in any parking area serving the use.
- Outdoor dining areas are permitted subject to the following requirements:
  - The outdoor dining area shall be conducted adjacent to a legally operated eating and/or drinking establishment.
  - The outdoor dining area shall not block access to contiguous properties or obstruct emergency access to any property.
  - The hours of operation shall be limited to the hours of operation of the associated eating and drinking establishment.
  - No sound amplification device, musical instrument or sound reproduction device shall be operated or used within an outdoor dining area unless approved through a Conditional Use Permit.
  - Outdoor dining areas which serve alcohol shall be surrounded by a wall, decorative steel fence, landscaping, or other feature, no less than 3 feet in height, which limits access to outdoor dining areas.
  - Tables, chairs, picnic benches or other outdoor dining furniture shall be utilitarian in design and constructed from vandal resistant materials. Outdoor dining furniture shall not encroach into drive aisles, parking spaces, emergency accesses, pedestrian walkways or landscaped areas.
  - The operator of any outdoor dining areas shall take adequate and appropriate measures to control the proliferation of vertebrate and invertebrate pests including but not limited to: ants, roaches, flies, mosquitoes, rats, mice, and crows. The storage, application, and disposal of pesticides, insecticides, rodenticides and other pest control substances shall be subject to all applicable federal, state and local standards.
  - The use of retractable fire resistant awnings is permitted provided that they maintain a minimum unobstructed vertical clearance of 8 feet above sidewalk level, and that they provide coverage of

the dining area only. A horizontal 4-foot unobstructed clearance must be maintained for the public walkway. Portable umbrellas may be permitted provided that they do not obstruct the public right-of-way or walkway and they do not contain advertising. In buildings with fire sprinklers, awnings will also need to be protected by fire sprinklers if they exceed 4 feet in width.

- Any lighting which may be incorporated into the facade of the building must be approved through design review. Lighting shall compliment the architectural style of the building. Lights on the building shall not be oriented so as to direct glare or other visual obstruction to pedestrians or vehicle drivers along the street and public walkway and should illuminate only the sidewalk café area. Portable table lamps or candles are encouraged. An electrical permit must be issued by the city prior to installation of outdoor lighting. Candles or other open flame devices must meet the Uniform Fire Code requirements.
- Design materials and colors used for chairs, tables, lighting and other fixtures, including umbrellas and awnings, shall compliment the architectural style and colors used on the adjacent building facades.
- The outdoor dining area shall not adversely affect nor be detrimental to contiguous property owner, their patrons, employees or other persons who may be visiting or working in the area.
- The sale of alcoholic beverages shall adhere to ABC and City of Upland regulations and applicable provisions of this Specific Plan.
- Loading and trash areas shall be clearly designated, and shall not be located at drive-through areas.

#### **7.8.12 Health Clubs and Spas**

Activities typically include, but are not limited to, sports and health-related activities performed either indoors or outdoors. Uses typically include, but are not limited to, health clubs, spas, beauty spas, tanning salons, gyms, racquet clubs and tennis clubs. These uses shall conform to the following standards:

##### **a. Locker Facilities**

- Health and fitness facilities shall provide separate locker, changing and shower facilities for male and female clients.

##### **b. Pools**

- Any pool or other structure or device which utilizes water in which there will be human contact shall comply with all applicable water quality standards stated in Title 22 of the California State Health and Safety Code, Chapter 20 (Public Swimming Pools).
- Exterior swimming pools shall be enclosed with a decorative steel security fence or wall at least 6 feet high, with entry through a controlled gate or turnstile to prevent unsupervised access by children.

##### **c. Outdoor Game Courts**

- Outdoor game courts shall be located no closer than 5 feet from any property line if the court is fenced, and 3 feet from any side yard property line if the court is unfenced.
- Fencing for outdoor game courts shall not exceed 10 feet in height as measured from the finished court surface. This fencing shall be no closer than 5 feet from any side or rear property line. Fences, if provided, shall be composed of wire mesh or steel mesh capable of admitting at 90 percent of light as measured on a reputable light meter. Court areas fenced with such materials shall be screened from public view by walls, landscaping or a combination thereof.
- Lighting of outdoor courts shall be permitted. A maximum of eight lighting fixtures, not to exceed 22 feet in height from the finished grade of the court, and no closer than 10 feet to a side or rear property shall be permitted.

- Lighting and lighting fixtures shall be designed, constructed, mounted, and maintained so that, the maximum illumination intensity measured at the wall of any adjacent use shall not exceed ½ foot candle above ambient levels, and the light source is cut-off when viewed from any point above 5 feet measured outside of the lot at the lot line.
- Use of court lighting shall be prohibited between the hours 10:00 P.M. and 6:00 A.M. Hours may be extended with the approval of a Conditional Use Permit.

**d. Massage Services**

- Massage services provided at permitted health clubs and spas shall comply with the provisions of the Upland Municipal Code.

**7.8.13 Hotels and Motels**

This section sets forth the development and operational standards for hotels, motels, and similar uses.

- Any hotel/motel use shall be adequately buffered from any incompatible adjoining use.
- The density of rooms, intensity of facilities, and scale and design of buildings shall be harmonious with surrounding uses and development.
- Hotels/motels shall not be converted to long-term (exceeding 30 days) residences, nor shall any hotel/motel offer short-term (hourly) occupancy.
- Where appropriate, hotel/motels shall provide retail/service outlets at pedestrian levels.
- All parking designs shall provide through-flow circulations or maneuvering space in a cul-de-sac or “hammerhead” design to allow exiting in a forward direction when all parking spaces are full.
- Each hotel/motel shall include a reception area (lobby), office space for administrative use, service areas and facilities for employees (such as a lounge, lockers and showers), and laundry facilities for use by the hotel/motel.
- Hotel/motels shall provide adequate and appropriate security measures to ensure the safety of guests and staff and to prevent loitering, trespassing, and criminal activity. Such security measures shall include surveillance of arrivals and departures, and parking areas which can be monitored from office/front desk. The design and installation of any such security system including but not limited to: cameras, alarms, and lighting shall be submitted to the City of Upland Police Department for review and approval.

**7.8.14 Indoor Recreational Facilities**

Activities typically include, but are not limited to, bowling alleys, billiard/pool halls, ice and roller rinks, indoor skate parks, indoor basketball and racquet courts, and similar uses. The preparation and sale of food and beverages is permitted ancillary to the primary use.

- Activities at any such uses shall be confined within enclosed structures.
- The owner/operator of any structure where an indoor recreational use is located shall provide sufficient security measures to effectively regulate interior and exterior loitering, parking congestion, and criminal activity.
- The sale of alcoholic beverages shall be subject to all applicable State and local standards, including applicable provisions of this Specific Plan.

### 7.8.15 Kiosks and ATMs

This section sets forth the development and operational standards for locksmith/key; photographic processing, specialty retail, and prepared food/coffee kiosks; reverse vending machines; freestanding Automatic Teller Machines (ATMs); and similar uses.

- The size of any such structure shall not exceed 400 square feet. Such structures shall be no more than 15 feet in height. These standards may be increased with the approval of a Conditional Use Permit.
- If drive-through facilities are provided, a minimum of 40 feet of queuing distance shall be maintained between the kiosk/ATM and the nearest public street.
- Kiosks and ATMs shall be constructed in a manner which complements the design theme for the area in which it is located.
- Kiosks and ATMs shall provide adequate protection from the elements for patrons.
- Kiosks and ATMs shall be constructed of weatherproof, vandal resistant materials.
- Kiosks and ATMs shall be located in areas which provide convenient vehicular and pedestrian access. The placement of ATM shelters shall not hinder or otherwise impact vehicular and/or pedestrian access.
- Pedestrian and bicycle access to kiosks and freestanding ATMs should not intersect with drive-thru access, but where such an intersection cannot be reasonably avoided, they shall have clear visibility, and must be emphasized by enhanced paving or striping.
- Lighting at kiosks and freestanding ATMs shall be designed and installed in a manner as to provide illumination sufficient for a secure nighttime environment.
- Security measures (surveillance cameras, lighting, etc.) shall be permitted in/around kiosks and freestanding ATMs.
- Trash receptacles shall be provided in the vicinity of kiosks and freestanding ATMs. These receptacles shall be maintained in a neat and clean condition, and shall be emptied on a regular basis.

### 7.8.16 Live Entertainment

Activities consist of *ancillary* uses typically including, but not limited to, live music, amplified vocal or instrumental sounds, recorded music played by a disc jockey, karaoke, and song or dance acts (excluding adult business uses), which occur more than three times per calendar year *within an enclosed building*. Live entertainment uses shall conform to the following standards:

- Establishments featuring live entertainment shall obtain, and abide by applicable provisions of, a City of Upland Live Entertainment Permit as specified in the Upland Municipal Code, Public Peace and Welfare, Title 9.
- Live entertainment shall cease operation between 2:00 a.m. and 9:00 a.m.
- The establishment shall provide adequate staffing, management, and supervisory techniques to prevent loitering, unruliness, and boisterous activities of the patrons outside the establishment and in the parking area.
- The exterior of the premise shall be maintained in a neat and clean manner, and maintained free of graffiti at all times.
- Noise emanating from the entertainment venue shall be within the limitations prescribed by the noise performance standards of this Specific Plan (see Section 7.9.7 Noise) and shall not create a nuisance to surrounding uses.

- If entertainment is to include amplified live entertainment, an acoustical study for the building confirming that the amplified entertainment will meet applicable Specific Plan and City standards shall be reviewed and approved by the Community Development Director. The entertainment venue shall be responsible for implementing the measures identified in the acoustical study.
- For live entertainment that can reasonably be expected to draw more than 125 observers, the establishment must have an approved security plan on file with the Police Department, and comply with such plan.
- All exterior glass windows or doors shall be equipped with double-pane glass, or a comparable substitute with equivalent sound dampening properties, to the satisfaction of the Community Development Director.
- The entertainment venue shall be responsible for maintaining music/entertainment volumes at reasonable levels.
- During any live entertainment, the exterior doors (and windows) of the entertainment venue shall remain closed. The building shall be equipped with an operable air conditioning system.

### **7.8.17 Medical Services and Support**

Activities typically include the provision of diagnostic, medical, therapeutic and emergency transport services. Uses typically include doctor, psychologist, dentist and chiropractic offices, ambulance dispatch centers, physical therapy centers, clinics, pharmacies, and related uses. The ancillary sale and/or rental of medical/hospital equipment is permitted. These uses shall conform to the following standards:

- The transport, use, storage, and/or disposal of tissue, blood or blood products, bodily fluids, medical waste and/or any equipment, linen, paper or other product which has come into contact with such substances, shall be subject to applicable local, state and federal regulations and standards. The on-site incineration of such materials is not permitted.
- Ambulance parking areas at ambulance dispatch centers shall be surrounded by wall, landscaping or combination thereof. Any such wall or landscaping shall conform to applicable provisions of this Specific Plan.
- Access to/from ambulance dispatch centers shall be designed and maintained so as not interfere with the vehicular/pedestrian circulation.

### **7.8.18 Mixed Use Commercial and Residential**

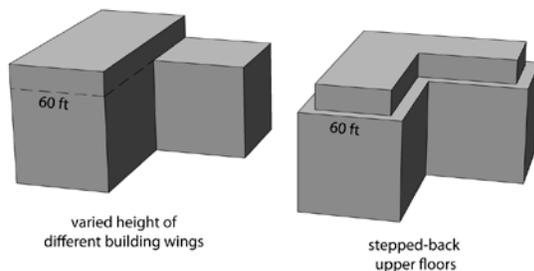
This section sets forth the development and operational standards for mixed use commercial and residential uses.

- At least 20 percent of the net site acreage shall be devoted to common open space.
- Common open space serving residential uses shall be improved as a usable area with turf or outdoor recreational facilities.
- Roofed structures may occupy a maximum of 50 percent of the common open space area; however, no enclosed buildings shall be counted towards the required common open space area.
- Rooftop common space may be provided to count toward the open space requirement. If rooftop common space is not provided, then at least 50 square feet of useable private exterior open space shall be provided adjacent to each residential dwelling unit, with a minimum dimension of 4 feet.
- In the event that residential and commercial uses are mixed on site, NC commercial standards shall apply to the commercial buildings, and mixed use standards shall apply to the residential buildings.

- In the event that residential and commercial uses are mixed within a building, mixed use standards shall be used, and a parking study will be required.

Massing of buildings shall be articulated as follows:

- Façades shall be composed of increments of 25 feet in width or less. Increments may be created through projecting or recessing wall surfaces, changes in roofline and/or placement of architectural features.
- Façades in excess of 60 feet in height (measured from grade) shall not exceed 100 feet in length without a vertical break from the base of the building to the roof line, giving the building an appearance of multiple attached buildings.
- Vertical articulation shall be provided by reducing the footprint of the portions of a building above 60 feet in height. The building footprints shall be limited as follows:
  - A maximum of 85% of the building footprint may be up to 70 feet in height. This may be achieved by stepping back the building walls of upper floors, or by varying the building height of different wings (see diagram below); and/or
  - A maximum of 70% of the building footprint may be up to 85 feet in height. This may be achieved by stepping back the building walls of upper floors, or by varying the building height of different wings (see diagram below).
  - In addition, applicable setbacks may limit the permissible building height.



### 7.8.19 Offices

Activities typically include, but are not limited to, executive management, administrative, or clerical uses of private firms and public utilities. Additional activities typically include the provision of advice, design, information or consultation of a professional nature. This section sets forth the development and operational standards for stand-alone office buildings.

- Landscaping shall be provided adjacent to building perimeters along at least 80 percent of the exterior of the building. Landscaping provided to meet this requirement shall have a width of at least 10 feet as measured perpendicular to the building.
- Stand-alone office buildings shall have at least a 20 foot separation from other buildings.
- Office buildings shall provide a plaza area adjacent to the front or side building elevation. Its size shall be at least four percent of the building footprint square footage. Such plaza shall feature enhanced paving materials and include or be adjacent to landscaping, including trees to create opportunities for shade in the plaza. Pedestrian amenities such as benches and trash receptacles are encouraged in plaza areas.

### **7.8.20 Outdoor Recreational Facilities**

Activities typically include, but are not limited to, golf driving ranges, miniature golf, batting cages, go-cart tracks, and similar uses. The preparation and sale of food and beverages is permitted ancillary to the primary use. Outdoor recreational facilities shall conform to the following standards.

- Site lighting shall be directed on-site and shall not cause objectionable glare or spill over to adjacent properties.
- Exterior lights shall not exceed a post height of 50 feet.
- Public address systems or amplified systems for providing recorded music shall be designed, located, and operated so they are not audible 50 feet beyond the boundaries of the use.
- Outdoor recreational facilities shall be designed so that they comply with the Commercial Design Guidelines (Section 8), including semi-enclosed or unenclosed portions of the facility, to the satisfaction of the Community Development Director, or his or her designee.
- At least 12 percent of the area that is semi-enclosed or unenclosed within an outdoor recreation facility shall be landscaped, either clustered or evenly distributed on the site. Turf or artificial turf areas within golf driving ranges or miniature golf facilities shall not count towards meeting this standard.
- The owner/operator of an outdoor recreation facility shall provide sufficient security measures to effectively regulate interior and exterior loitering, parking congestion, and criminal activity.
- The sale of alcoholic beverages shall be subject to all applicable state and local standards, including applicable provisions of this Specific Plan.

### **7.8.21 Religious Assembly**

Activities typically include religious services and assembly such as customarily occurs in churches, synagogues, and temples. Religious assembly uses shall comply with the following standards:

- If a religious assembly use is developed adjacent to residential uses, a landscaped buffer not less than 20 feet in width shall be planted and maintained at the perimeter of the religious assembly use.
- If a religious assembly use is developed adjacent to residential uses, a six foot high solid masonry wall shall be provided to screen the residential uses.
- Parking for religious assembly uses shall be as specified for theater uses in Table 7.E.
- Building architectural styles and materials for religious assembly uses shall be consistent with the commercial design guidelines set forth in this Specific Plan.

### **7.8.22 Residential Pools and Spas**

Private residential swimming pools, spas, fish ponds, or other bodies of water 12 or more inches in depth at any point shall comply with the following standards:

- Swimming pools, spas, fish ponds, or other bodies of water shall be completely enclosed by a child-proof fence or wall in compliance with the adopted version of the Uniform Building Code.
- Swimming pools, spas, fish ponds, or other bodies of water shall be located at least 5 feet from side or rear property lines. Patio supports may not be constructed on the coping of a swimming pool, fish pond, or other body of water.
- No pools or ponds over 12 inches deep shall be located within the front setback area.

- Mechanical equipment shall be located consistent with the setback requirements set forth in this Specific Plan.

### 7.8.23 Senior Care Facilities

This use consists of senior care facilities and supportive services such as meals, housekeeping, social activities, and transportation, depending on level of need, allowing residents to remain living in the community as their care needs change. The Assisted Living Federation of America defines Assisted living Facilities as “An assisted living residence is a special combination of housing, personalized supportive services and health care designed to meet the needs — both scheduled and unscheduled — of those who need help with activities of daily living.” The intent of this zoning designation is to provide services for seniors, ranging from adult day care to assisted living facilities. These services are considered commercial land uses, as individual kitchens would not be provided for any of the rooms. Therefore the senior care facilities will not be counted as dwelling units.

**a. General Development Standards.** The following general standards shall apply to all senior care facilities within The Colonies at San Antonio Specific Plan.

- The transport, use, storage, and/or disposal of tissue, blood or blood products, bodily fluids, medical waste and/or any equipment, linen, paper or other product which has come into contact with such substances, shall be subject to applicable local, state and federal regulations and standards. The on-site incineration of such materials is not permitted.
- The density of rooms, intensity of facilities, and scale and design of buildings shall be harmonious with surrounding uses and development.
- Each assisted living facility shall include a reception area (lobby), office space for administrative use, service areas and facilities for employees (such as a lounge, lockers and showers), and laundry facilities for use by the hotel/motel.
- A common outdoor area of no less than 75 square feet per resident, but in no case less than 450 square feet in area shall be provided. At least 50 percent of this area shall be covered with turf, sand or other natural materials.
- Common outdoor areas shall be located so as to be shielded from freeway noise.
- Parking shall be provided pursuant to applicable sections of this Specific Plan. If deemed necessary by the City of Upland Community Development Department, on-site vehicle turnaround, separate entrance-exit points, or passenger loading areas shall be provided.
- The design of any senior care facility shall be reviewed and approved by the City of Upland Fire Department to ensure adequate emergency access. The senior care facility shall install and maintain smoke detectors, carbon monoxide detectors, fire sprinklers, fire extinguishers, and/or other equipment required by the Upland Fire Department.

**b. Building Standards.** The following standards shall be provided to all buildings within the Senior Care Facilities zone in The Colonies at San Antonio Specific Plan.

- Amenities including the following shall be provided for each assisted living building:
  - Variety of comfortable community areas including:
    - Main living room for socializing
    - Private dining room for personal entertaining
    - Library and activity rooms
    - Country kitchens
    - Screened porches
  - Beautifully landscaped grounds

- Fire safety system
- Washers and dryers available for personal use
- Amenities including the following shall be provided for each suite:
  - Emergency call response system
  - Bedroom of at least 12'6" by 15'6" size
  - Closet
  - Bathroom of at least 5'6" by 5'6" size that includes a sink, toilet, and shower

#### **7.8.24 Temporary/Seasonal Outdoor Sales and Events**

This use consists of the *outdoor* retail sale of materials on a temporary and/or seasonal basis and/or *outdoor* performances. Typical activities include, but shall not be limited to, Christmas tree and pumpkin sales, tent sales, farmers markets, arts and craft fairs, arts festivals, and individual and group performances. Temporary/seasonal outdoor sales and outdoor events shall conform to the following standards:

- Operators of any temporary/seasonal outdoor sales or outdoor events shall obtain and abide by applicable provisions of a City of Upland Special Events Permit in compliance with the Upland Municipal Code, Section 17.16.160.
- Sales or events held in other than areas designated in an approved site plan for outdoor sales shall not exceed the number of days specified by the Special Events Permit, including the renewal or extension provision, in compliance with the Upland Municipal Code, Section 17.16.160.
- For periodic outdoor sales or outdoor events that are organized by the property manager, the property manager shall apply to the Community Development Department for a master Special Events Permit that specifies conditions of operation and limitations regarding frequency and duration of on-going events that may exceed the duration specified in the Upland Municipal Code, Section 17.16.160.
- For outdoor sales and outdoor events that can reasonably be expected to draw more than 125 patrons, the organizer must have an approved security plan on file with the Police Department, and comply with such plan.
- Sales or events shall not interfere with normal vehicular/pedestrian circulation nor limit access to individual tenants within the Specific Plan.
- Sales or events shall cease operation after 10:00 p.m.
- Booths, stalls, pavilions, tents, amusement devices, rides, and/or any other temporary structure shall be subject to the following:
  - At least two weeks before erection of any structures, amusement devices, and rides, application shall be made to the Building Department. Certificates of safety shall be furnished and inspections shall be made prior to operation.
  - Temporary structures or amusement devices shall not exceed the height of adjoining permanent structures or 30 feet, whichever is the lesser height.
  - Temporary structures shall maintain a setback of 15 feet from any public road right-of-way, and shall not hinder emergency access or obstruct pedestrian pathways.
- The keeping of animals during temporary outdoor events (e.g., petting zoos, animal exhibitions/shows) shall adhere to City of Upland regulations. Animals temporarily permitted during this use may include but shall not be limited to: horses (and other equine species), cows (and related bovine species), goats, sheep, fowl, poultry, species common to zoos, and exotic (non-native to North America) species. No wild or ferocious animals shall be allowed. The on-site keeping of animals shall be limited to the duration of the event.

- All performances, either in groups or individuals, roving or stationary, shall be conducted in a manner which minimizes disruptions to any form of circulation or the normal operating of surrounding commercial establishments.
- Temporary lighting, if required, shall be installed and maintained in a manner which limits the overspill of light and glare into adjacent uses. Special use lighting such as searchlights, strobe lights and laser light shows shall be utilized in a manner which limits any adverse impacts to surrounding land uses.
- The installation of multimedia equipment including, but not limited to, sound reproduction and amplification devices, video screens and monitors, image projection equipment and other devices which produce “special effects” (fog machines, laser light shows, strobe lights, etc.) shall be installed, operated and maintained in a safe manner. The production of special effects shall not unnecessarily or adversely impact the users of surrounding commercial or residential areas.
- Insurance coverage shall be required at the request of the Community Development Director.
- The site shall be restored to its original condition at the end of each event.

#### **7.8.25 Theaters**

This use type includes structures *primarily* devoted to the exhibition of live or prerecorded theatrical, musical, comedic or other performances. Sale of prepared foods and beverages is permitted ancillary to the primary use.

- Any theater or similar use shall provide a drop off/pick up location.
- The operators of a theater, auditorium or similar use shall employ measures to minimize loitering.
- Arcades within theaters shall adhere to applicable standards of this Specific Plan.

#### **7.8.26 Utility/Roadway Facilities**

Equipment, facilities, or features required for the operation or maintenance of public utilities or roadways, including parking areas, bus stops, and taxi stands. The following standards shall apply:

- The queuing of buses, taxis and other means of public conveyance shall not be permitted outside areas specifically designed for this purpose.
- Public transit facilities shall be incorporated into the site design of high-demand and/or high-occupancy uses. The design of any such facility shall adhere to applicable provisions of the Americans with Disabilities Act (ADA).
- Transit facilities shall be located in a manner which does not disrupt the normal flow of traffic.
- Bus stops, taxi stands, and related facilities shall be constructed of vandal resistant materials.
- Bus stop seating shall be designed and installed in a manner which prevents persons from sleeping at bus stops.
- Bus shelters and similar uses shall provide adequate protection from inclement weather.
- The agency or individual responsible for the operation and/or maintenance of any transit facility shall implement adequate and appropriate measures to prevent loitering, disturbing noise and light, criminal activity, and other public nuisances.
- Parking, repair, and embarkation/debarkation areas for bus terminals shall be adequately screened from adjacent uses and public right-of-ways by decorative steel fences, walls and/or landscaping. Any such screening shall adhere to applicable provisions of this Specific Plan.

### **7.8.27 Electronic Signage Standards**

All proposed LED electronic message centers and attendant landscaping and irrigation plans shall be subject to design review in accordance with Section 11.2.3 of the Specific Plan. The scope of review by the Design Review Board shall include consideration of the requirements of Section 7.7.7 and the following:

- That the proposed sign displays are proportional to its face and its supporting structures; and
- That the proposed sign and its supporting structure contains a full architectural treatment which is consistent with and complementary to the architectural design, massing, color, size and treatment of the business itself; and is harmonious with the general character of immediately surrounding properties.

## **7.9 ENVIRONMENTAL PERFORMANCE STANDARDS**

### **7.9.1 Purpose and Applicability**

The purpose of this section is to set forth standards to protect the public and nearby uses from potential negative effects of commercial and other uses within the Specific Plan area by regulating air quality, electrical or electronic interference, hazardous materials management, light and glare, liquid and solid wastes, noise, odors, vibration, and water quality. The following performance standards shall be applied to all development within the Colonies at San Antonio Specific Plan area.

### **7.9.2 Air Quality**

Any operation or activity which might cause the emission of any smoke, fly ash, dust, fumes, vapors, gases, or other forms of air pollution, which can cause damage to human health, vegetation, or other forms of property, or can cause excessive soiling on any other parcel shall conform to the requirements of the SCAQMD.

### **7.9.3 Electrical or Electronic Interference**

No operation or activity shall cause any source of electrical or electronic disturbance that adversely affects persons or the operation of any equipment on any other parcel, and which is not in conformance with the regulations of the Federal Communications Commission.

### **7.9.4 Hazardous Materials Management**

All uses involving the use, storage, handling, transportation, or disposal of hazardous materials are required to comply with the provisions of the most current amendments to the California Code of Regulations, Titles 22 and 27; applicable requirements under the National Pollutant Discharge Elimination System (NPDES); applicable requirements of the City of Upland Fire Department; and any other applicable City, County, state or federal standard relating to the use, storage, handling, transportation, or disposal of hazardous materials.

The storage of hazardous materials in quantities less than 55.0 gallons shall follow applicable State regulations governing the use, handling, storage and disposal of these substances. Quantities of hazardous materials exceeding 55.0 gallons but less than 2,000 gallons (20,000 gallons for the storage of gasoline at service stations and automotive vehicle sale dealerships), shall require approval of the method for hazardous materials underground storage and review of the use's business plan. Hazardous materials in excess of this amount shall require additional environmental review and specific approval by the City.

### 7.9.5 Light and Glare

Except for on-site lighting and project identification signage, no operation, activity, or lighting fixture shall create illumination exceeding 0.5-foot candles 50 feet beyond any commercial area property line to reduce light pollution and minimize light spillage and direct glare onto adjacent properties. On-site light fixtures exceeding 20 feet in height shall be full cutoff type fixtures for glare control. In open space areas and around the perimeter of commercial parking areas, cut-off fixtures shall incorporate mounting arms with the capability for tilt adjustment. All fixtures north of the freeway shall be limited to 30 feet in height. No post mounted, flood type fixtures will be permitted north of the freeway. All lighting shall be energy efficient to reduce energy consumption, and fixtures spaced to maximize efficiency. LED parking lot lighting may be permitted in Planning Areas 20 and 21, provided that all of the parking lots in those Planning Areas are lighted with LED lighting to maintain a uniform color in the area. LED parking lot lighting shall also be full cutoff type fixtures where lighting will be directed down and/or shielded such that no light will be emitted above the 90 degree angle to avoid spillage and glare onto adjacent properties. LED lighting is no brighter than other lighting sources; thus, the lighting levels in Parking Areas 20 and 21 will be the same as the remainder of the Specific Plan area.

### 7.9.6 Liquid and Solid Wastes

In order to avoid contaminating water supplies, interfering with bacteriological processes in sewage treatment, or otherwise creating a public health hazard, all discharges of materials into any public or private street or storm drain shall be in accordance with the adopted standards of the City, the California Department of Health Services, and other governmental agencies having legal jurisdiction.

- Liquid waste disposal and runoff control shall be conducted within the applicable guidelines.
- Disposal of liquid waste shall meet the applicable guidelines.
- Hazardous waste handling, transportation, recovery, and disposal shall comply with applicable federal and state laws, as well as with the regulations of the City of Upland.
- Solid waste disposal shall comply with applicable federal and state laws, and shall be regulated as per City Ordinance.

### 7.9.7 Noise

No operation or activity shall create exterior noise levels in excess of the standards stated in the City of Upland Noise Ordinance at the nearest residential dwelling to the commercial use. Ambient noise standards shall be as follows in Table 7.I.

**Table 7.I - Noise Standards**

<b>Decibels</b>	<b>Time</b>	<b>Use</b>
45 dB(A)	10:00 p.m. to 7:00 a.m.	Residential
55 dB(A)	7:00 a.m. to 10:00 p.m.	Residential
65 dB(A)	Anytime	Uses not specified
75 dB(A)	Anytime	Industrial and commercial

Actual base decibel measurements exceeding the above levels at the corresponding time and use indicated shall be deemed as the “base ambient noise level” (BANL). Exterior noise shall be measured on the exterior of any residential property and no noise level shall exceed the following as indicated in Table 7.J for the duration periods specified.

**Table 7.J - Noise Levels and Duration**

<b>Noise Level Exceeded</b>	<b>Maximum Allowed Duration Period</b>
Base Ambient Noise Level	30 minutes in any hour
5 dB(A) above Base Ambient Noise Level	15 minutes in any hour
10 dB(A) above Base Ambient Noise Level	5 minutes in any hour
15 dB(A) above Base Ambient Noise Level	1 minute in any hour
20 dB(A) above Base Ambient Noise Level	Not permitted

The following sources are exempt from the provisions of this section:

- Safety devices and warning signals.
- Emergency equipment, vehicles, devices, and activities.
- Temporary construction, maintenance, or demolition activities conducted between the hours of 7:00 a.m. and 6:00 p.m.

To address noise concerns, outdoor public address systems, intercoms, and speakers for Drive-Through Sales and Service uses and Outdoor Recreational Facilities uses shall be allowed only under the conditions stated in Section 7.8.10 and 7.8.20, respectively. Outdoor public address systems, intercoms, and speakers shall not be permitted with any other land uses (including Automotive Vehicle Sales uses and Home Improvement uses as listed in Table 7.B); instead, alternative technologies (i.e., the wearing of pagers by employees) shall be used when needed. (Communication devices as required by the Americans with Disabilities Act or other State building codes shall be exempt from these restrictions.)

### **7.9.8 Odors**

No operation or activity that emits odorous gases or other odorous matter in such quantities as to be dangerous, injurious, noxious, or otherwise objectionable to a level that is detectable with or without the aid of instruments at or beyond the property within which the odor is created shall be permitted.

Uses shall conform to the applicable requirements of the SCAQMD.

### **7.9.9 Ground Vibration**

No operation or activity shall be permitted to cause an earth-borne vibration beyond the property within which the vibration was originally created which produces a particle velocity greater than two-tenths inches per second (0.2 inch/sec) measured at or beyond the property line.

Vibration velocity shall be measured with a seismograph or other instrument capable of measuring and recording displacement and frequency, particle velocity or acceleration. Readings are to be made at points of maximum vibration along any lot line.

Ground vibration caused by moving vehicles, trains, aircraft, or temporary construction or demolition is exempted from this requirement, as is ground vibration caused by emergency equipment, vehicles, devices, and activities, as well as from temporary construction maintenance, or demolition activities conducted between the hours of 6:00 a.m. and 7:00 p.m.

### **7.9.10 Water Quality**

All runoff from repair areas, fueling islands, and outdoor storage areas shall be drained on-site in a controlled manner so as to prevent groundwater and surface water contamination by fuel, oils, and solvents.

## **8.0 DESIGN GUIDELINES**

### **8.1 PURPOSE OF THE COLONIES AT SAN ANTONIO DESIGN GUIDELINES**

Previous chapters of the Colonies at San Antonio Specific Plan have addressed the project’s land use and infrastructure plans. The purpose of this Chapter is to ensure the City, as well as Upland and Colonies at San Antonio residents, that all individual development undertaken pursuant to this Specific Plan will conform to high standards of design excellence. The following guidelines seek to establish a design framework that individual developers of commercial centers and residential neighborhoods will use as a guide for new construction. The guidelines also seek to encourage creativity in site planning and architectural design.

The design guidelines contained in this Chapter establish basic provisions for commercial and residential architecture, as well as for site planning and on-site walls and fences, outdoor mechanical equipment, and lighting. They are intended to guide and inform to the same extent that they may restrict or require. The text, photographs, and examples contained in these design guidelines have been prepared to assist, inspire, and direct outstanding design solutions.

The Colonies at San Antonio design guidelines are intended to allow design latitude and flexibility, while ensuring that the value of property within and adjacent to the Colonies at San Antonio will be enhanced through careful design control. Variation within the context of these guidelines is encouraged in order to achieve attractive commercial centers and individually distinctive neighborhoods.

Projects in which nearly identical buildings line the streets without variation in placement and form will not be encouraged or approved. Design should clearly reflect the “place making” techniques described in this Chapter. Through careful collaboration, the Colonies at San Antonio is destined to be a desirable place for its new residents to live.

### **8.2 COMMERCIAL DESIGN GUIDELINES**

The purpose of these Commercial Design Guidelines is to broadly establish planning and design parameters that will allow both design controls and design flexibility for the future development of non-residential Planning Areas. The following guidelines shall apply to the development of commercial and other non-residential uses within Planning Areas 4, 17, 18, 19, 20, 21, and potentially the newly created Planning Area 22. These guidelines provide design criteria for site planning, design influence, architecture, parking and circulation, screen walls and fences, retaining walls, service facilities, mechanical equipment, trash enclosures, outdoor storage, outdoor lighting, pedestrian areas, pedestrian oriented development, and special uses including car washes/gas stations, drive-through facilities, large volume buildings, and vehicle dealerships.

### **8.2.1 Site Planning**

Within the Colonies at San Antonio Specific Plan, site design should first address the nature and function of the use, building, or feature being considered. Architectural design and details are then, as a rule, expected to be oriented to areas within public view. Landscaping, including entry features, are to be provided to highlight positive visual features, and to provide a pleasant outdoor environment. Design within the Colonies at San Antonio Specific Plan should meld function and form, not one to the exclusion of the other. Consequently, each development plan submittal will be reviewed for its overall design, with allowances for individuality and special functional needs.

To facilitate design integrity between the varieties of uses within the Colonies at San Antonio Specific Plan, the following shall be incorporated into the design of non-residential projects.

- Compatibility shall be promoted through appropriate location and orientation of buildings.
- Individual tenant identity shall be reinforced and maintained.
- Placement of buildings shall consider the context of the commercial area, the location of nearby residential land uses, the location of major traffic generators, and circulation routes.
- Service areas (i.e., areas for loading/unloading, unpacking of goods, auto service, etc.) shall be simple, efficient, and functional.
- Physical and psychological security for patrons shall be enhanced through appropriate design, such as locating buildings and windows to maximize “eyes” on entryways, pedestrian pathways, and parking lots; providing adequate lighting throughout a development; and designing and maintaining landscaping so that it does not block views of entries and pedestrian pathways.

### **8.2.2 Design Influences**

The goal for the Colonies at San Antonio’s commercial areas is to combine shopping, dining, recreation, entertainment, and professional office uses into a gathering place for the City of Upland and the surrounding communities, as well as a destination landmark for all of the Inland Empire.

In order for the project to be successful, the Colonies’ commercial centers must ensure that people will want to return frequently to view a movie, dine in one of the many cafes and theme restaurants, exercise, people watch, or enjoy leisure activities, while shopping in a collection of upscale retail stores.

Visual excitement should be created by utilizing a variety of lighting, signage, public artwork, and a rich palette of colors and materials. The architecture should incorporate bold geometric forms organized within a backdrop of color, stone accents, and landscape. The design should vary roof materials and forms. Enhanced paving and water features should enrich the pedestrian experience throughout the area. Artwork should be considered, such as wall graphics on the buildings. Theater uses should include a plaza with patios, seating areas, and terraces for dining. Designers should consider incorporating building details and site furniture influenced by the City of Upland’s heritage. These may include finishes, textures, materials, and colors which evoke the City’s rich inventory of public and private spaces.

The Colonies commercial centers should successfully serve both the community of Upland and the surrounding region by respecting basic retail design and planning parameters. Tenants who desire a presence in these centers will require that these key site planning and design elements are offered: convenient access and parking, visibility, orientation to either the freeway or major thoroughfares, functional loading and servicing, and signage appropriate to the large scale of the centers. All of these issues are extremely critical influences to the centers’ success. Suburban commercial retail facilities that respect these parameters can be full of life and positive contributors to their communities.

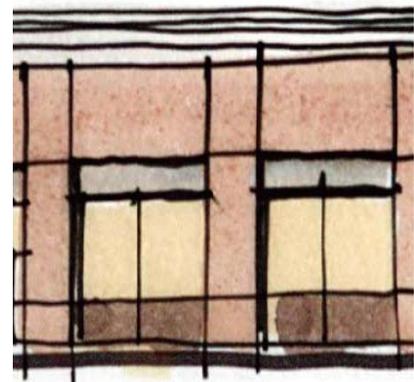
- A consistent design influence shall be established for buildings, signage, walls, and landscaping treatments within a Planning Area.
- If visible from Interstate 210, public streets, central parking lots, primary circulation routes, or adjacent buildings, the side and rear building elevations shall be designed with attention to architectural character, scale, detail, signage, and corporate identity.
- Phased projects shall be designed for continuity within the entire Planning Area and each phase shall appear to be a completed project.

### 8.2.3 Architecture

**Articulation.** Building facades facing public rights-of way (e.g., public streets and the I-210 freeway) shall be articulated to provide architectural interest and variety, and to reduce large massing elements. Articulation of horizontal and vertical surfaces, along with fenestration and openings, color, texture and use of varied materials, should be used to reduce the appearance of undifferentiated massing. Large smooth, unarticulated surfaces shall be avoided, although design simply for the “sake of decoration” is to be avoided as well.



- Buildings should have wall articulation, such as insets and/or pop outs on at least one building elevation.
- Buildings are encouraged to visually illustrate the three traditional parts of a building: base, mid-section, and top, through the use of wall articulation and/or contrasting materials and colors.
  - Materials and colors used for the building base should appear “heavier” and “darker” than the mid section of the building.
- Horizontal and vertical elements of exterior walls should vary in height and projection to provide substantial architectural interest and style.
- Details that create shade and cast shadows should be used to provide visual relief to the building.
- Patterns created by storefront, window, and door placement should add variety and interest.

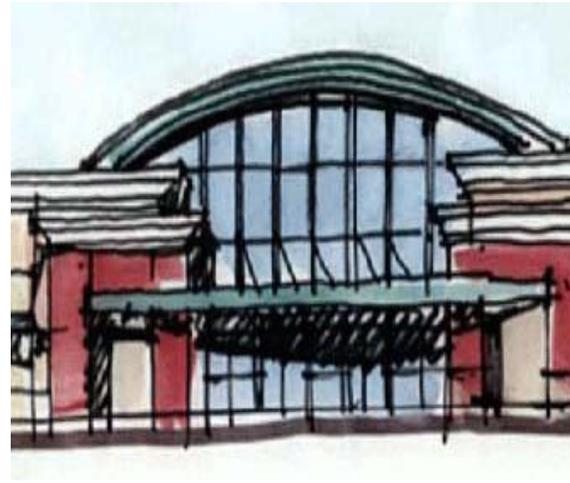


**Scale.** The scale of building components shall remain consistent with the proportion of the building.

- Intimate scale should be provided at ground floor levels, especially in multi-storied buildings, to humanize the building's scale to the pedestrian user.

**Proportion.** The proportion of the major elements of a building shall be complementary to the overall proportion of the building. These elements include roof type, roof height and overhang, store entrance and fascia design, storefront, wall openings, arcades, and other architectural features.

- The height, scale, and style of each building within the commercial areas of the Specific Plan should be compatible with other adjacent commercial structures.



*Details such as this canopy element at the ground floor help prevent an “overpowering” building scale.*



*These building elements are diverse, yet share similar proportions. The windows on the tower element are roughly the same size as the windows accented by awnings. Building walls are also visually divided into similar sizes through the use of score lines.*

**Emphasis.** Architectural focal points shall be encouraged.

- Building entries should be pronounced and easily recognizable, form a transition between exterior and interior areas, contribute to the building's design character, and provide protection from the elements. Consideration should be given to:
  - Canopy, portico, overhang, or recess/projection
  - Change in roof line, tower, or break in the surface of the entry wall
  - Arcade, arch, or columns
  - Pedestrian plaza at the entry
  - Integral planters or wing walls that incorporate landscaped areas adjacent to or flanking the entry.



*This tower element makes a strong entry statement that adds character to the building.*



*The change in roof line and arch identify this building entry.*



*Pilasters, a gabled roof, and a recess clearly mark this entry.*

**Orientation.** Building storefronts shall face pedestrian walks, public parking, or adjacent buildings.

**Roofs.** Building roofs shall be designed as architectural elements and contribute to the overall articulation of the building.

- Roof forms should be an integral part of the building design. False mansard roofs are not acceptable.



*This dome roof extending above the adjacent parapet creates an interesting roof “horizon”*

- Roof pitches and overhangs should generally be consistent within a single building; however, differing roof forms and materials are encouraged.
- Architectural elements such as projecting cornices should be used to add interest to flat roofs by defining the edge of the roof.
- Roof features and parapets should complement the character of adjacent buildings or other buildings within the commercial center.
- The appearance of the top of a building shall be considered when visible from Interstate 210 (see Section 8.2.7).



*Projecting cornices add interest to flat roofs.*

**Details.** Details shall be used as a method of enhancing the design character in a building.

- Pilasters and columns should feature decorative caps to provide detail and create a finished look.
- Storefronts should feature trim details around window and door openings.
- Windows and openings should be consistent with the architectural style of the building and maintain similar proportions and rhythm with those on adjacent buildings.
- The design of outdoor patios (i.e., enclosures, landscaping, furnishings) should be compatible with the architectural elements of the building.



*Details such as multi-pane windows, balconies, and overhangs enhance the design of the building*

**Materials and Colors.** Buildings shall use a variety of compatible materials and colors to create visual interest.

- At least four distinct colors and/or materials should be used on a building to create variety and detail.
- The texture of the building materials should enhance the function or appearance of the design by adding detail and richness.
- Avoid large expanses of smooth surfaces such as concrete or glass. A mixture of smooth and textured blocks for exterior building walls is encouraged.
- High quality materials and paint should be used to prevent degradation and for ease of maintenance. Finish materials that are susceptible to staining, fading, or other discoloration are strongly discouraged.
- Recommended building materials include the following:
  - Cement plaster (stucco)
  - Stone, stone veneer, or simulated stone veneer
  - Wood
  - Metal, wood, and clear glass storefront
  - Canvas and/or metal awnings
  - Textured and/or colored precast concrete panels
  - Textured and/or colored tilt-up concrete panels
  - Concrete masonry (articulated and/or used together with cement plaster).
- Color schemes should be harmonious within a single Planning Area. Colors of all facade elements should be coordinated, including awnings and trim. Even if a building uses a unique architectural theme or design, it should acknowledge some of the center's color palette.
- Bright colors should not be used as a base color, but may be used as an accent color. Accent colors should be compatible with the main color theme of rich tones. Accent colors may be used to highlight architectural features, such as detailing, trim, columns, storefronts, window sashes, doors, and door frames.

- Material and color changes should occur at a change in building plane, and reinforce the building’s massing concept. Material or color changes at the outside corners of a structure give the impression of “thinness” and artificiality of the material and are thus discouraged.

## **Other**

- Buildings may use overhangs, awnings, and other design elements to reduce heat gain. These are also encouraged in order to reduce the apparent height and scale of multi-story buildings.
- Building security systems may include use of interior electric security and fire alarm systems and vandal-proof glazing. Metal grilles should only be allowed in the interior of display windows and where the grilles recess into pocket overhead cylinders that completely conceal the grille when retracted.

### **8.2.4 Parking and Circulation**

Parking lot design shall include an efficient traffic movement pattern, adequate turning radii, convenient parking locations, a useful pedestrian circulation system, and landscaping to enhance appearance.

- Parking lots should be designed with a hierarchy of circulation: major access drives with no parking; major circulation drives with little or no parking; and parking aisles for direct access to parking spaces.
- Major entry drives may include a landscaped center median to separate incoming and outgoing traffic.
- Parking areas should be designed to separate cars and pedestrians. Parking area design should encourage pedestrians to walk parallel to moving cars and minimize the need for pedestrians to cross parking and drive aisles. Where pedestrian circulation paths cross vehicular circulation paths, the intersection should be designed to give pedestrians the right-of-way. Textured or colored pavement materials may be used to draw attention to pedestrian circulation paths.
- Pedestrian walkways may be provided to connect parking lots to building entrances for major tenants.
- Pedestrian walkways may be designated with textured or colored pavement, buffered by low landscaping, and well lit to provide visibility and security.
- Parking lots located in Planning Areas 20 and 21 can include LED lighting only if all of the parking lots within those Planning Areas are lit with LED lighting. The determination as to the type of parking lot lighting to be used uniformly throughout Planning Areas 20 and 21, whether LED or a “standard” lighting source shall be made with the review and approval of the first site plan within either Planning area 20 or 21.
- Landscape areas should be designed to minimize unwanted pedestrian cut-throughs or should provide appropriate hardscape breaks to allow for appropriate pedestrian traffic.
- Pedestrian drop-off locations may be incorporated into the circulation pattern and should be convenient and safe for pedestrians.
- Customer and employee parking should be separated from truck/loading access.

### **8.2.5 Non-Residential Screen Walls and Fences**

Walls and fences shall not be used unless needed or required for screening, security, or buffering land uses. Within commercial and business park uses, walls and fences may be used to screen parking areas, loading and storage areas, trash enclosures, and utility structures. The intent is to keep the walls as low as possible while still performing their screening function.

- Walls and fences shall be designed to complement the design, color and materials of adjacent buildings.
- Landscaping may be used in lieu of walls and fencing.
- The enclosure of permanent outdoor sale areas and garden centers that are components of a larger use, shall be reviewed and approved concurrent with each primary use. Such enclosure shall complement the design, configuration, color, and material of the primary use.
- Provisions of this document shall not set aside or reduce requirements established for security fencing by local, state, or federal law.

**Materials.** Walls and fences constructed within the Specific Plan site shall be made of durable materials, and shall be maintained in good condition at all times.

- Walls should be constructed of concrete block, masonry, brick, stone, simulated stone, or other similar materials. Decorative tubular metal fencing (i.e., wrought iron) may also be constructed, provided that, where it is located adjacent to a public street or will be visible from a public right-of-way, complementary landscaping is also provided.
- Walls should be constructed of vandal resistant materials and/or areas adjacent to walls may be planted with hedges, vines, or other plant material to deter graffiti.
- Except where it is adequately screened by plant materials or shielded from public view, permanent fences constructed with chain link, chain mesh or similar material shall not be permitted within the Specific Plan area.

**Height.** The following minimum heights shall apply to walls within the Specific Plan area:

Outdoor Storage Areas	6 feet
Loading Docks	8 feet
Trash Compactors	8 feet
Trash Enclosures	6 feet

- Variation in wall height may be permitted where it is integrated with building architecture and approved by the Community Development Director.
- No wall or fence within any front setback shall exceed 42 inches in height without the approval of the Community Development Director. Retaining walls within setback areas are exempt from this height limit.

**Configuration.** Where long, linear walls or fences are needed, they shall be accented through the use of pilasters and may be combined with landscaped berms.

- Screen walls for loading areas shall be located behind parking setback lines.
- Berms shall be constructed with a maximum slope of 3:1.
- Where possible, wall patterns shall be designed to be consistent with the geometry of adjacent landscaping.

**8.2.6 Retaining Walls**

Retaining walls may at times be useful and necessary to achieve customary utilization of land.

- Retaining walls facing Campus Avenue and 19th Street shall be constructed of decorative materials complementary to the commercial architecture, and shall be integrated with surrounding grades and landscape planting.
- Retaining walls located along the Interstate 210 freeway right-of-way shall be constructed of “landscape friendly” retaining wall structures (i.e., interlocking, stacking, or crib gravity/reinforced earth walls, or as approved by the Community Development Director).

### **8.2.7 Service Facilities, Mechanical Equipment, and Trash Enclosures**

Service facilities such as utilities, transformers, generators, mechanical equipment, loading areas, and trash enclosures shall be sited in areas that are generally not visible to the public, to the extent feasible.

- All new gas, telephone lines, and electrical lines of 12 kV or less within the project shall be placed underground.
- Ground-mounted utility appurtenances shall be located away from public view to the extent feasible or adequately screened from view from main circulation routes, parking areas, pedestrian areas, and adjacent buildings. Screening shall be of a material complimentary to the structure and/or heavy landscaping and berming.
  - Utility equipment such as transformers and backflow preventers should not be a dominant element of the front landscape area. When equipment is unavoidable in the front setback area, it shall be screened by walls and/or landscaping and shall not obstruct views of tenant spaces, monument signs, or driveways.
  - Utility appurtenances shall be placed such that they do not interfere with safe pedestrian movement.
  - Utility equipment such as electric meters, electrical panels, junction boxes, and similar equipment should be located either in a utility room within the building, or shall be enclosed within an exterior cabinet or screened from view using architecturally compatible walls or landscaping.
- Roof-mounted mechanical equipment shall be substantially concealed from view by integral building elements. (See I-210 exemption below.)
  - Mechanical equipment such as compressors, air conditioners, antennas, pumps, heating and ventilating equipment, emergency generators, chillers, elevator penthouses, water tanks, stand pipes, solar collectors, satellite dishes and communications equipment, and similar mechanical equipment shall be substantially concealed from view of primary circulation routes and neighboring properties.
  - Roof-mounted mechanical equipment that is visible from the I-210 freeway right-of-way shall be either: 1) entirely screened or obscured from view; 2) specially designed in an unobtrusive manner with the placement of equipment in a regular (grid) pattern, not haphazard, and the equipment painted to match the roofing color.
- Trash bins and compactors shall be stored in an approved enclosure or service yard.
  - Trash enclosures or service yards shall be constructed with masonry walls and metal doors that are architecturally compatible with the main building(s).
  - Refuse bins shall be provided in sufficient number, and shall be placed in convenient location(s).
  - A concrete pad shall be provided in front of and within trash enclosures to prevent damage to pavement.
  - All trash shall be deposited in the trash enclosure, and the gate leading thereto shall remain closed except when in use, and shall remain in good working order.

- Trash areas shall not be used for storage. The premises shall be kept in a neat and orderly condition at all times, and all improvements shall be maintained in a condition of good repair and appearance.
- Landscaping may be used to screen trash enclosures and service yards. Vines and hedges planted to obscure enclosure walls can also deter graffiti.
- The use of chain link fencing, barbed wire, or razor wire is prohibited.
- If roll-up doors/gates are used, they shall be installed so that the hood enclosure is integrated into the structure.

### **8.2.8 Outdoor Storage**

Outdoor storage provisions seek to create an organized and unified storage area that does not detract from the main building or center. (Outdoor storage provisions apply to *storage* areas that are accessory to a use [i.e., vehicles awaiting repair, raw goods awaiting assembly, etc.]. Outdoor storage provisions do not apply to outdoor *sales* areas [e.g., garden centers]) and sidewalk sales.)

- Outdoor storage areas shall be adequately screened from view from circulation routes by a solid masonry wall, constructed and maintained per applicable provisions of this document.
- Outdoor storage of shopping carts shall be provided by tenants with cart usage:
  - Locations shall be designated within adjacent parking areas for use as temporary storage or staging areas for carts until their retrieval.
  - Permanent cart storage areas shall be provided in sufficient aggregate quantity adjacent to the building and in proximity to the entrance, screened from direct view with a low decorative wall and/or landscaping.
  - Permanent cart storage areas shall not be located where they will hinder pedestrian access into the building.

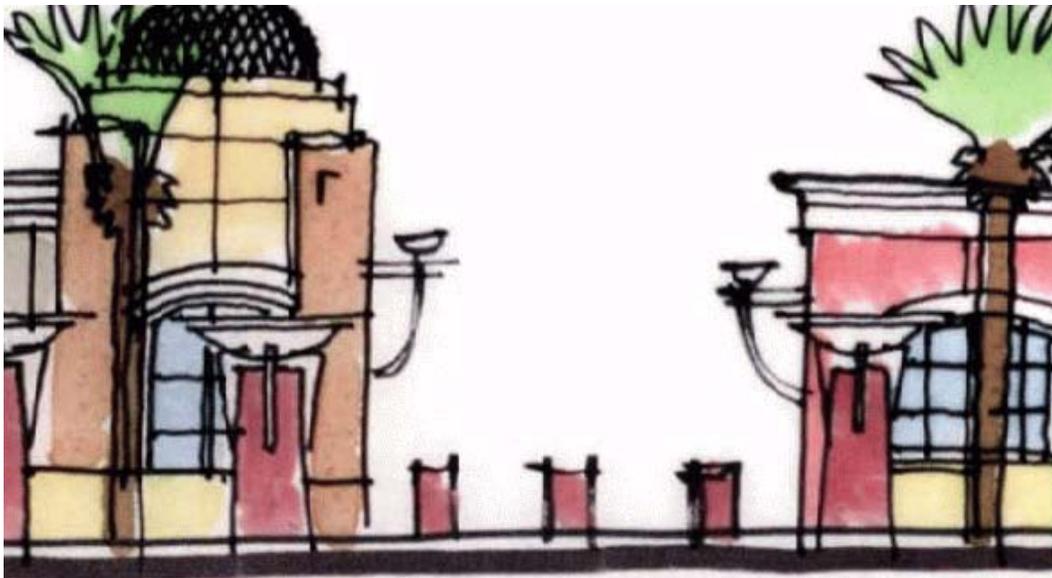
### **8.2.9 Outdoor Lighting**

Lighting levels shall be sufficient for the safety and security of vehicular and pedestrian traffic, but shall not spill onto adjacent properties.

- A project’s lighting scheme should use a hierarchy to provide a type of wayfinding system. The perimeter of the project and project entrances should be highlighted and should visually direct patrons by carefully designing light intensity and placement. General parking lot illumination should provide for safety and security in a manner that is not overpowering, and shall be designed to Illuminating Engineering Society (IES) standards for uniformity. Lighting at storefronts should feature a pedestrian scale and be inviting.
- In open space areas and around the perimeter of commercial parking areas, cut-off fixtures shall incorporate mounting arms with the capability for tilt adjustment.
- All fixtures north of the freeway shall be limited to 30 feet in height. No post-mounted, flood type fixtures will be permitted north of the freeway.
- Fixtures in parking lots in Planning Areas 20 and 21 are allowed to employ LED lighting only if all of the parking lots in these Planning Areas employ LED lighting. The determination as to the type of parking lot lighting to be used uniformly throughout Planning Areas 20 and 21, whether LED or a

“standard” lighting source shall be made with the review and approval of the first site plan within either Planning area 20 or 21

- Lighting shall be provided in all parking, vehicular and pedestrian circulation, loading, and storage areas. The minimum maintained average footcandle level shall be 1.00 fc, with a minimum of 0.40 fc.
- The height of the light source or pole shall be appropriate for the site activity and surrounding environment.
- Lighting fixtures used in parking lots shall be located to assure adequate light levels and to avoid displacing planned trees.
- Exterior lighting shall be architecturally integrated with the building or project style, materials, and colors.
- All exterior lighting, including LED parking lot lights, shall be full cutoff type fixtures where lighting will be directed down and shielded to avoid light spill and glare onto adjacent properties.
- Use of low, cut-off type bollard-type lighting and landscape accent lighting is encouraged, especially in pedestrian areas. Care, however, must be taken to limit the light coverage to the task to be illuminated. Fixtures without glare control are prohibited.
- Light standards shall not exceed 50 feet in overall height (47-foot high pole with 3-foot high base) from the finished grade of the parking area for commercial Planning Areas 4, 17, 18, 19, 20, 21, and potentially the newly formed PA 22.
- Accent lighting may be allowed to illuminate significant architectural elements within a building or center; however, lighting that creates “building washes” on unadorned walls shall be discouraged.
- Neon lighting that enhances the building or project and complements the architecture may be allowed subject to the approval of the Development Services Director.
- No fixtures without a means of mitigating glare will be approved.
- All exterior lighting shall be controlled by photocell in series with microprocessor base time switch.
- All security lighting shall utilize high color rendering lamp source.



*Lighting fixtures should complement the architectural style of the buildings and be appropriate to the purpose, in this case illuminating a pedestrian area.*

## 8.2.10 Pedestrian Areas and Pedestrian-Oriented Development

Pedestrian areas and pedestrian oriented development shall provide an environment that facilitates and invites visits to stores and uses, and creates informal social gathering areas.

- A continuous pedestrian pathway system shall be provided within each commercial Planning Area.
  - Pathways shall provide safe and convenient access by connecting building entrances to surrounding parking areas and public rights-of-way.
  - Freestanding arcade or trellis structures may be utilized to connect pedestrian pathways, provide shade, and visually enhance the site's pedestrian wayfinding system.
  - Pedestrian pathways shall be integrated with building arcades.
- Avoid large, unbroken wall surfaces; place extra emphasis on relating to the human scale in building design through the use of wall insets/offsets, details, windows, colors, and materials.
- Awnings, canopies, arcades, or other architectural means, featuring compatible materials and colors and sized appropriately to the scale of the building, should be used to enhance entrances.



*Pedestrian plazas may feature enhanced paving and landscaping, and be complemented by buildings with detail focused at the pedestrian level.*

- Enhance building design, particularly at pedestrian eye level, through the use of a variety of details, patterns, materials, textures, and/or colors. *Pedestrian pathways that lead to major uses may employ enhanced paving and landscaping.*
- Locate and design ground floor windows to maximize pedestrian interest. Large window openings that facilitate indoor-outdoor view interaction are encouraged (i.e., views into display windows, reading areas, and activities; views out from dining tables to pedestrian areas).
- Clear windows, as opposed to tinted windows, should be used in pedestrian areas to allow for views into buildings to draw interest.
- Courtyards, patios, plazas, covered walkways, enhanced paving, water features, site furniture, and similar pedestrian amenities may be incorporated into pedestrian oriented building areas whenever possible.

## 8.2.11 Pedestrian Crossing

An enhanced pedestrian crossing, similar to this conceptual drawing, will be place across 19<sup>th</sup> Street, located between Planning Areas 4 and 19.

- The enhanced pedestrian crossing, including enhanced pavement, monument signs, and pedestrian crossing signals, shall be designed to integrate with and complement the adjacent planning areas.
- The quality of design and construction of the enhanced pedestrian crossing shall be similar to that employed in the adjacent commercial planning areas. Materials and colors used for the enhanced pedestrian crossing shall be consistent with the materials and colors used within the adjacent commercial and mixed use planning areas.

- Monument signage will be incorporated into the enhanced pedestrian crossing design. This signage may only identify the project and the City of Upland. Tenant signage is not allowed on the monument signs at the enhanced pedestrian crossing.

The following are renderings of how the intersection could look.



Street Level



Plan View



Birdseye View

### 8.2.12 Special Uses

The following uses require special siting and architectural criteria because of unique on-site circulation and ingress/egress, building design, and site issues.

#### Car Washes/Gas Stations

- On-site circulation and ingress/egress shall be integrated to function with the circulation patterns of the surrounding commercial center.
- The design of on-site circulation patterns shall take into account vehicular and pedestrian circulation for all on-site uses (i.e., fuel pumps, convenience store, car wash, etc.).
- Car wash and gas station buildings shall provide the same architectural quality and elements as other buildings in the commercial center and be compatible with surrounding buildings.
- Accessory structures (i.e., canopies, cashier booths, car wash tunnels) shall feature architectural design and detail consistent with the main building and adjacent buildings.
- For car wash facilities in which customers exit their cars (i.e., “full service” facilities), an indoor or shaded waiting area shall be provided.
- Tank vents shall be screened and/or incorporated into the building architecture.

#### Drive-Through Facilities

- The siting of drive-through facilities, including their ingress and egress, shall be integrated to function within the circulation patterns of the surrounding commercial center, and shall minimize conflicts.
- Drive-through lanes shall be sited to minimize conflicts with walk-in patrons traveling from the parking lot to the building entrance.

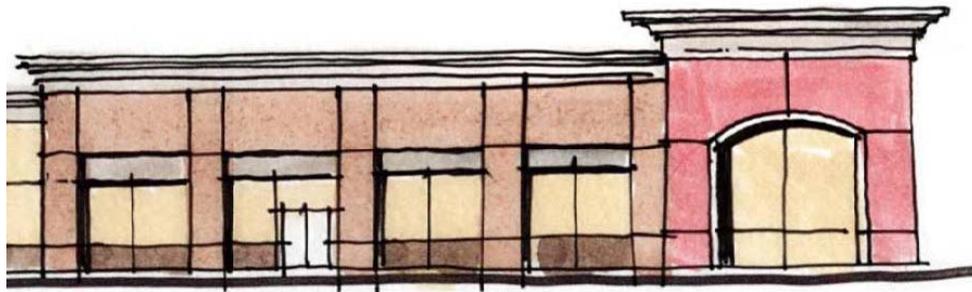
- Drive-through uses shall provide similar architectural elements and quality as other buildings. “Corporate” or “prototype” architecture is allowed.
- Drive-through elements shall be architecturally integrated with the building, rather than appearing to be applied or “stuck on” to the building.
- Drive-through windows shall not face prominent circulation routes or primary views within a commercial center without adequate screening and/or landscaping.
- Landscaping shall be designed and located to soften the visual impact of vehicle stacking areas for drive-through windows.
- Outdoor seating areas, play equipment, and perimeter fencing shall have an attractive design that is compatible with the architecture of the main building.

### Large Volume Buildings

- Reduce the apparent scale and uniformity of facades in order to make large buildings seem more inviting.
- Long, undifferentiated surfaces, facades, or building frontages are strongly discouraged. At least three features that add visual interest, such as arcades, decorative cornices, display windows, windows articulated with trim, and entry awnings, should be used on front facades.



*The apparent mass of large volume buildings can be reduced by designing breaks in roof lines, using materials to create accents, and employing landscaping to add interest.*



*Even less prominent elevations should feature articulation, as shown here through the use of a corner element, vertical columns and horizontal bands that create a base, mid-section, and top.*

- In situations where blank walls are necessary for structural reasons, these surfaces should be enhanced with decorative materials, colors, trellis structures, or other detail elements that complement the building.
- Varying roof levels and forms should be used to create interest and reduce the apparent scale of large volume buildings.
- Roofline variations should be used to provide architectural style or character for large volume buildings that are limited in wall configuration due to functional constraints.
- Three-dimensional cornice treatments, parapet wall details, overhanging eaves, and similar methods should be used to enhance the architectural character of the roof.
- Color schemes should help reduce apparent size and bulk of buildings.

### **Professional Office**

- Professional office buildings shall be designed to be compatible with and to the same level of quality as other adjacent commercial buildings.
- Service areas for professional office buildings (i.e., trash enclosures, mechanical equipment) shall meet the same requirements as other commercial buildings (see Section 8.2.7).
- Landscaping for professional office buildings shall be integrated with and complementary to the landscaping for other adjacent commercial buildings (see Section 9.4 - Commercial Landscape Concepts).

### **Vehicle Dealerships**

- All buildings on the site (i.e., showrooms, sales offices, service buildings, auto part sales) shall provide the same architectural quality and elements as other commercial buildings within this Specific Plan.
- Walls that are necessary to screen portions of the site (i.e., vehicle storage areas, service bays) shall be compatible with the architectural style of the buildings.
- Raised vehicle displays shall be compatible with the architectural design, materials, and colors used for the buildings.
- Perimeter landscaping should favor visibility from public rights-of-way. Edge treatments abutting a public road in the display/sales areas where parking is allowed on street should provide walk-on ground covering such as lawns with vertical tall trees regularly spaced. Where parking is not allowed on public streets, low growing or spreading shrubs with regularly spaced trees may be used.
- Diamond tree wells (6'x6') within the vehicle sales/display areas should be provided with vertical tall trees at one tree well per 20 parking spaces. Decomposed granite rock, cobble or other nonliving material may be used as "ground covering" in these "diamond" planters.
- Other parking areas such as auto storage and customer/employee parking not fully screened with a minimum 6-foot high masonry wall shall be landscaped according to commercial standards.
- Customer parking spaces shall be conveniently located on site and be clearly marked.

### 8.3 RESIDENTIAL DESIGN GUIDELINES

The goal of the following residential design guidelines is to create a community that is unique within the Inland Empire, and yet reminiscent of Upland’s heritage. It is the further goal of these residential guidelines to assure that the building architecture is historically accurate in the use of material. Each aspect of every project is viewed as a significant element in reinforcing the neighborhood concepts proposed for the Colonies at San Antonio Specific Plan.

#### 8.3.1 Architectural Diversity

The Colonies at San Antonio’s residential design guidelines are intended to promote visual compatibility and individuality for the neighborhood and home. The residential guidelines do not promote rigid adherence to style descriptions, but assist in the design direction and quality of the end product.



*Single-Family Detached*

Achieving variety and diversity in neighborhood architecture is a key community objective. For inspiration, the Colonies at San Antonio Specific Plan has embraced compatible architectural styles. Each style has historical precedence in Southern California, and is capable of contemporary interpretation and variation in a modern merchant-built house. The intent is to avoid “stage-front” architecture, but instead to encourage the application of detail and materials authentic to the spirit and character of the style.

Each single family detached planning area shall utilize a minimum of three architectural styles selected from Group I, II or III, below. A fourth style may be chosen from any one group. A maximum of 40 percent and a minimum of 20 percent of any one style shall be used. Adjacent planning areas not separated by Colonies Parkway or Tanglewood Avenue shall have no more than two matching styles; however, planning areas that share interconnected local streets providing access to a collector road (i.e., Colonies Parkway) may be considered as one planning area for the purpose of determining the number of architectural styles, and thus could have more than two matching styles.

Each multifamily and mixed-use development shall utilize a maximum of three architectural styles selected from group I, II, or III, below. Individual building clusters (i.e., courtyard, flat or townhouse structures) however, shall only utilize one architectural style. Multifamily and mixed-use building clusters facing a public street and flanking major entrance areas to individual developments from 19<sup>th</sup> Street or Colonies Parkway shall utilize only one identical architectural style selected from Group I, II, or III, below.

**Group I**

Spanish Colonial  
Italianate

**Group II**

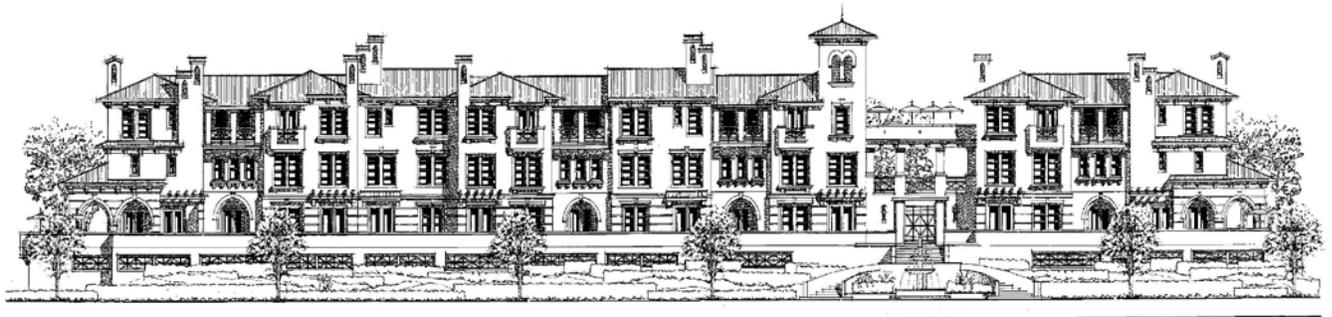
Colonial  
Monterey

**Group III**

American Cottage  
Craftsman

Residential projects that have gated private streets or have lots less than 4,500 square feet may be designed as “themed” projects, using a single style from Group I, II, or III. Multifamily housing may be designed as a themed project using a single style from Group I, II, or III.

Each residential style is presented on the following pages in the form of an image collage and a matrix description of its main components.



*Multifamily Housing*

**a. American Cottage**



Style Elements	Preferred	Allowed
Form	<ul style="list-style-type: none"> <li>• Simple plan form massing with predominant roof design</li> <li>• Typically steep 2nd story roofs breaking over 1st story elements</li> <li>• Gable, hip, and/or shed roof dormers</li> <li>• Dominant chimney form</li> <li>• Minimum 2 stories, Maximum 3 stories / 45' (RF)</li> <li>• Minimum 2 stories, Maximum 8 stories / 85' (MU)</li> <li>• Four Sided Architecture (RF and MU)</li> </ul>	<ul style="list-style-type: none"> <li>• Gentle to steeply pitched roofs with projecting gable ends</li> </ul>
Roof	<ul style="list-style-type: none"> <li>• 6:12 to 12:12 high roof pitch</li> <li>• Flat concrete tile with “slate” look</li> <li>• Tight to 12” overhangs</li> <li>• Stucco soffit</li> </ul>	<ul style="list-style-type: none"> <li>• Multiple gable element roofs with broken pitches</li> <li>• Shallow “s” roof concrete tiles</li> </ul>
Walls	<ul style="list-style-type: none"> <li>• Smooth stucco with stone or brick veneer base</li> <li>• Smooth stucco finish</li> <li>• 1 ½ to 2 story stone or brick accent wall planes at visible elevations</li> </ul>	<ul style="list-style-type: none"> <li>• Sand stucco finish</li> <li>• Siding accents in gable ends</li> <li>• Blended stucco and siding</li> </ul>

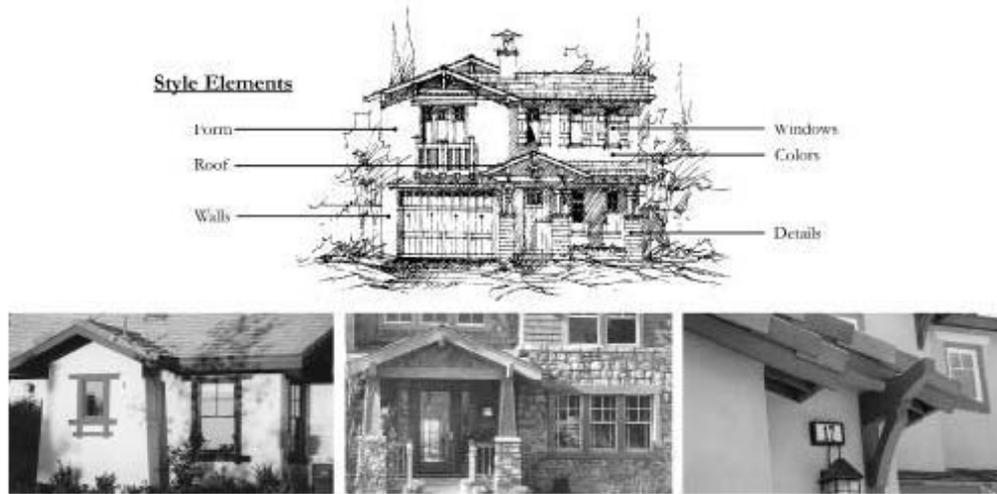
Style Elements	Preferred	Allowed
Windows	<ul style="list-style-type: none"> <li>• Bay windows</li> <li>• Rectangular multi-paned windows</li> <li>• White vinyl wrapped aluminum frames</li> </ul>	<ul style="list-style-type: none"> <li>• Single paned windows on sides and rear elevations (not on corner lots).</li> <li>• Round and curved top windows</li> </ul>
Colors	<ul style="list-style-type: none"> <li>• Light earth tone bodies</li> <li>• Contrasting cool or warm trims</li> </ul>	<ul style="list-style-type: none"> <li>• White or dark stucco body colors</li> </ul>
Details	<ul style="list-style-type: none"> <li>• Shutters</li> <li>• Black wrought iron balconies</li> <li>• Real or faux door surround</li> <li>• Recessed windows C Stone and brick chimney designs</li> <li>• Herringbone brick accents</li> <li>• Decorative wrought iron pot shelves</li> <li>• Battered columns</li> </ul>	<ul style="list-style-type: none"> <li>• Entry porches</li> <li>• Wood columns with braces</li> <li>• Wood pot shelves</li> </ul>

## b. Colonial



Style Elements	Preferred	Allowed
Form	<ul style="list-style-type: none"> <li>• Dominant gable roof</li> <li>• Simple rectangular massing stacked wall planes</li> <li>• Minimum 2 stories, Maximum 3 stories / 45' (RF)</li> <li>• Minimum 2 stories, Maximum 8 stories / 85' (MU)</li> <li>• Four Sided Architecture (RF and MU)</li> </ul>	<ul style="list-style-type: none"> <li>• Dominant hip roof</li> <li>• Simple wall planes throughout the height of the building</li> </ul>
Roof	<ul style="list-style-type: none"> <li>• 5:12 to 7:12 roof pitch C Flat, smooth concrete tile</li> <li>• 18" - 24" overhangs C Cornice at gables</li> </ul>	<ul style="list-style-type: none"> <li>• 8:12 to 12:12 roof pitch</li> <li>• Texture flat concrete tile</li> <li>• 12" - 18" overhangs</li> <li>• Gambrel roofs</li> <li>• Boxed eaves</li> </ul>
Walls	<ul style="list-style-type: none"> <li>• Smooth stucco finish or horizontal lap siding</li> </ul>	<ul style="list-style-type: none"> <li>• Sand stucco finish</li> <li>• Blended stucco and siding</li> </ul>
Windows	<ul style="list-style-type: none"> <li>• Vertical multi-paned windows in groupings</li> <li>• White vinyl wrapped aluminum frames</li> <li>• Decorative round or half round windows</li> </ul>	<ul style="list-style-type: none"> <li>• Simple window surrounds (not on corner lots of Multifamily buildings)</li> </ul>
Colors	<ul style="list-style-type: none"> <li>• White to mid-value bodies</li> <li>• White trims</li> </ul>	<ul style="list-style-type: none"> <li>• Pastel and cool colors</li> <li>• White trims</li> </ul>
Details	<ul style="list-style-type: none"> <li>• Brick veneer accents on walls</li> <li>• Chimney detailed with brick veneers</li> <li>• Dormers</li> <li>• Shutters on front elevations</li> <li>• Entry door pediments Columns</li> </ul>	<ul style="list-style-type: none"> <li>• Entry door porches</li> <li>• Oval, decorative windows</li> <li>• Corbels</li> </ul>

**c. Craftsman**



Style Elements	Preferred	Allowed
Form	<ul style="list-style-type: none"> <li>• Cross gabled or gable fronted one and two story box like massing under main roof</li> <li>• Full front porch with heavy square upper column or post on a massive pier base</li> </ul>	<ul style="list-style-type: none"> <li>• Varied plan shapes</li> <li>• Varied roof heights</li> <li>• Entry porch</li> </ul>
Roof	<ul style="list-style-type: none"> <li>• 3:12 to 4:12 low roof pitch</li> <li>• Unenclosed eave overhang with elaborate exposed rafter tails</li> <li>• Decorative ridge beams and purlins</li> <li>• Triangular knee braces</li> <li>• Overhangs at 36" to 48"</li> <li>• Gable roofs</li> </ul>	<ul style="list-style-type: none"> <li>• 4:12 to 7:12 normal roof pitch</li> <li>• Shingle texture flat concrete roof tile</li> <li>• Plain cut rafter tails</li> <li>• Overhangs 18" to 30"</li> <li>• Hip and gable roofs</li> </ul>
Walls	<ul style="list-style-type: none"> <li>• Horizontal siding</li> <li>• Plain wood shingle siding</li> <li>• Stone or brick base</li> </ul>	<ul style="list-style-type: none"> <li>• Smooth sand finish stucco</li> <li>• Blended stucco and siding</li> <li>• Stucco with stone base</li> <li>• Battered wall forms</li> </ul>
Windows	<ul style="list-style-type: none"> <li>• Ribbon windows - 3 or more ganged</li> <li>• Single hung windows</li> <li>• Mullion upper half of windows</li> <li>• White vinyl wrapped aluminum frames</li> </ul>	<ul style="list-style-type: none"> <li>• Vinyl color coated aluminum sliding windows</li> <li>• Single pane windows at sides or rear elevations (except that the preferred option shall be used for all four sides on corner lots)</li> </ul>
Colors	<ul style="list-style-type: none"> <li>• Light earth tone colors</li> </ul>	<ul style="list-style-type: none"> <li>• Deep earth tone colors</li> </ul>
Details	<ul style="list-style-type: none"> <li>• Black wrought iron or copper accents</li> <li>• Stone and brick blended accent on chimney</li> <li>• Full length porch - stone finish</li> <li>• Layered wood door and window details</li> <li>• Wood entry door with basic geometric pattern</li> <li>• Battered columns</li> </ul>	<ul style="list-style-type: none"> <li>• Classically styled columns</li> <li>• Faux copper patina accents</li> <li>• Arts and crafts lighting fixtures</li> <li>• Matt finish tiles</li> <li>• Carved wood style entry door with organic designs</li> </ul>

**c. Craftsman – Additional Guidelines Specific to Multifamily and Mixed Use Dwelling Units**

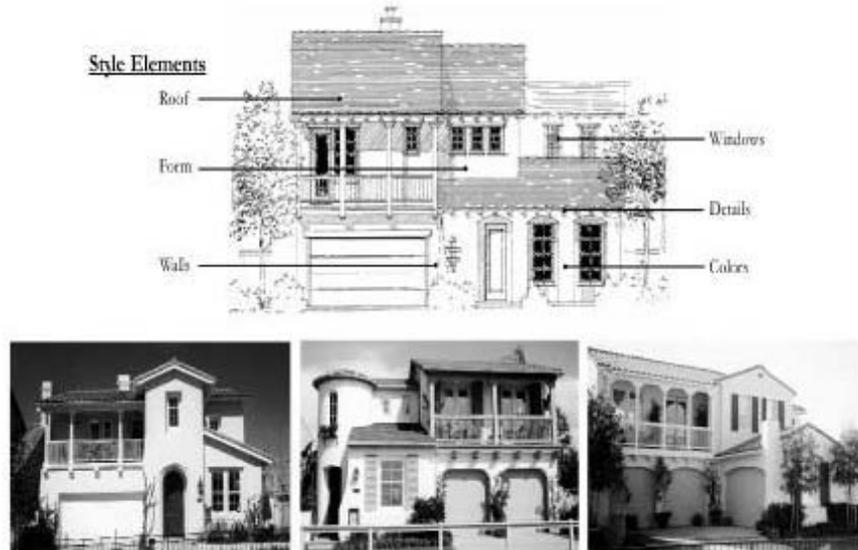
Style Elements	Preferred	Allowed
Form	<ul style="list-style-type: none"> <li>• 2-3 Stories (3 at maximum)</li> <li>• <i>Duplex/Triplex/Quadplex and Townhouse only</i></li> <li>• Front porches distinguish individual units on the ground level with heavy square upper column or post on a massive pier base</li> </ul>	<ul style="list-style-type: none"> <li>• Four Sided Architecture</li> <li>• <i>Duplex/Triplex/Quadplex and Townhouse only</i></li> <li>• Entry porch</li> </ul>
Details	<ul style="list-style-type: none"> <li>• <i>Duplex/Triplex/Quadplex and Townhouse only</i></li> <li>• Porches distinguish individual units on the ground level – stone finish</li> </ul>	<ul style="list-style-type: none"> <li>• <i>Duplex/Triplex/Quadplex and Townhouse only</i></li> <li>• Entry porch with matte finish tiles and organic designs</li> </ul>

**d. Italianate**



Style Elements	Preferred	Allowed
Form	<ul style="list-style-type: none"> <li>• Two story irregularity in massing throughout the height of the building</li> </ul>	<ul style="list-style-type: none"> <li>• Two story simple massing with at least 1 vertical and 1 horizontal break</li> </ul>
Roof	<ul style="list-style-type: none"> <li>• Main hip roof with intersecting hip roof</li> <li>• 3:12 to 4:12 low roof pitch</li> <li>• 12" to 36" overhangs with decorative brackets below</li> <li>• Concrete "s" or barrel roof tile</li> </ul>	<ul style="list-style-type: none"> <li>• Main hip roof with minor intersecting gable roof</li> <li>• 12" to 36" overhangs</li> </ul>
Walls	<ul style="list-style-type: none"> <li>• Smooth stucco finish</li> <li>• Faux, precast or cut stone window and door surrounds</li> </ul>	<ul style="list-style-type: none"> <li>• Faux cut stone window and door surrounds</li> </ul>
Windows	<ul style="list-style-type: none"> <li>• Vertical windows often ganged together in doubles and triples</li> <li>• Deep recess in wall plane</li> <li>• Round top window over rectangular windows at feature locations</li> <li>• White vinyl wrapped aluminum frames</li> </ul>	<p>Single pane windows at sides and rear (except that the preferred options should be used for all four sides on corner lots of Multifamily buildings)</p> <ul style="list-style-type: none"> <li>•</li> </ul>
Color	<ul style="list-style-type: none"> <li>• Mid-value saturated</li> <li>• Contrasting trim - Tuscan hues</li> </ul>	<ul style="list-style-type: none"> <li>• Saturated deep Tuscan hues</li> </ul>
Details	<ul style="list-style-type: none"> <li>• Door and window header trim match</li> <li>• Massive chimney</li> <li>• Tower element</li> <li>• Black wrought iron hardware and light fixtures C</li> <li>• Belt course trim</li> <li>• Classically styled columns</li> </ul>	<ul style="list-style-type: none"> <li>• Tapered round or square simple stucco or pre-cast concrete columns</li> <li>• Colonial arch window trim</li> <li>• 2nd story balconies in wrought iron</li> <li>• Entry porches on ground floor units accented by columns</li> <li>• Shutters</li> </ul>

**e. Monterey**



Style Elements	Preferred	Allowed
Form	<ul style="list-style-type: none"> <li>• Single gable roof side to side</li> <li>• Two story horizontal massing, simple form</li> <li>• 2nd story balcony across the front elevation</li> </ul>	<ul style="list-style-type: none"> <li>• Vertical massing held to 25% to 35% of front elevation</li> <li>• Horizontal and vertical massing breaks</li> </ul>
Roof	<ul style="list-style-type: none"> <li>• 6:12 to 8:12 normal main roof pitch</li> <li>• Balcony shed roof break at 3 ½ to 4 ½ slope</li> <li>• Shingle texture flat tile or “s” concrete roof tile</li> <li>• Tight rakes and extended eaves with shaped rafter tails</li> </ul>	<ul style="list-style-type: none"> <li>• Main hip roof with balcony shed roof break at 3 ½ to 4 ½ slope</li> <li>• Overhangs with continuous fascia board</li> </ul>
Walls	<ul style="list-style-type: none"> <li>• Smooth stucco finish</li> <li>• Horizontal or vertical siding at second floor balcony wall</li> <li>• Horizontal or vertical siding accents at gable ends</li> </ul>	<ul style="list-style-type: none"> <li>• All smooth stucco walls with horizontal or vertical siding accents at gable ends</li> </ul>
Windows	<ul style="list-style-type: none"> <li>• Vertical window shape with multiple panes often used in groupings</li> <li>• Wood single hung</li> <li>• Gothic shape feature window at first floor</li> <li>• Colonial window and door trim</li> <li>• White vinyl wrapped aluminum windows</li> </ul>	<ul style="list-style-type: none"> <li>• Curved top feature window</li> <li>• Simple window trim at sides and rear</li> <li>• Small beveled or stain glass accent window at front elevation</li> </ul>
Colors	<ul style="list-style-type: none"> <li>• White toned bodies</li> <li>• Wood toned trims</li> </ul>	<ul style="list-style-type: none"> <li>• Medium range earth tones</li> </ul>
Details	<ul style="list-style-type: none"> <li>• Wood framed balcony with knee brace posts</li> <li>• 4" - 6" diameter round gable tile vents</li> <li>• Simple wood column detail</li> <li>• Arched and sloping fin walls</li> <li>• Shuttered windows</li> </ul>	<ul style="list-style-type: none"> <li>• Brick veneer wainscot at base of building</li> <li>• Chimney top detailing</li> <li>• Eclectic accent details</li> </ul>

**e. Monterey – Additional Guidelines Specific to Multifamily and Mixed-Use Dwelling Units**

Style Elements	Preferred	Allowed
Form	<ul style="list-style-type: none"> <li>• Minimum 2 stories, Maximum 3 stories / 45' (RF)</li> <li>• Minimum 2 stories, Maximum 8 stories / 85' (MU)</li> <li>• Four Sided Architecture (RF and MU)</li> <li>• Balconies across visible elevations and throughout the height of the building to distinguish individual units</li> </ul>	<ul style="list-style-type: none"> <li>• Four sided architecture</li> </ul>
Walls	<ul style="list-style-type: none"> <li>• Horizontal or vertical siding at balcony walls throughout the height of the building</li> </ul>	<ul style="list-style-type: none"> <li>• All smooth stucco walls with horizontal or vertical siding accents at gable ends</li> </ul>

## f. Spanish Colonial



Style Elements	Preferred	Allowed
Form	<ul style="list-style-type: none"> <li>Simply articulated two story boxed plan with a 50% one story element across front elevation</li> <li>Deep recessed arches</li> </ul>	<ul style="list-style-type: none"> <li>Two story massing with one story elements</li> <li>Boxed out elements at 2nd floor</li> <li>Varied plan forms</li> </ul>
Roof	<ul style="list-style-type: none"> <li>3 ½:12 to 4 ½:12 low roof pitch</li> <li>Predominately gable side to side roof with forward facing hip roof element</li> <li>Unenclosed eave overhang with elaborate exposed rafter tails</li> <li>Concrete “s” or barrel tiles</li> <li>Overhangs with tight rakes and 12" eaves</li> <li>Exposed rafter tails</li> </ul>	<ul style="list-style-type: none"> <li>Boxed in overhangs with tapered stucco soffit</li> <li>Continuous fascia boards</li> <li>Predominately hip roof with cross gable element</li> </ul>
Walls	<ul style="list-style-type: none"> <li>Smooth stucco finish</li> </ul>	<ul style="list-style-type: none"> <li>Sand stucco finish</li> </ul>
Windows	<ul style="list-style-type: none"> <li>9 and 12 pane windows with colonial accent trim</li> <li>Rectangular shape feature windows on visible elevation - single or ganged curve top shape</li> <li>White vinyl wrapped aluminum windows</li> </ul>	<ul style="list-style-type: none"> <li>Rectangular, simplified mullion windows at sides and rear</li> <li>Recessed windows</li> </ul>
Colors	<ul style="list-style-type: none"> <li>White toned body</li> <li>Wood toned trims</li> </ul>	<ul style="list-style-type: none"> <li>Mid-value body with contrasting fascia and window trim colors</li> </ul>
Details	<ul style="list-style-type: none"> <li>Ornate black wrought iron accent details at walls, windows, light fixtures, and gutters</li> <li>Carved wood style entry door</li> <li>Full length porch</li> <li>Black wrought iron balconies</li> <li>Decoratively shaped rafter tails</li> </ul>	<ul style="list-style-type: none"> <li>Stucco over foam window and door trim (not on corner lots of Multifamily buildings)</li> <li>Beveled glass accent window</li> <li>Decorative tiles recessed in wall plane</li> </ul>

### f. Spanish Colonial – Additional Guidelines Specific to Multifamily and Mixed-Use Dwelling Units

Style Elements	Preferred	Allowed
Form	<ul style="list-style-type: none"> <li>Minimum 2 stories, Maximum 3 stories / 45' (RF)</li> <li>Minimum 2 stories, Maximum 8 stories / 85' (MU)</li> <li>Four Sided Architecture (RF and MU)</li> </ul>	Simple massing with one story elements across visible elevations Boxed out elements at floors above the ground floor Four sided architecture w/ varied plan forms
Details	<i>Duplex/Triplex/Quadplex and Townhouse only</i> Porches should distinguish individual ground floor units	<i>Duplex/Triplex/Quadplex and Townhouse only</i> Entry Porches

### **8.3.2 Colors and Materials**

The colors and materials used within the Colonies at San Antonio residential planning areas should reflect a general theme of environmental harmony with the surrounding community, topography, and neighborhood character. The palette of architectural styles selected for the residential guidelines contributes to this goal, as each style has an historic lineage that shares this “common sense” approach to the use of materials and colors indigenous to the region. The use of elements such as terra-cotta color roof tile, white washed plaster, painted siding, and black wrought iron provides variety and contrast. Color palettes that reflect traditional architectural themes are the basis for successful modern interpretations. New interpretations of these classic combinations of materials and inherent colors are encouraged as they relate to a general feeling of neighborhood unity.

A variety of natural materials and earth tone-based colors provides the diversity required for visual interest while unifying the buildings with their settings and creating a timeless appeal. The primary purpose of the architectural style palette selection is to avoid monotony, provide a variety of colorful schemes, and promote visual diversity. To further this goal of diversity, the following criteria should be met.

#### **a. Single Family Detached Dwelling Units**

- Each elevation shall have a minimum of three colors, four is preferred (i.e., one body, one trim, and two accent colors).
- Front elevation design and materials shall wrap the side elevation to an appropriate transition point.
- Each planning area shall have a minimum of three different roofing colors.
- No adjacent dwelling unit shall have the same color scheme.

#### **b. Multifamily and Mixed-Use Dwelling Units**

- Elevations shall have a minimum of four colors (i.e., one body, one trim, and two accent colors).
- As all building elevations are typically visible, they shall exhibit a similar level of architectural expression in the design and materials along all facades of the building.

#### **c. All Dwelling Units**

- Material samples and manufacturers cut sheets shall be submitted to the Community Development Department at the preliminary design phase.
- Individual color schemes shall be appropriate to the architectural styles selected, with a harmonious selection of accent materials and roof profile and colors.

#### **d. Presentation Drawings**

- Foam trim shall be limited in areas subject to damage.
- Applicant shall indicate on presentation drawings where foam trim is to be used.

## e. Recommended Brick and Stone Products

### Stone Manufacturers and Type\*

El Dorado - Copper Mountain LedgeStone  
Cultured Stone - Chardonnay Drystack LedgeStone  
Cultured Stone - Walnut Southern LedgeStone  
El Dorado - Buckskin Mountain LedgeStone  
El Dorado - Yosemite Creed Cobble  
El Dorado - American Blend River Rock  
El Dorado - Sierra Creek Cobble  
El Dorado - Sonoma Creek Cobble

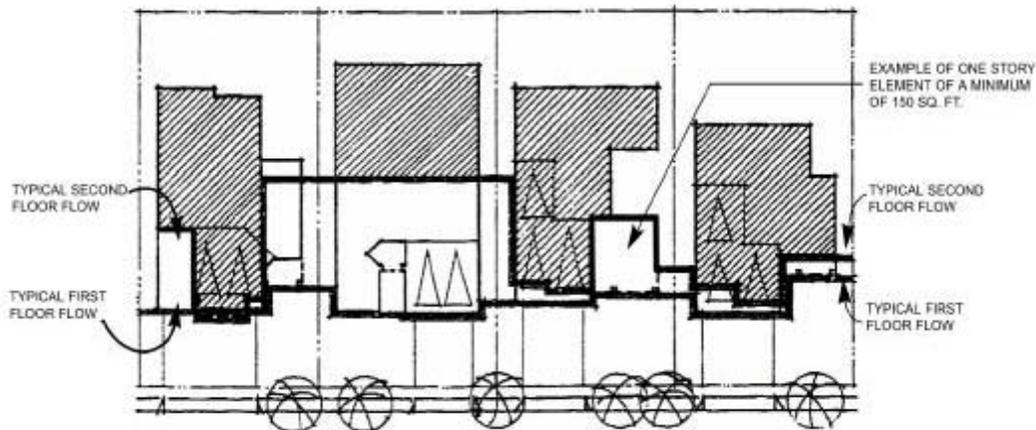
### Brick Manufacturers

Robinson  
Interpace  
Acme  
Interstate  
Muddox  
Boral

\* Subject to manufacturers identification name revisions

## 8.3.3 Massing and Plotting – Single Family Detached Dwelling Units

Creating a street scene that functions well and has visual variety is a key community objective. The following massing and plotting criteria are intended to develop variation in appearance and a sense of individuality for each dwelling. Projects with nearly identical dwelling units and streets without variation in placement and form are discouraged. This section will list massing and plotting techniques and standards that help to create a quality environment.



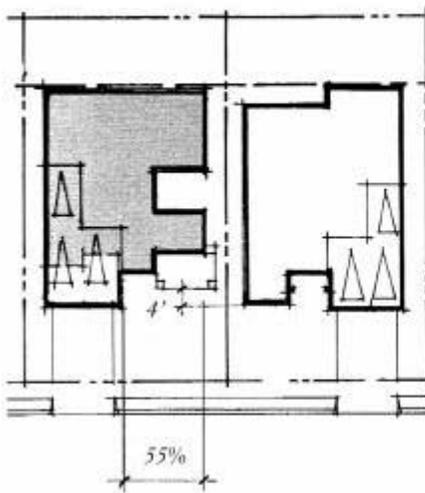
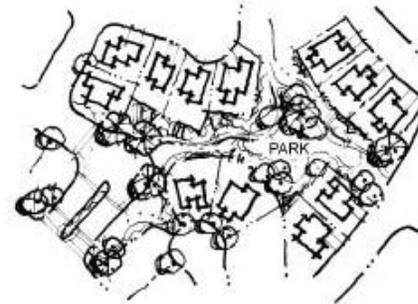
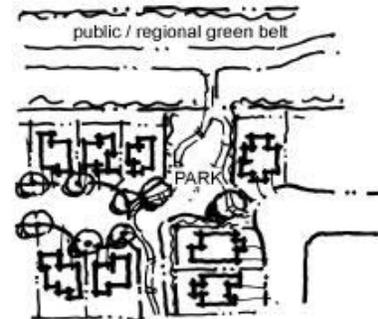
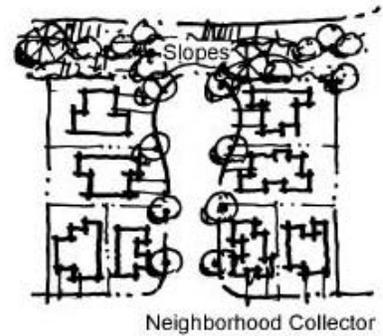
### a. Vary Massing

- Encourage single-story elements to create a sense of variety within the street scene. Utilize porches, reduced height of living spaces and/or garage areas to create single story components. Fifty percent of all plans and elevations plotted shall have a significant single story element (significant single story elements should be a minimum of 150 square feet) and corner plotted units shall have significant single story elements to the corner sideyard (i.e., wrap around porch element could be considered).

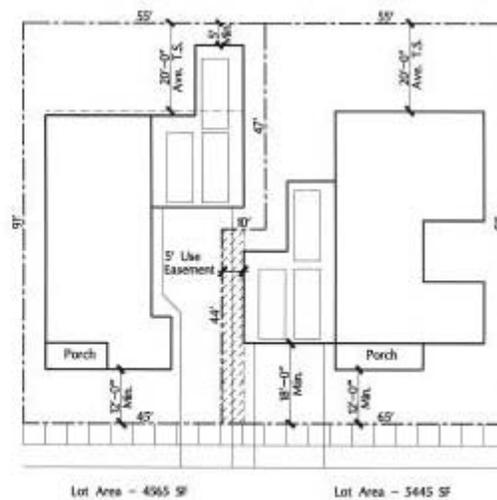
- Provide variable setbacks to different parts of the dwelling unit to encourage vertical and horizontal massing breaks, in addition to alternative garage layouts and alternate plan configurations.

**b. Plotting**

- Reduce the apparent scale and size of each neighborhood (planning area) by creating "sub-neighborhoods" through the use of eyebrow clusters, short street segments, short cul-de-sac streets, grade separations between housing clusters, and other clustering concepts.
- Plotting alternatives such as “zero lot line” and “z-lot” patterns may be used to add variety (as long as applicable development regulations are met).
- Plot alternative plan types and/or elevation styles on contiguous lots.
- At least one out of every three contiguous lots, provide a 4'-0" setback to adjacent houses for a minimum of 55 percent of the house width to create variable front yard setback.
- When plotting, refrain from strict compliance to minimum garage setbacks to avoid contributing to a repetitious street scene. Typically, plans should be reversed and plotted so that garages and entries are adjacent to each other to create an undulating setback. Occasionally, this pattern should be broken to avoid monotony.



Example of a 4-foot additional setback to create variable front yard setback.



Example of a “z-lot” pattern (see footnote 8 in Table 7.D in Development Regulations for applicable standards).

### 8.3.4 Rear Articulation - Single Family Dwellings

**a. Vertical and Horizontal Plane Breaks.** The second story portion of the rear elevation of all structures that back onto either roads, public open space, or are elevated more than 20 feet above the rear or side of the adjoining building pad shall incorporate varied wall planes, roof forms and orientation, and window treatments.

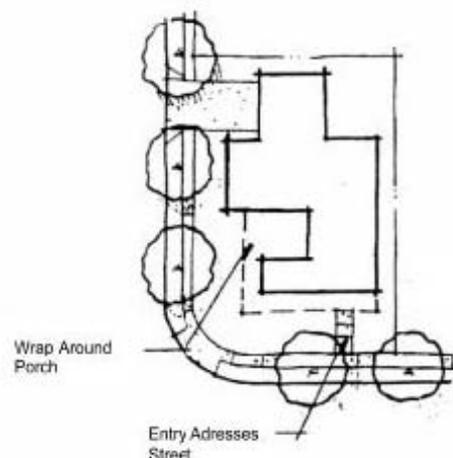
**b. Roof Forms.** Rows of dwelling units seen from a distance or along arterial roads are perceived by their contrast against the background or skyline. The dominant impact is the shape of the building and roof line. Articulate the rear elevation and roof plane to minimize the visual impact of repetitious flat planes, similar building silhouettes, and similar ridge heights. Provide varied rear elevation forms as outlined in the table below.

*Two-Story Massing Types at Rear Elevations*

TYPE	TYPICAL EXAMPLES	RESTRICTIONS
<b>A</b> REAR FACING SLOPE WITH GABLE INTERSECTING		80% OF REARS MAXIMUM
<b>B</b> SIDE TO SIDE GABLES		40% OF REARS MAXIMUM
<b>C</b> VARIED REAR FACING SLOPE WITH SOME INTERSECTING HIP ELEMENTS		80% OF REARS MAXIMUM

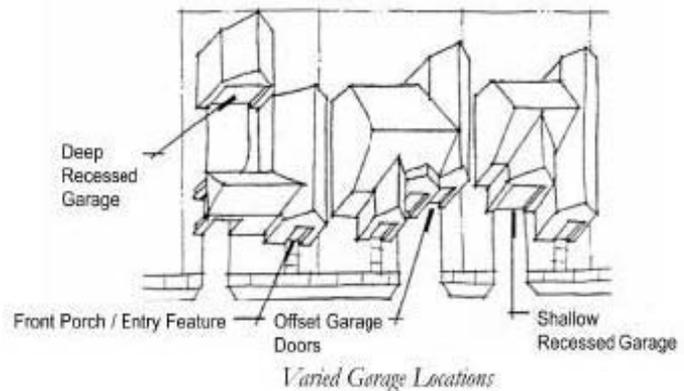
### 8.3.5 Corner Lots – Single Family Dwellings

Encourage development of at least one plan per planning area designed specifically for corner lots where more than 10 percent of the lots provide corner or side-on conditions. This type of dwelling requires wrap-around architecture, the ability to turn the garage for side entry, and significant one story elements on the corner sideyard.

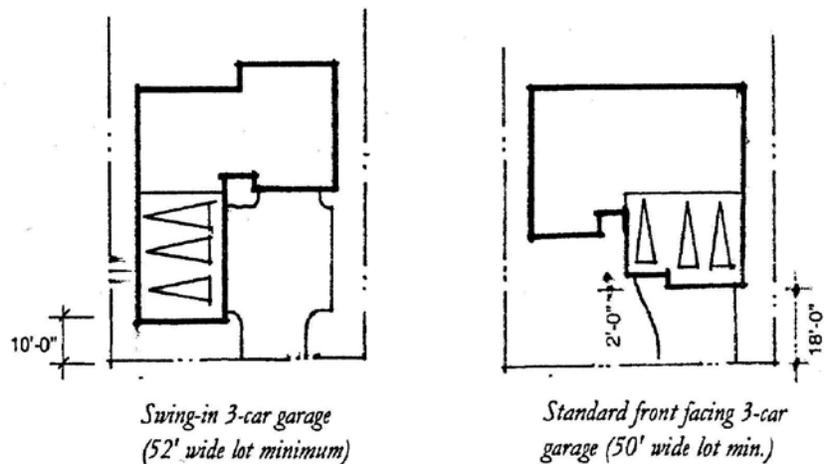


### 8.3.6 Garage Treatments – Single Family Dwellings

- The dwelling and front yard, rather than the garage, should be the primary emphasis of the front elevation. Dwelling designs shall place entries, windows, front porches, and living areas directly adjacent to the street on most plan variations, while recessing the garages.
- Minimize the impact of garages facing the street by varying garage door patterns and utilizing a deep recessed door, varying colors, splitting doors into two single doors and using alternative garage configurations, such as corner lot garages and alley loaded garages.

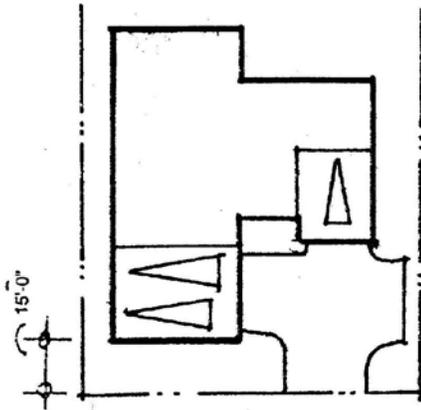


- Swing-in garages are prohibited on lots less than 50 feet wide. It is recommended that a minimum lot width of 52 feet be used.
- The side elevations of garages facing the street should have architectural detailing.
- The three car garage configuration requires careful consideration to de-emphasize its impact on the street. The standard front facing three car garage layout shall not be used for a lot width less than 50 feet.
- The following design techniques shall be incorporated into dwellings with three car garages:
  - Where allowed, the standard front facing three-car configuration shall be used in a maximum of 33 percent of plan configurations. Refer to the alternative three car garage layouts for other acceptable garage configurations to be used.

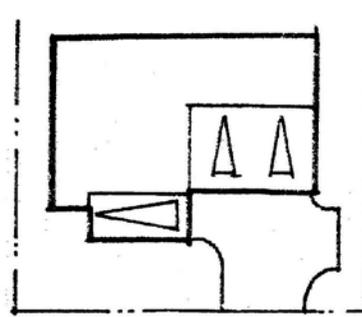


- When the standard front facing three car configuration is used, it shall be combined with one of the following measures:
  - Provide a minimum of 50 square feet of porch area.
  - Extend a portion of the dwelling a minimum of 2'-0" in front of the garage.
  - Offset the dual car garage door 2'-0" from the single car garage door.
  - Provide three single car garage doors.
  - Recess all garage doors a minimum of 12 inches.

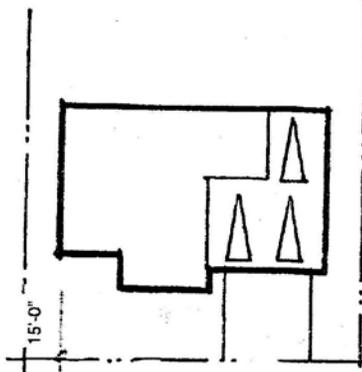
Alternative Three Car Garage Layouts



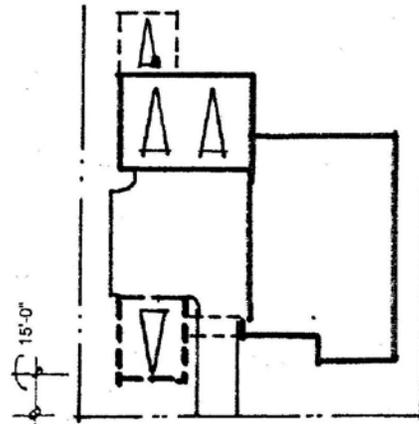
A.  
Split 3-car garage  
(52' wide lot minimum)



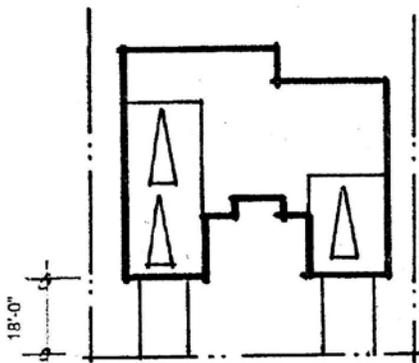
B.  
Split 3-car garage  
(52' wide lot minimum)



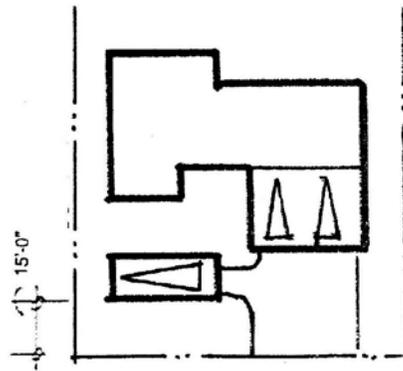
C.  
3-Car Garage  
Tandem Garage



D.  
Deep Recessed 3 Car Garage



E.  
Split 3-Car Garage  
Tandem Garage

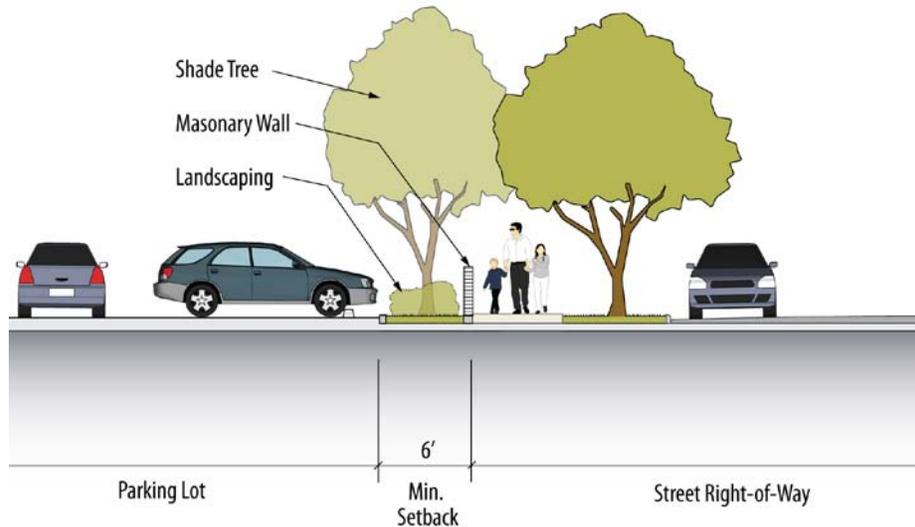


F.  
Detached 3-Car Garage  
(52' Wide Lot Minimum)

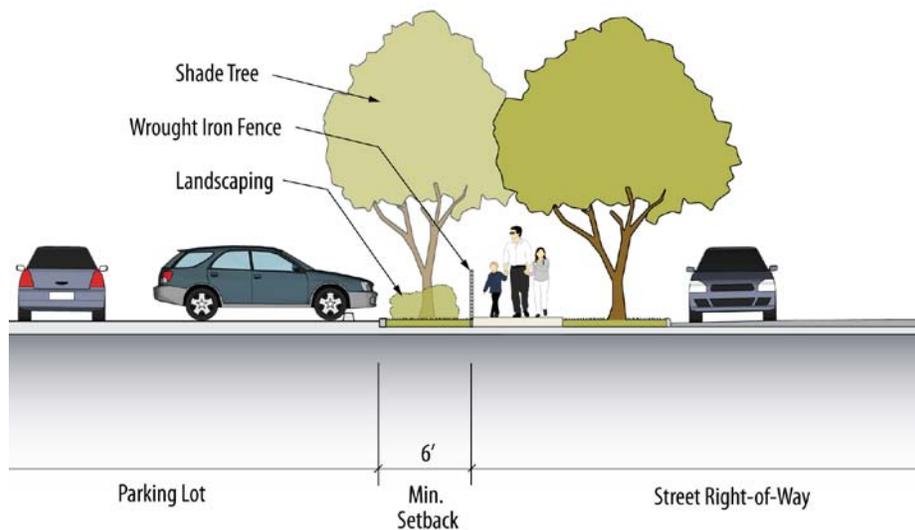
### 8.3.7 Multifamily Residential and Mixed Use Concepts.

- **a. Site Planning/Hardscape.** Good site planning combined with other design elements can create an inviting atmosphere for a residential development. Landscaped courtyards or entries that open out to the street provide an inviting visual interest as well as pedestrian access into the interior of the development. The following guidelines have been established to ensure the creation of a balanced and well thought out development:
- When the opportunity exists, the main residential structures shall have frontage on a street with pedestrian access.
- Entries to residential units shall be oriented to streets or public and private open spaces when the opportunity exists. First floor entries oriented toward alleys, parking lots, or streets with heavy traffic should be avoided, or otherwise buffered in an acceptable manner.
- Long straight driveways visible from the street should be avoided. If unavoidable, the driveways must contain design features including landscaping and textured/colored paving rather than just continuous asphalt or concrete.
- Parking lots shall be screened from view from public streets, using a combination of a decorative wall or hedge and bermed planters.
- Pedestrians shall be protected from water run-off from roofs and balconies through the use of rain gutters, or proper location of walkways.
- Security lighting shall be provided for driveways and parking areas. Pedestrian walkways shall be well-lit. The design of the lighting shall be compatible with the building architecture, and glare onto adjacent properties shall be avoided. Low-level architectural lighting of the buildings and landscaped areas is encouraged.
- Trash enclosures shall be compatible with the overall architectural design of the development. The location shall be convenient for tenant use as well as for trash pick-up service.
- The relation of the multi-family and mixed use areas to the adjacent single-family residential areas will be considered. Site plans will be laid out to minimize impacts to existing residents.
- All entrances to buildings not directly fronting a public street shall be accessed by internal walkways or sidewalks on private streets. All internal pedestrian ways shall lead to and be directly connected with the sidewalks on the adjoining public streets as to provide a continuous, uninterrupted pedestrian network.
- Where possible, internal vehicular connections should be designed as private streets that resemble public streets, rather than driveways. Sidewalks should be provided on both sides of the street, unless buildings abut the street on one side only, in which case a sidewalk should be provided at minimum on the building side of the street. Sidewalks should be separated from the roadway by continuous parkways at minimum 5 feet in width and planted with street trees with a spacing of 25 to 30 feet. Parallel on-street parking is encouraged to provide additional parking and a buffer between pedestrians and moving vehicles.
- Parking lots shall provide safe and comfortable pedestrian access. In no instance shall the distance between a walkway and a parking stall exceed 75 feet, measured from the centerline of the walkway to the farthest point of the parking stall. Walkways shall be separated from parking stalls and/or drive aisles by a landscape buffer of at minimum five feet in width. Shade trees planted in the landscape buffers are encouraged with a spacing of 25 to 30 feet.

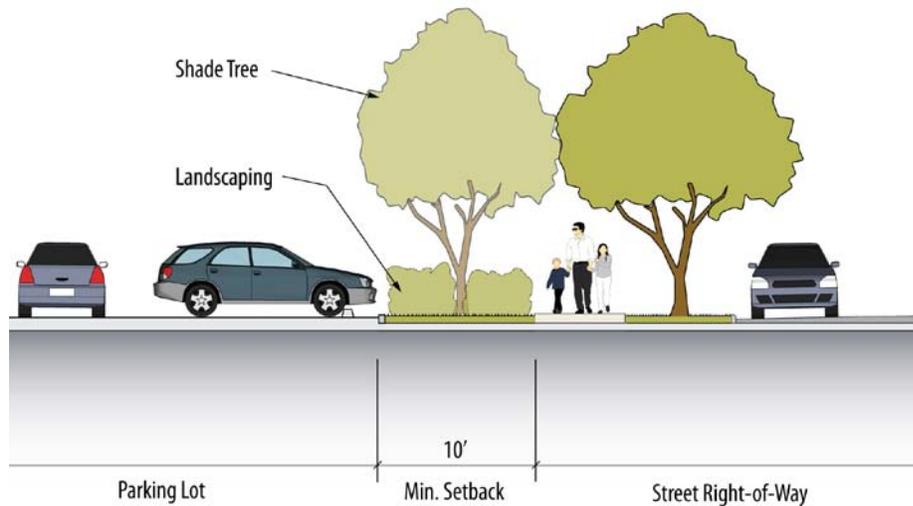
- Where a parking lot abuts a public street the parking lot shall be screened from the public realm in one of the following ways:
  - Pavement shall be set back from the back of sidewalk a minimum of six feet. A decorative masonry wall a minimum of 30 inches and a maximum of 42 inches in height shall be placed at the back of sidewalk providing a continuous screen. The setback area shall be landscaped with groundcovers, grasses and/or shrubs no more than 42 inches in height at maturity. In addition, shade trees shall be planted in the setback area spaced at about 30 feet on center. (Typical Parking Lot/Street Interface A). Or,



- Pavement shall be set back from the back of sidewalk a minimum of six feet. A continuous screen shall be created through a combination of a wrought iron fence (or steel fence faithfully imitating wrought iron - see Section 3.7, Architectural Standards) and landscaping. The fence shall be a minimum of 30 inches and a maximum of 42 inches in height and be placed at the back of sidewalk. Landscaping may consist of vines, shrubs and hedges that provide an evergreen screen a minimum of 30 inches and a maximum of 42 inches in height at maturity. In addition, shade trees shall be planted in the setback area spaced at about 30 feet on center. (Typical Parking Lot/Street Interface B). Or,

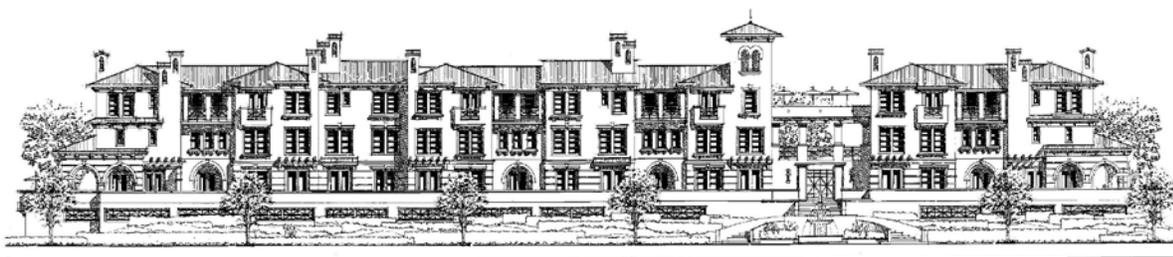


- Pavement shall be set back from the back of sidewalk a minimum ten feet. The setback area shall be landscaped with shrubs and/or hedges providing a continuous, evergreen landscape screen. At maturity all plant material shall be 30 inches minimum and 42 inches maximum in height. In addition, shade trees shall be planted in the setback area spaced at about 30 feet on center. (Typical Parking Lot/Street Interface C).



**b. Articulation of Building Form and Elevations.** In the case of multifamily and mixed-use buildings, there is a tendency to "build out" to the maximum building envelope without utilizing articulated massing or treatment on wall planes. This results in a multi-story box, without vertical or horizontal relief. In an effort to prevent this, the following section includes additional design considerations for attached building types.

A variety of stacked massing arrangements in keeping with the selected architectural style should be used to vary individual buildings. Stepped down elements should be provided at entries and building corners to provide a transition to ground level and adjacent buildings and to enhance the pedestrian scale of those spaces.



Building massing should also be stepped in response to the site and/or to adjacent buildings. Where site elevations vary between building clusters, particular attention must be paid to building elevations as they are exposed at grade. For multifamily developments, a maximum of three visible stories of living space and a half-story of visible podium parking (below living space) is permitted. For mixed-use developments, commercial use and retail use shall be allowed at the ground floor with residential above, up to 8 stories or 85'. Any visible exposed parking structures must be landscaped so as to reduce their visibility.

In most cases, all elevations are typically visible; therefore, they must be treated with adequate articulation, materials, and color in keeping with their individual architectural styles to enhance their

appearance (see individual style descriptions, Section 8.3.1, Architectural Diversity). It is required that all four building elevations share common materials and degrees of articulation. Large, blank expanses of wall are to be avoided. The following suggests a number of ways to accomplish this.

- Create single story projections at entries and porches, etc. to vary both vertical and horizontal planes.
- Improve the articulation of the plan forms by offsetting upper floor and wall planes.
- Enhance usable and accessible private and common open space by recessing or projecting the plan and elevation.
- Provide enhanced trim and details at primary focal points such as building entries, doors, porches, or patios to emphasize their importance.
- Minimize large, blank masses through articulation of surfaces and wrapping of trim.
- Design unique window treatments including shutters, awnings, etc., to provide sources of shade, articulate wall surfaces, and contribute to the architectural character of the building and the neighborhood.
- Use elements appropriate to the style, such as false shuttered windows, decorative louvered vents, and horizontal banding to further articulate and characterize building elevations.



### c. Design Elements

**(1) Entries.** The entry serves several important architectural and psychological functions. It creates an initial impression, locates and frames the doorway, acts as an interface between public and private spaces, and further identifies individual unit entries. The entry design should emphasize these prime functions.

- Wherever possible, site plans should orient the front door image and principal access toward the public street.
- Appropriate roof elements, columns, feature windows, and/or architectural forms should be incorporated into the entry statement to emphasize the building character and the location of individual doorways.
- If the front entry location is not immediately obvious due to building configuration, the observer should be directed and drawn to it with added elements such as signs, lighting, and landscape.

**(2) Windows.** Typically, the location of windows is determined by the practical considerations of room layout, furniture placement, views, and privacy. Design emphasis should be of particular concern as windows play an important role in the exterior architectural character of multi-family buildings.



- Within the appropriate style requirements, group and coordinate windows with other design elements to create a composition and sense of order.
- Where possible and appropriate to style, use of multi-paned windows is encouraged.

**(3) Balconies.** The inclusion of balconies is encouraged for both aesthetic and practical purposes. They are useful in breaking up large wall planes, offsetting floors, creating visual interest, and adding human scale to the building. They provide the practical advantage of creating outdoor living areas and elevated open space.

- Balconies may be covered or open. They may be either recessed into the mass of the building or serve as a projecting element.
- Design balconies as an integral element of the building using details, eaves, supports, and railings in keeping with the architectural style and elements of the building.
- Avoid designing and plotting plans with balconies located side by side.



**(4) Landscape.** Landscaping provides important visual relief from the hard-surfaced built environment. The blending of buildings and landscaping not only compliments the architecture of the buildings, but also provides human scale to larger structures and generally enhances the usefulness and appeal of properties. It can also visually lower the height of taller structures, relieve the bareness of large blank walls, and reduce the harshness of expansive areas such as parking lots.



- Plans shall include provisions for the placement of trees around the perimeter of the property. Large, more open type trees are preferred to denser trees.
- Landscaping shall be used as a design element to the project and reflect a hierarchical design. The locations and type of trees, shrubs, flowers, and groundcovers must be complimentary to the adjacent structures. The use of undulating berms shall be incorporated into the design.
- Trees shall be provided within parking areas for shading and to relieve the harshness of the expanse of paving and walls.

**(5) Open Space Amenities.** The enjoyment of outdoor living is encouraged and shall be a traditional part of life in the Colonies at San Antonio. Open space and common useable open areas for all multifamily and mixed-use developments is highly important for the benefits of the residents.

- The arrangement of open space should be appropriate in its design and function. The common open space must be centrally and conveniently oriented to the residential units and shall be in one large area in order to provide ample space for amenities. Private open space shall be designed as an extension of the indoor living area, providing an area that is useable and has some degree of privacy.
- Amenities for the common open space area shall be provided for the active and passive use by the tenants, and must reflect the scale of the project. Some of those amenities may include:



- Children’s playground with equipment and/or paved area with bench seating
- Barbeque
- Gazebos/patio covers

**(6) Fences.** Yards adjacent to a street represent a transitional area between a public corridor and the private and semi-private space of multifamily and mixed-use developments. Fencing is a further means of separating this public and private space and providing a greater sense of security and privacy for the residents. Yards adjacent to public corridors shall provide and maintain an open and friendly atmosphere within its multifamily and mixed-use residential developments.



Fence designs shall include the use of decorative elements and durable materials that compliment the residential quality of the development and the surrounding neighborhood. This would include:

- Use of materials such as brick, stone, decorative block (such as slump-stone, split face, or common block with a stucco finish)
- Decorative wrought iron or tubular metal
- Fencing shall be in subdued colors that compliment or incorporate the building colors of the development.

Within yards adjacent to a street, fences (including retaining walls) shall be located no closer than 3 feet from the property line, with landscaping to be provided adjacent to the public sidewalk. Fencing directly adjacent to a public sidewalk shall be avoided.

- For larger complexes, fencing along a street must be staggered to break up long, linear stretches of fencing.

- Low shrubbery, in combination with vines and other accent plants shall be planted on the street side of any fence facing a public street in order to soften the edge of the fencing and help integrate it into the landscaped yard.

All fences (including retaining walls) shall have a maximum height of 6 feet. A fence or wall within any front setback shall maintain a 42" height limitation to avoid sight obstruction.

### (7) Secondary Elements.

- It is important that columns and posts project a substantial image and are durable to withstand local climatic conditions. Where appropriate to style, it is desirable that columns incorporate both base and capitals.
  - Columns or posts, whether square, round or rectangular, should have a substantial and proportionate ratio of height to width.
  - Posts should be constructed of wood, whereas columns may be clad in masonry or stucco.
- The type of exterior railing used on balconies, decks, and stairs creates a significant impact on the elevation. Choice of rail type should reflect good taste and be consistent with the chosen architectural style and building design.
  - Allowable types and materials for railings include closed stucco, open wrought iron and wood picket, wood rail, tubular metal, and pre-formed concrete.

The design and details of railings will be reviewed for their appropriateness and compatibility with the chosen architectural style.



### (8) Garages.

- Locate garages below primary living areas partly or completely below grade levels. This is intended to help accommodate parking requirements while reducing the visual emphasis of the garage.
- Set the garage back in relationship to the face of the building to reduce the overall visual mass of the garage.
- When designing garage structures, use the same style as the primary buildings, as well as compatible materials. This will help to tie garage facilities into the overall project design while reducing their visual emphasis.

**(9) Parking.** In the case of multifamily and mixed-use buildings adjacent to primary streets, the focus of the multifamily buildings should be their street front image and pedestrian access. While accommodating vehicular and parking needs, the intent should be to de-emphasize the presence of the automobile.

Each project shall incorporate interior oriented parking solutions and use the following design techniques to enhance the architecture of the street scene:



- Locate garage and parking areas interior to the site off of interior vehicular access roads or drives.
- Where the site orientation may dictate alternate solutions, turn the short side of parking courts to the street to avoid lengthy parking areas adjacent to the street.
- Where parking areas are visible from the street, these areas should be screened from view with landscape and/or architectural solutions.
- Distribute resident parking on site to provide as close proximity as possible to individual units.
- Guest parking should typically be within 200 feet of any unit entry.
- Group open/guest parking and clearly identify guest versus tenant spaces for adequate coverage and convenience.

**d. Building and Frontage Type Standards.** The standards in this section determine the design requirements for each of the building types allowed in the Colonies at San Antonio Planning Areas 4, 15, and 22, including building size and massing, frontage design, primary pedestrian access, vehicle access, and parking and services. A parking study will be required for PA 4 to demonstrate that adequate parking is being provided. The study will allow that parking for both commercial and residential uses to be integrated in the same area, dependent on the parking analysis.

**(2) Allowed Building Types.** The following table indicates which building types are allowed in each of the Planning Areas. Each of the building types is described in detail below.

Building Type	PA-4	PA-15	PA-22
Mixed-Use Building	Y	N	N
Apartment Building	Y	N	Y
Courtyard Housing	Y	N	Y
Villa	N	Y	N
Duplex/Triplex/Quadplex	N	Y	N
Townhouse	N	Y	N
Single Family	N	Y	N

**(3) Mixed-Use Buildings.** A Mixed-Use Building is designed for occupancy by a minimum of two different uses that may be vertically or horizontally combined. Uses generating visitor or customer traffic (such as retail, restaurants, personal services) shall be located on the ground floor facing the sidewalk, whereas residential uses shall be located on upper floors or behind street fronting commercial uses. Residential units may consist of any of three dwelling types: flats, townhouses, and lofts.



- Building size and massing.

- Buildings shall be principally composed of three to four-story volumes.
- Facades shall be composed of increments of 25 feet or less. Increments may be created through projecting or recessing wall surfaces, changes in roofline and/or placement of piers and pilasters.
- Building facade lengths shall not exceed 100 feet without a vertical setback from the base of the building to the roof line of at least 18 inches in width and depth, giving the building an appearance of multiple attached buildings.



- Frontage

- The transition from public to private, indoor to outdoor at the main entrance to each use shall be created by appropriate frontages. These include shopfronts, arcades, galleries, and forecourts.
- Retail or restaurant guest areas shall be oriented toward the street, rather than service or storage rooms.



- Primary pedestrian access

- The main entrance to a Mixed-Use Building’s ground floor commercial space shall be located within the facade and accessed directly from the street through an allowed frontage.
- Access to upper story commercial space or dwelling units shall be through a street level lobby and/or corridors accessed directly from the street.

- Vehicle access, parking and services

- Vehicular access shall be provided through an alley, where available. Where an alley is not present, vehicle access may be provided through a driveway with a minimum width as determined by the Fire Department, and with five-foot minimum planters on each side.
- Parking may be provided in a garage, subterranean garage, parking structure, carport, uncovered, or a combination of any of the above. Required parking spaces shall be located behind or beneath the building.
- Parking ratios for sharing parking spaces between commercial and residential uses shall be determined in a parking study.
- Where present, entrances to subterranean or structured parking shall be located to the side or rear of the lot.
- Services, including all “dry” utility access, above-ground equipment, and trash containers, shall be located on the alley where applicable.

**(4) Apartment Building.** The Apartment Building is a multi-dwelling building with one common main entrance and lobby. The Apartment Building consists of stacked flats that are accessed from the lobby via one or multiple corridors.



- Building size and massing.
  - Buildings shall be principally composed of two to four-story volumes.
  - Building elevations exceeding 25 feet in length shall be designed to provide at least one vertical break created through projecting or recessing wall surfaces, changes in the roofline, and/or placement of piers, pilasters or chimneys.
  - Building façade lengths shall not exceed 100 feet without a vertical setback from the base of the building to the roof line of at least 18 inches in width and depth, giving the building an appearance of multiple attached buildings.
- Frontage.
  - The transition from public to private, indoor to outdoor at the main entrance to the building shall be created by appropriate frontages. These include stoops, dooryards and forecourts.
  - First floor living areas shall be oriented toward the front of each dwelling unit rather than sleeping and service rooms.
- Primary pedestrian access.
  - The main entrance to the lobby of an Apartment Building shall be located within the facade and accessed directly from the street through an allowed frontage.
  - Each dwelling unit within an Apartment Building shall be accessed directly from the lobby, by a corridor or by a central stairway located in the lobby.
- Vehicle access, parking and services.
  - Parking and services shall be accessed from an alley.
  - Parking may be provided in a garage, subterranean garage, parking structure, carport, uncovered, or a combination of any of the above. Required parking spaces shall be located behind or beneath the building.
  - Where present, entrances to subterranean or structured parking shall be located to the side or rear of the lot.
  - Where present, a podium shall be raised no more than five feet above average grade to avoid physical separation of the courtyard(s) and the surrounding public realm.

- Services, including all “dry” utility access, above-ground equipment, and trash containers, shall be located on the alley.

**(5) Courtyard Housing.** Courtyard Housing is an arrangement of stacked and/or attached dwelling units around one or more common courtyards, which provide direct access to all dwelling units that do not directly front on a street. The courtyard is intended to be a semi-public space that functions as an extension of the public realm into the private lot. Courtyard Housing may be built at grade or on top of a podium with parking beneath.



- Building size and massing.
  - Buildings shall be principally composed of two to four-story volumes.
  - Building facades shall have an identifiable base, middle and top.
  - Buildings on corner lots shall be designed with two facades of equal architectural expression.
  - Building elevations exceeding 25 feet in length shall be designed to provide at least one vertical break created through projecting or recessing wall surfaces, changes in the roofline, and/or placement of piers, pilasters or chimneys.
  - Building façade lengths shall not exceed 100 feet without a vertical setback from the base of the building to the roof line of at least 18 inches in width and depth, giving the building an appearance of multiple attached buildings.
- Frontage.
  - For dwelling units facing the street the transition from public to private, indoor to outdoor at the main entrance to the building shall be created by appropriate frontages. These include stoops, dooryards and forecourts.
  - First floor living areas shall be oriented toward the front of each dwelling unit rather than sleeping and service rooms.

- Primary pedestrian access.
  - The main entrance to each Courtyard Housing unit shall be accessed directly from the street or the common courtyard(s) through an allowed frontage.
  - Common stairs may provide access from the courtyard to no more than three second-floor units. Stairs may be open or roofed, but not enclosed.
  - Each courtyard shall be directly accessed from the street.



- Vehicle access, parking and services.
  - Parking may be provided in a garage, subterranean garage, parking structure, carport, uncovered, or a combination of any of the above. Required parking spaces shall be located behind or beneath the building.
  - Where present, entrances to subterranean or structured parking shall be located to the side or rear of the lot.
  - Where present, a podium shall be raised no more than five feet above average grade to avoid physical separation of the courtyard(s) and the surrounding public realm.
  - Services, including all “dry” utility access, above-ground equipment, and trash containers, shall be located on the alley.

**(6) Villa.** The Villa is a small multi-dwelling building with one common main entrance and is designed to have the appearance of a large house. The dwelling units within a Villa may be arranged side by side or one on top of the other, or a combination thereof.



- Building size and massing.
  - Buildings shall be composed of two or two and one half-story volumes and have the appearance of a large house.
  - Building facades shall have an identifiable base, middle and top.
  - Buildings on corner lots shall be designed with two facades of equal architectural expression.
  - Building elevations exceeding 25 feet in length shall be designed to provide at least one vertical break created through projecting or recessing wall surfaces, changes in the roofline, and/or placement of piers, pilasters or chimneys.
  - Buildings shall not exceed 100 feet in length.

- Frontage.

- The transition from public to private, indoor to outdoor at the main entrance to the building shall be created by appropriate frontages. These include stoops, dooryards and forecourts.
- First floor living areas shall be oriented toward the front of each dwelling unit rather than sleeping and service rooms.



- Primary pedestrian access.

- The main entrance to the lobby of a Villa shall be located within the facade and accessed directly from the street through an allowed frontage.
- Each dwelling unit within a Villa shall be accessed directly from the lobby, by a corridor or by a central stairway located in the lobby.

- Vehicle access, parking and services.

- Parking and services shall be accessed from an alley.
- Parking may be provided in a garage, carport, uncovered, or a combination of any of the above. Required parking spaces shall be located behind or beneath the building.
- Services, including all “dry” utility access, above-ground equipment, and trash containers, shall be located on the alley.

**(7) Duplex/Triplex/Quadplex.** The Duplex, Triplex or Quadplex is a small multi-dwelling structure containing two, three or four separate units, respectively, on a single lot, each with its own entrance. The dwelling units within a Duplex, Triplex or Quadplex may be arranged side by side or one on top of the other, or a combination thereof.



- Building size and massing.
  - Buildings shall be composed of two or two and one half-story volumes and have the appearance of a large house.
  - Building facades shall have an identifiable base, middle and top.
  - Building elevations exceeding 25 feet in length shall be designed to provide at least one vertical break created through projecting or recessing wall surfaces, changes in the roofline, and/or placement of piers, pilasters or chimneys.
- Frontage.
  - The transition from public to private, indoor to outdoor at the main entrance to the building shall be created by appropriate frontages. These include porches, stoops, and dooryards.
  - First floor living areas shall be oriented toward the front of each dwelling unit rather than sleeping and service rooms.
- Primary pedestrian access.
  - The main entrance to each unit in a Duplex, Triplex or Quadplex shall be located within the facade and accessed directly from the street through an allowed frontage.
  - At corner lots at least one entrance each shall be required at both street frontages.
  - The entrances to all units shall be located at the ground floor level, even if the units are stacked. Exterior stairs to upstairs entrances are not allowed.

- Vehicle access, parking and services.
  - Parking and services shall be accessed from an alley.
  - Parking may be provided in a garage, carport, uncovered, or a combination of any of the above. Required parking spaces shall be located behind or beneath the building.
  - Services, including all “dry” utility access, above-ground equipment, and trash containers, shall be located on the alley.

**(8) Townhouse.** The Townhouse is a single-family house on a narrow lot and shares common walls with one or two of the adjacent units. Private yard space separates the dwelling unit in the front and the garage in the rear of each lot. The Townhouse is the most urban single-family type and is very compact, combining efficient land-use with the benefits of fee simple land ownership.

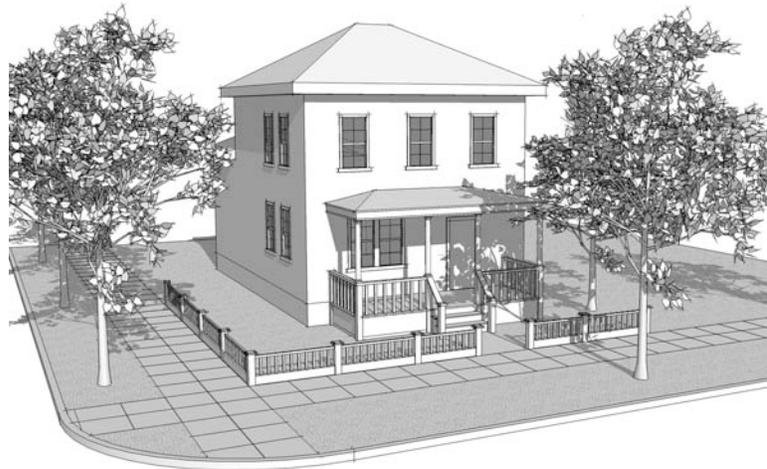


- Building size and massing.
  - Buildings shall be composed of two or two and one half-story volumes and have the appearance of a large house.
  - Building facades shall have an identifiable base, middle and top.
  - Groups of Townhouses may consist of two to six attached units.
  - Building elevations exceeding 25 feet in length shall be designed to provide at least one vertical break created through projecting or recessing wall surfaces, changes in the roofline, and/or placement of piers, pilasters or chimneys.
- Frontage.
  - The transition from public to private, indoor to outdoor at the main entrance to the building shall be created by appropriate frontages. These include stoops, dooryards and porches.
  - First floor living areas shall be oriented toward the front of each dwelling unit rather than sleeping and service rooms.

- Primary pedestrian access.
  - The main entrance to each Townhouse shall be located within the facade and accessed directly from the street through an allowed frontage.
- Vehicle access, parking and services.
  - Vehicular access shall be provided through an alley.
  - Parking may be provided in a garage, carport, uncovered, or a combination of any of the above. At least one required parking space shall be in a garage. Required parking spaces shall be located behind the building.
  - Services, including all “dry” utility access, above-ground equipment, and trash containers, shall be located on the alley.

**(9) Allowed Frontages.** For each type of building, different entrances and types of facades are appropriate. These are called frontages. Frontage types are described in detail below, and the allowable uses for each building type are detailed. A summary table is included at the end of this section.

- Porch. The Porch type offers the classic American front yard and consists of a porch that encroaches into the front setback, and an optional fence that delineates the property line. Fences are only permitted if there is at least a six foot setback between the property line and the face of the porch. Porches shall be at minimum eight feet deep to provide usable space, and typically occupy at least 50 percent of the facade width. However, narrower porches may be permitted in accordance with the building’s architectural style. Porches shall be raised above grade a minimum of 18 inches and a maximum of 36 inches. Fences enclosing the front yard shall not exceed 42 inches in height. Porches are allowed on Duplex/Triplex/Quadplex, Townhouse, and Single Family building types.



**(11) Dooryard.** Dooryards are elevated gardens or terraces that provide flexible outdoor space. Dooryards are typically landscaped to provide additional buffer and privacy. Dooryards are enclosed by low garden walls at or near the property line, with a few steps leading from the sidewalk to the elevated yard. Building facades are set back from the property line. Buildings are accessed directly from the Dooryards. Garden walls enclosing the Dooryard shall not exceed 42 inches in height, unless necessary for structural reasons. Garden walls may be constructed of stucco, brick, or stone; a transparent metal railing may be affixed atop a garden wall if additional height is necessary for safety. Dooryards are allowed on Apartment, Courtyard, Villa, Duplex/Triplex/Quadplex, Townhouse, and Single Family building types



**(12) Stoop.** Stoops are exterior stairs with landings that provide access to buildings placed close to the property line. Building facades are set back just enough to provide space for the Stoop. The exterior stair of a Stoop may be perpendicular or parallel to the sidewalk. A Stoop's landing may be covered or uncovered. Stoops shall be raised above grade a minimum of 18 inches and a maximum of 36 inches to provide privacy for ground floor residences. A Stoop's landing shall be at minimum four feet in width and depth. Landscaping on either side of the Stoop may be at grade or elevated, and may be demarcated by a garden wall that shall not exceed 24 inches in height. Stoops are allowed on Apartment, Courtyard, Villa, Duplex/Triplex/Quadplex, Townhouse, and Single Family building types



**(13) Forecourt.** The Forecourt is an urban frontage type appropriate for residential and non-residential uses and may be combined with other types, such as Shopfront and Awning or Stoop. Forecourts are created by setting back a portion of a buildings facade, typically the middle, to create a small entry square. Forecourts often provide access to a central lobby of a larger building, but may also be combined with other frontage types that provide direct access to the portions of the facade that are close to the sidewalk. Forecourts may be landscaped or paved, depending on the ground floor uses of the building. Forecourts may be at grade or elevated above the sidewalk a maximum of 24 inches. Forecourts shall be at minimum ten feet in width and depth, however, the width of a Forecourt shall not exceed one third of the overall facade width, and the depth shall be equal to or less than the width. Forecourts are allowed on Mixed-Use, Apartment, Courtyard, Villa, and Single Family building types.



**(14) Shopfront.** The Shopfront is the primary frontage type for buildings with ground floor commercial uses. Shopfront frontages are created by inserting storefronts with large transparent windows into the ground floor facade of a building. The facade is aligned with the property line, although partially recessed storefronts, such as recessed entrances, are also common. The building entrance is at sidewalk grade and provides direct access to a non-residential ground floor use.



Shopfronts are composed of storefronts, entrances, awnings or sheds, signage, lighting, cornices, and other architectural elements. Awnings or sheds may encroach into the public right-of-way and cover the sidewalk to within two feet of the curb. Awnings, sheds, signage or other sidewalk encroachments shall be at minimum eight feet above sidewalk grade. Shopfronts are allowed on Mixed-Use, building types.

**(15) Arcade.** Arcades are created by projecting a building's upper floors above the sidewalk while aligning the ground floor facade with the property line. Arcades typically contain ground-floor storefronts, making this frontage type is ideal for retail use. A colonnade structurally and visually supports the building mass that encroaches into the public right-of-way. Arcades are most effective if they are used on both sides of the street and for the entire length of the block, except where used to emphasize civic buildings. Arcades and Galleries may be combined to achieve this. Arcades shall provide at minimum eight feet clear between the ground-floor facade and the inside of the posts or columns, and a minimum clear height above the sidewalk of ten feet. The space between the face of the curb and the outside face of the posts or columns shall be between 24 and 30 inches to provide sufficient room for overhanging bumpers but to discourage walking along the outside of the Arcade. Arcades are allowed on Mixed-Use, building types



The following table summarizes the allowable frontage types per each type of building.

<b>Building Type</b>	<b>Porch</b>	<b>Dooryard</b>	<b>Stoop</b>	<b>Forecourt</b>	<b>Shopfront</b>	<b>Arcade</b>
Mixed-Use Building	N	N	N	Y	Y	Y
Apartment Building	N	Y	Y	Y	N	N
Courtyard Housing	N	Y	Y	Y	N	N
Villa	N	Y	Y	Y	N	N
Duplex/Triplex/Quadplex	Y	Y	Y	N	N	N
Townhouse	Y	Y	Y	N	N	N
Single family	Y	Y	Y	Y	N	N

- e. **Conceptual Multifamily Development Scenarios.** The concepts in this section exhibit low and high density scenarios for each Planning Area in the Colonies at San Antonio Planning Areas 4, 15, and 22.

(1) **Density Scenarios.** The following table indicates potential density scenarios for each Planning Area.

Planning Area	Plan Option	Commercial [sq. ft.]	Residential [DU]	Parking				Density [DU/ac]
				Lot	Podium	Garage	Street	
PA-4	A	20,000	85	237	-	-	25	14.4
	B	20,000	140	80	254	-	80	24.8
	C	25,000	350	-	827	-	80	62.1
	D	10,000	100	229	-	-	75	17.7
	E	10,000	180	180	209	-	75	31.9
PA-15	A	-	82	-	-	164	90	8.0
	B	-	160	366	-	-	65	15.7
	C	-	130	249	-	38	75	12.7
PA-22	A	-	70	134	-	-	-	22.4
	B	-	90	-	186	-	10	28.8
	C	-	53	109			-	17.0

The Planning Area 4 Conceptual Scenarios show both Mixed-Use and Courtyard Housing. However, any combination of Mixed-Use, Courtyard Housing, and Apartment building types could be used in the Planning Area. All the scenarios show a driveway from Planning Area 4 into the existing Planning Area 17 driveway. A reciprocal easement agreement will be required for construction of this driveway.

*Planning Area 4 –Scenario A*



Planning Area 4 – Scenario B



Planning Area 4 – Scenario C



Planning Area 4 – Scenario D

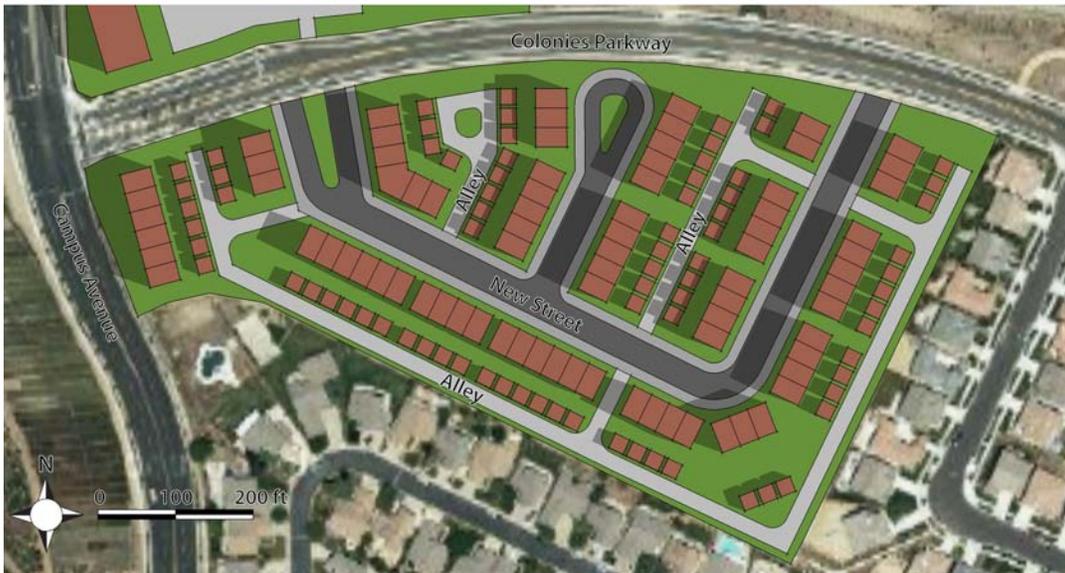


Planning Area 4 – Scenario E



The Planning Area 15 Conceptual Scenario A shows a Townhouse building type. However, any combination of Villa, Duplex/Triplex/Quadplex, Townhouse, or Single Family building types could be used in the Planning Area.

*Planning Area 15 –Scenario A*



The Planning Area 15 Scenario B shows an Apartment building type. However, any combination of Villa, Duplex/Triplex/Quadplex, Townhouse, or Single Family building types could be used in the Planning Area.

*Planning Area 15 –Scenario B*



Planning Area 15 –Scenario C



The Planning Area 22 Conceptual Scenario A shows an Apartment Building type. However, any combination of Apartment Building or Courtyard Housing building types could be used in the Planning Area.

*Planning Area 22 –Scenario A*



The Planning Area 22 Conceptual Scenario B shows an Apartment building type. However, any combination of Apartment Building or Courtyard Housing building types could be used in the Planning Area.

*Planning Area 22 –Scenario B*



Planning Area 22 – Scenario C



## **8.4 PARKING STRUCTURE DESIGN GUIDELINES**

Where used, parking structures within Colonies at San Antonio shall be designed and constructed to the same standard of quality as the uses they serve and shall be integrated into the overall development, making them more convenient, accessible, and safe. The following guidelines shall apply to the development of parking structures within Colonies at San Antonio.

### **8.4.1 Siting**

- Pedestrian connections between parking structures and the uses they serve shall be convenient and direct. Stairs and elevators shall be tied directly to walks leading to plazas, courts, or building entries.
- Vehicular access to parking structures shall be clearly identified and easily accessible, minimizing pedestrian and vehicular conflicts.

### **8.4.2 Architecture**

Parking structures within Colonies at San Antonio shall convey a simple image of order and quality.

- Architectural design, horizontal and vertical articulation, and detail shall be used to reduce the perception of massive scale.
- The parking structure's exterior appearance from adjacent buildings and public rights-of-way shall be that of flat, horizontal parking levels. Spandrel panels shall be level and uniform. Painted pipe rails are encouraged to reduce the mass of spandrel panels.
- Design, detail, building materials, and colors shall be compatible with adjacent buildings.
- Materials shall include only pre-cast or sand blasted, poured-in-place concrete of light color. Brick may also be used as an "infill" material, if it is used on adjacent buildings.
- Planted atriums may be provided to create an orientation point within large parking structures.
- Where employed within commercial Planning Areas, service and retail uses along the ground level of the parking structure are encouraged to break up the visual mass of the structure and enrich the pedestrian experience.
- If they do not feature service or retail uses along the ground level, parking structures shall be screened by berming and/or landscaping for visual relief.

### **8.4.3 Pedestrian Access**

- Designs shall include multiple points of pedestrian access into parking structures, such as elevated walkways, elevators, and stairways.
- Elevated walkways, elevators, and stairways shall be designed as an integrated part of the parking structure.
- These elements shall be placed on the outside of the structure to enhance personal safety through visibility.
- Glass elevators and open stairwells are encouraged to provide security.
- Open pipe railings along stairways are encouraged, painted using the accent colors of adjacent buildings.

#### **8.4.4 Rooftops**

- Landscaping of rooftop perimeters shall be required, and may be accomplished by use of column-mounted vine planters with attached structures or by raised planters.

## 9.0 LANDSCAPING

### 9.1 OVERALL LANDSCAPE CONCEPT

Landscaping within the Colonies at San Antonio Specific Plan will play an important role in establishing and reinforcing the desirability of the Colonies at San Antonio community. The carefully chosen landscape concept selected for Colonies at San Antonio will provide the project with a distinct community identity that also will allow it to respond to surrounding existing and proposed developments (Figure 9.1).

Landscape development within the Specific Plan area will be guided by the design standards and guidelines set forth in this Chapter to enhance the quality of the environment. The Colonies at San Antonio landscape design concept responds to those elements and features which are reminiscent of the historical significance and aesthetics of Upland's heritage. As designed, the Colonies at San Antonio landscape concept will provide a smooth transition from existing, adjacent uses, while distinguishing the project site as a distinct community within the City of Upland. The Specific Plan's landscape concept also allows for variation within individual Planning Areas, while maintaining the Colonies' overall landscape design integrity.

The objectives of the Colonies at San Antonio landscape concept include the following:

- In the residential areas, create an "early California" landscape that utilizes both native and non-native species, and in the commercial and mixed use areas, create a landscape that addresses specific retail needs while providing a pleasant shopping experience. Water efficient landscape will address long term maintenance concerns.
- Establish a community identity and character with a complementary streetscape, including a hierarchy of intersection monumentation, and employing materials that are consistent with the character of the area.
- Provide passive recreational facilities for picnicking, jogging, strolling, and viewing wildlife habitat, including educational opportunities related to the site's habitat conservation areas.
- Provide a system of community trails for convenient access between neighborhoods, commercial centers and open space, with connections to the regional trail system along the Cucamonga Creek Channel.
- Create natural wetland and riparian habitats in portions of Planning Area 16 for passive recreational usage.

The residential, open space, mixed use, and commercial landscape areas within the Colonies at San Antonio will have their own identity and character. Each landscape area within the Colonies at San Antonio will have its own identity and character, based on the specific land use (commercial and mixed use, residential, or open space). This character will be created through consistent use of specific building materials in each of the land use areas, as well as distinctive plant palettes. Continuity in form, color, and texture of site elements within each planning area will enhance the community image while blending in with the local foothill character and backdrop of the local mountains. Granite river cobble, which is prevalent in the area and surrounding foothill communities, will be used as an accent material for the

project monumentation and walls adjacent to the residential areas, and provide a transitional element to the open space / trails system. The commercial and mixed use areas will use materials appropriate to the architectural style and urban/retail character, and compatible with the overall community theme.

Use of uniform planting palettes is intended to create a dominance of certain suggested plant species, thereby establishing a recognizable community identity. These areas will allow each residential neighborhood and commercial center to have its own individual character, while remaining part of the overall community of the Colonies at San Antonio. Specific plant types for each of the planning areas will provide continuity, identity, and organization for the streets, parking areas, monumentation, trails, and open space areas. Landscape design should be water efficient and adapted to the dry climate of Upland.

## **9.2 OPEN SPACE/TRAILS**

### **9.2.1 Open Space Concept**

The Colonies at San Antonio Specific Plan's central landscape feature will be approximately 59 acres of restored natural open space (see Figure 9.2). As the focal point of the project, the central open space basin and associated features will be designed and constructed to present a natural appearance (Figure 9.3). In addition to providing a major open space amenity and a visual focus for the Colonies at San Antonio community, the central open space area will allow for passive recreation; provide stormwater treatment, flood control and maintenance of historic groundwater recharge; and function as a wetland and riparian habitat area.

Hiking trails within the open space area will provide interpretative opportunities and access to the basin during non-storm periods (see Section 9.2.2 - Trails). Overlooks and rest areas will be located along the top of the basin at key vista points (Figures 9.4 and 9.5).

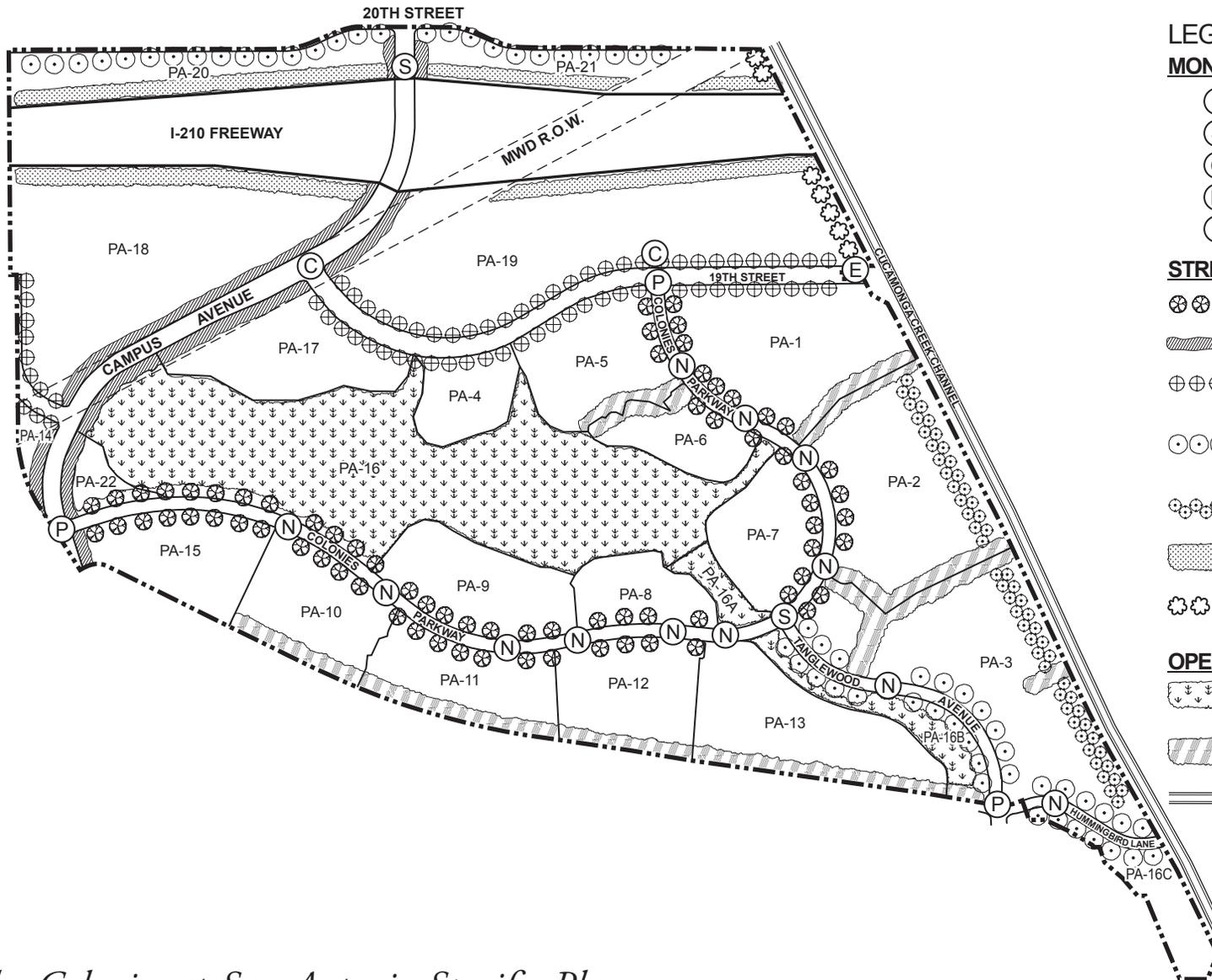
The ultimate open space area will also be designed to accommodate local and regional stormwater flows which currently run through the site to ensure that the capacity of the adjacent Cucamonga Creek Channel will not be exceeded. Substantial flood flows from off-site areas will be discharged into the central open space area from the 19<sup>th</sup> Street and 20<sup>th</sup> Street drainage structures (Figure 9.6), as well as from other sources adjacent developments.

The landscape restoration of the central open space area ("Basin A") will create a valuable environmental resource that will establish a positive link between the natural environment and the community. The restoration process will include re-vegetation, utilizing both native and non-native plant materials, installation of dramatic rock outcroppings that give the appearance of a natural landscape, and creation of wetland and riparian habitats that will be sustained by storm drain runoff within the basin.

The existing basin, which resulted from past sand and gravel extraction activities, will be graded and re-contoured to provide the necessary capacity for flood control purposes, maintenance of historic groundwater recharge, and establishment of suitable areas for the land uses that will surround the basin (Figures 9.7 and 9.8).

Drop structures made up of rock ledges to create waterfalls or "rapids" (Figure 9.9) will be established at four locations at generally equal intervals along the long (east-west) axis of the basin floor. The drop structures will be constructed of local rock, or of artificial rock, to blend with the setting while providing structural functions. The drop structures will reduce the velocity of flood flows entering the basin, and allow for low volume flows to spread laterally and support wetland and riparian vegetation. Below the waterfalls, a scouring basin will reduce the velocity of flood flows, and spread the water through the braided channels to increase the effectiveness of uptake and removal of water-borne contaminants from

# LANDSCAPE CONCEPT PLAN



## LEGEND

### MONUMENTATION

- (P) Primary intersection
- (S) Secondary intersection
- (C) Commercial intersection
- (N) Neighborhood intersection
- (E) Easterly project entry

### STREETS

- ⊗ ⊗ ⊗ ⊗ Colonias Parkway  
Formal row of deciduous canopy street trees
- ▨ Campus Avenue  
Theme hedge and bollards, turf/groundcover plantings
- ⊕ ⊕ ⊕ ⊕ 19th Street  
Date Palms in median, palms in parkway, flowering canopy behind sidewalk
- ○ ○ ○ Tanglewood Avenue, 20th Street, Hummingbird Lane  
Formal row of evergreen canopy street trees
- ⊗ ⊗ ⊗ ⊗ Enhanced neighborhood parkway  
Tall, narrow deciduous street trees
- ▨ Freeway edge  
Informal shrub/groundcover drifts
- ⊗ ⊗ ⊗ ⊗ Buffer at Cucamonga Creek Channel  
Evergreen tree/shrub screen

### OPEN SPACE

- ▨ Basin A  
Riparian/Upland/California native planting
- ▨ Community trail & greenbelt  
Drought tolerant indigenous plantings
- ▨ Cucamonga Creek Channel

*The Colonies at San Antonio Specific Plan*

↑  
N Not to Scale  
Source: RJM Design Group.

Figure 9.1

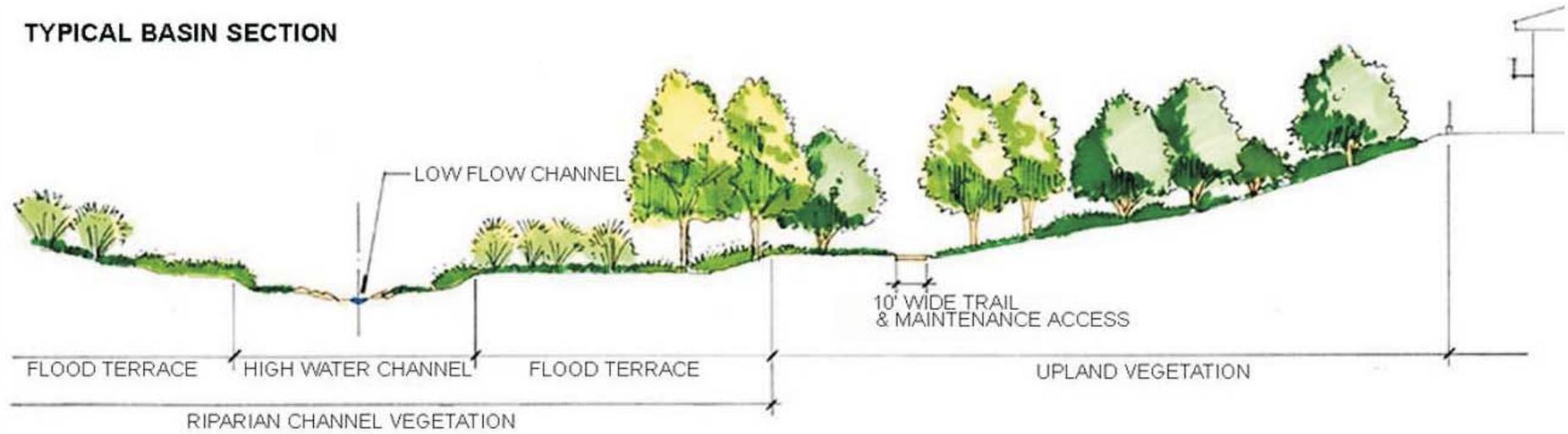
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**Figure 9.2 Open Space Concept**

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# TYPICAL BASIN SECTION

## TYPICAL BASIN SECTION



*The Colonies at San Antonio Specific Plan*

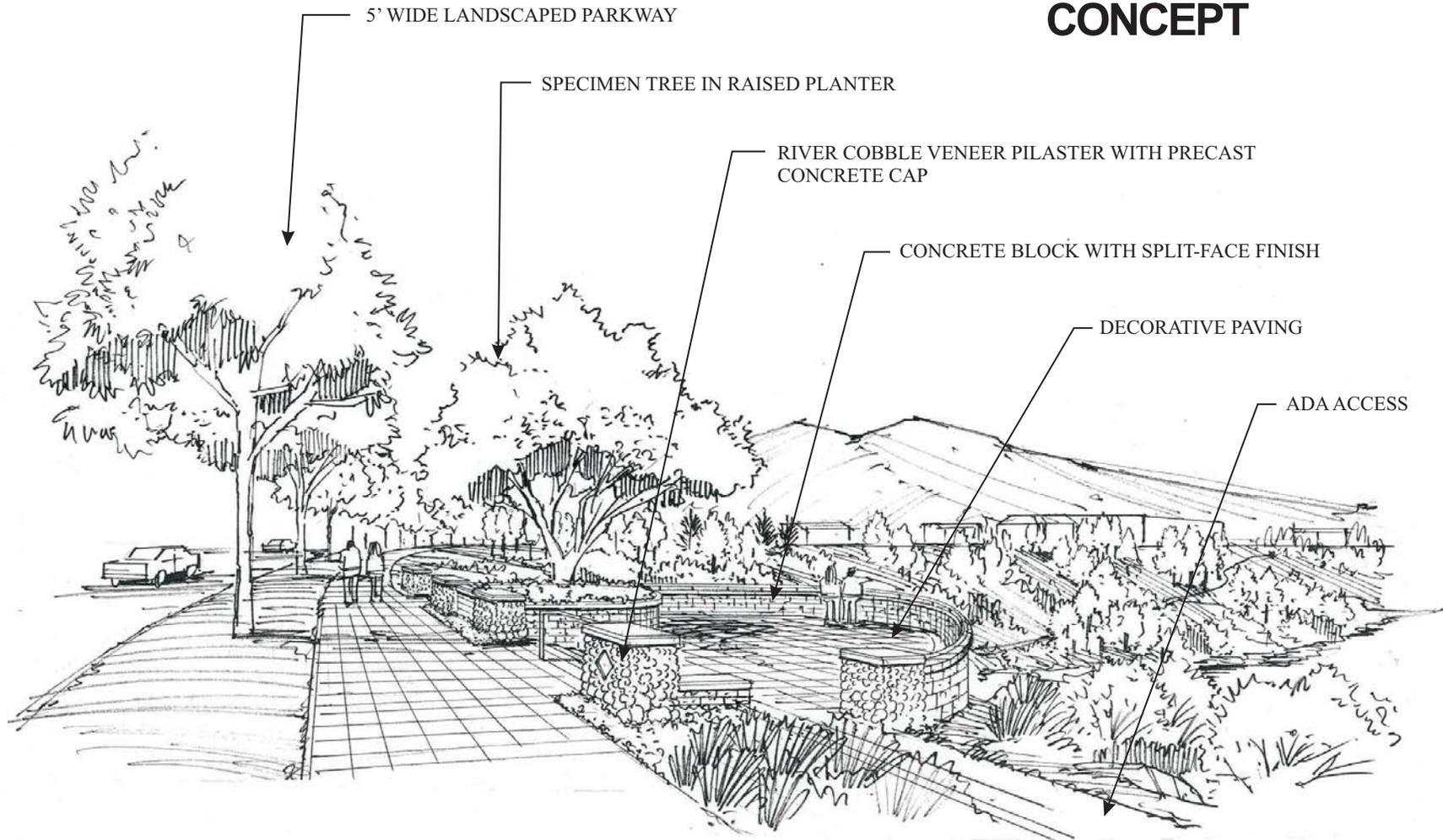
Figure 9.3

Not to Scale

Source: RJM Design Group..

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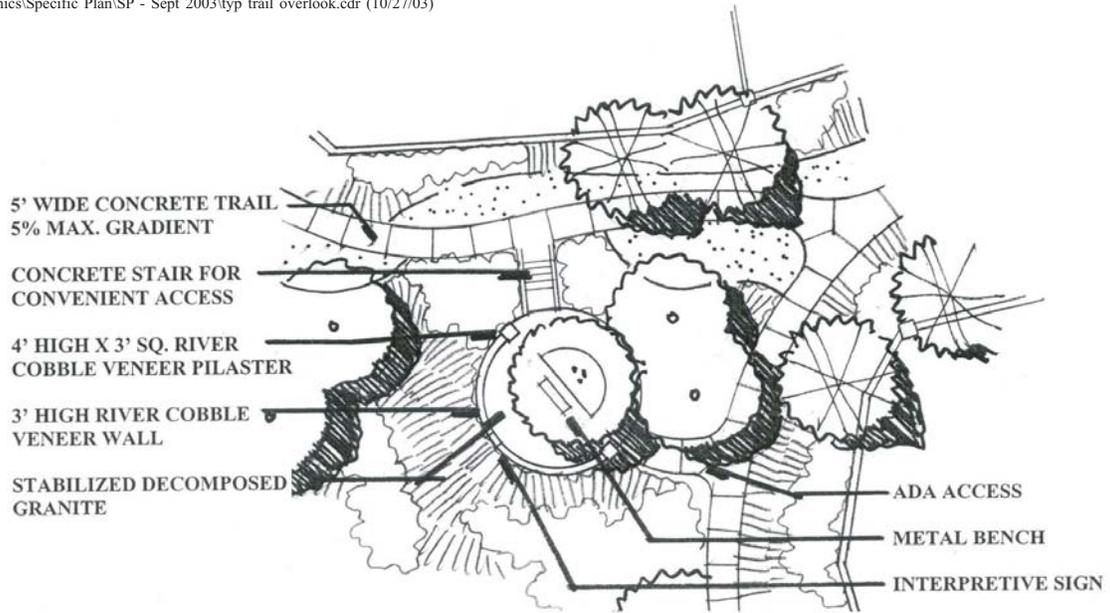
# TRAIL OVERLOOK CONCEPT



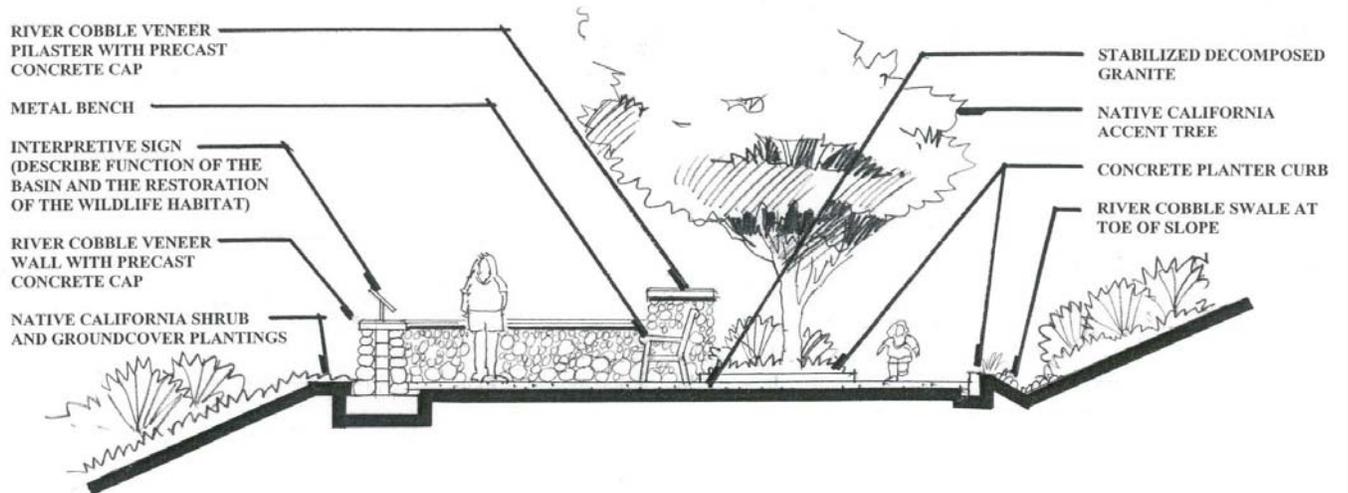
*The Colonies at San Antonio Specific Plan*

Figure 9.4

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Plan



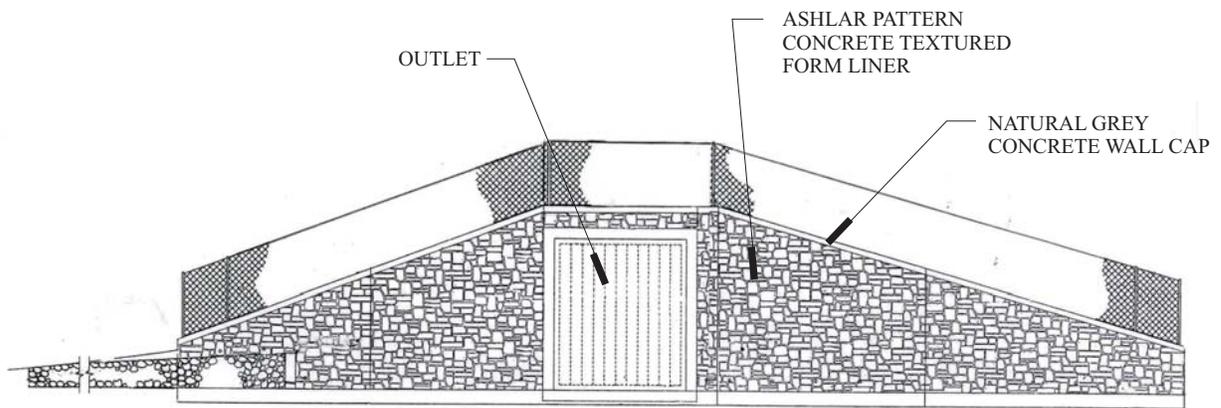
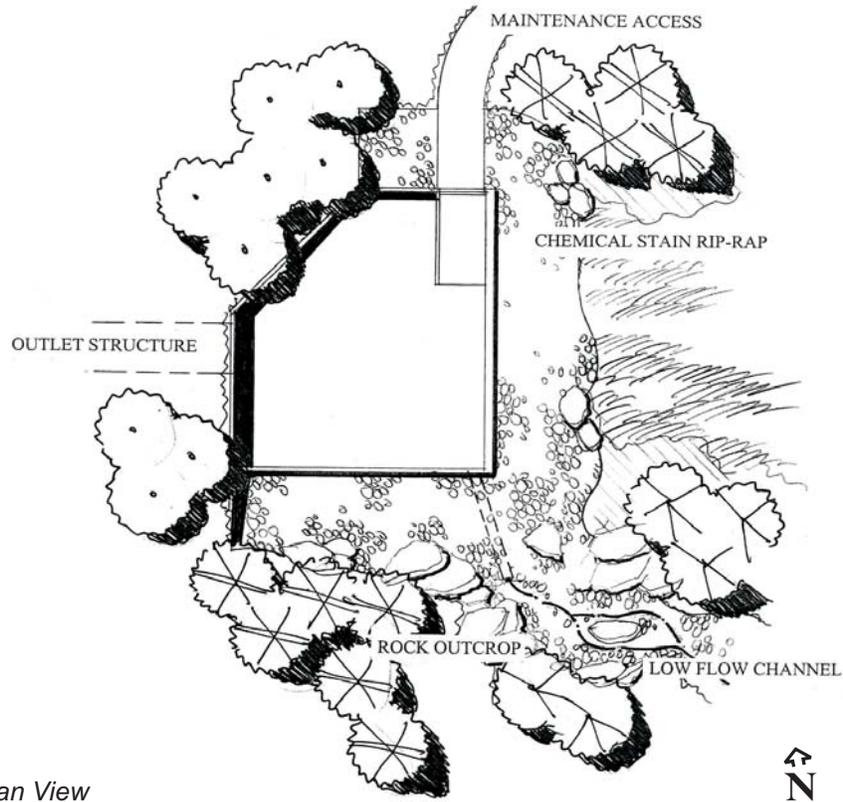
Section

## TYPICAL TRAIL OVERLOOK

*The Colonies at San Antonio Specific Plan*

Figure 9.5

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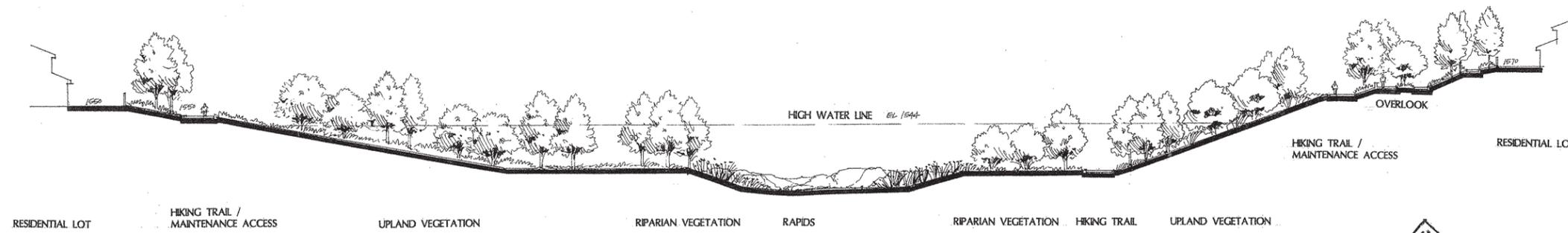
## 20TH STREET OUTLET STRUCTURE

*The Colonies at San Antonio Specific Plan*

Figure 9.6

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# OPEN SPACE BASIN COMPONENTS



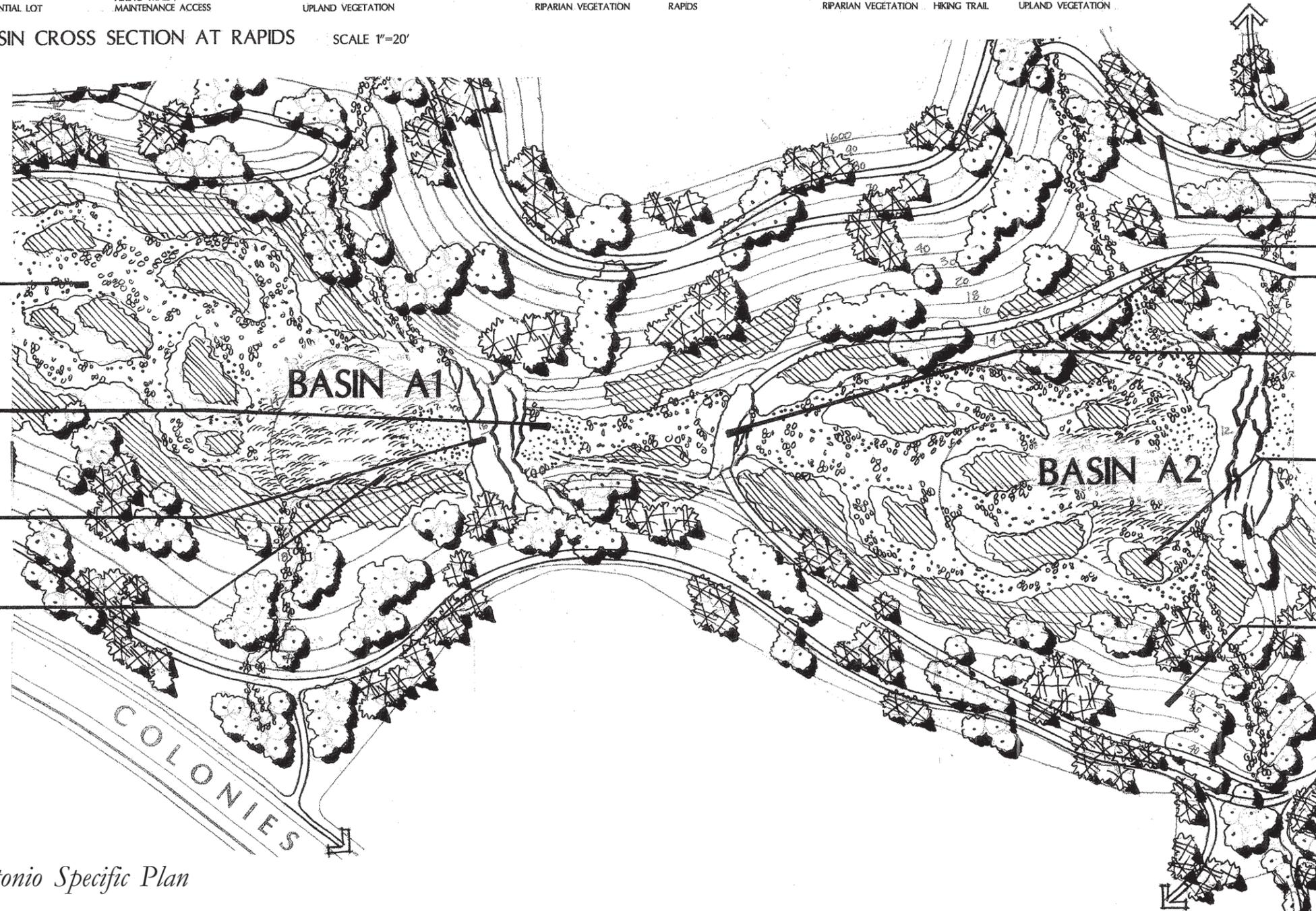
BASIN CROSS SECTION AT RAPIDS SCALE 1"=20'

- BRAIDED CHANNELS will convey low-volume storm flows and nuisance flows and provide a suitable substrate for percolation to improve groundwater re-charge.

- SCOURING BASINS will help to reduce storm water velocities and prevent erosion of the channel downstream and help to spread the water through the braided channels.

- RAPIDS are natural and/or artificial rock formations designed to reduce velocity of storm water flows and to retain flows within the sediment/percolation basins.

- SEDIMENT / PERCOLATION sub-basins will allow storm water infiltration and re-charge of the groundwater table, as well as support riparian vegetation.



HIKING TRAIL- TYPICAL

MAINTENANCE ACCESS- TYPICAL

BIO-SWALES will function to remove contaminants from first-flush storm flows and from nuisance flows.

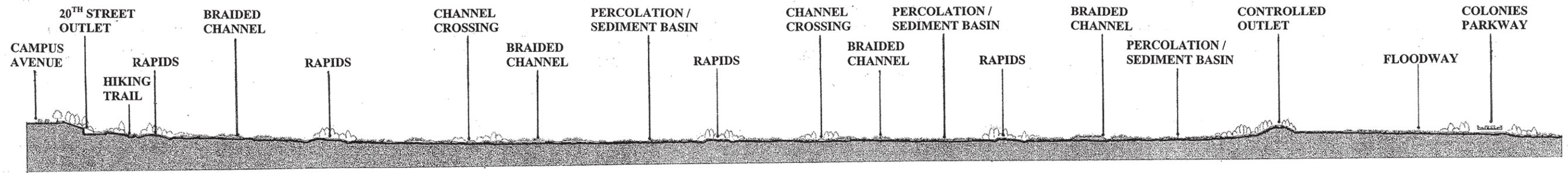
CHANNEL CROSSING will provide access to basin during non-storm events. The crossings will be of permanent construction to allow unobstructed flow of storm water, similar to an "Arizona" crossing.

- RIPIARIAN VEGETATION (partial plant list)
- Alkali sacaton
  - Bulrush
  - Cattails
  - Cottonwoods
  - Sycamores
  - Willows
  - Wheat grass
  - Deer grass

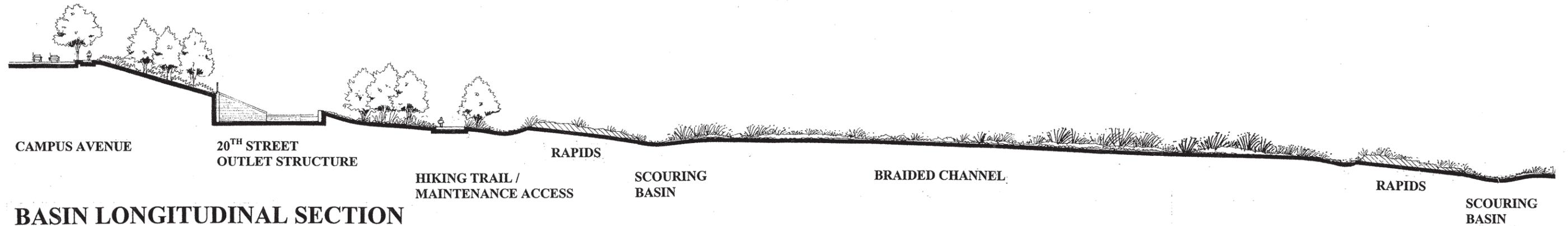
- UPLAND VEGETATION (partial plant list)
- Baccharis
  - Buckwheat
  - Ceanothus
  - Oak
  - Rhus
  - Ribes
  - Sagebrush
  - Sycamore
  - Toyon

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# BASIN SECTIONS



## BASIN A- EAST TO WEST CROSS SECTION



## BASIN LONGITUDINAL SECTION Campus Avenue-20<sup>th</sup> Street Outlet-Rapids-Braided Channel



PERCOLATION /  
SEDIMENT BASIN

RAPIDS

SCOURING  
BASIN

- RIPARIAN VEGETATION
- Sandbar Willow
  - Goodings Willow
  - Cattail
  - Bulrush
  - Common Reed

BRAIDED SAND CHANNEL  
0.5 % MIN. LOW FLOW

CHANNEL  
CROSSING

## BASIN LONGITUDINAL SECTION Percolation / Sediment Basin-Rapids-Braided Channel

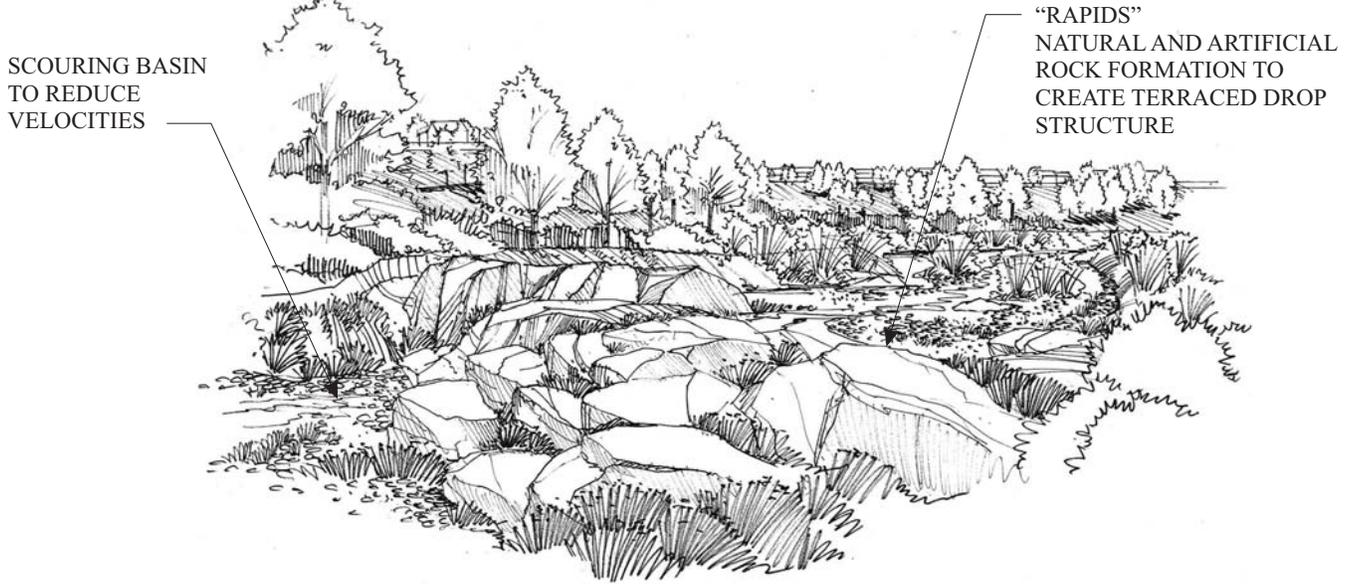
*The Colonies at San Antonio Specific Plan*

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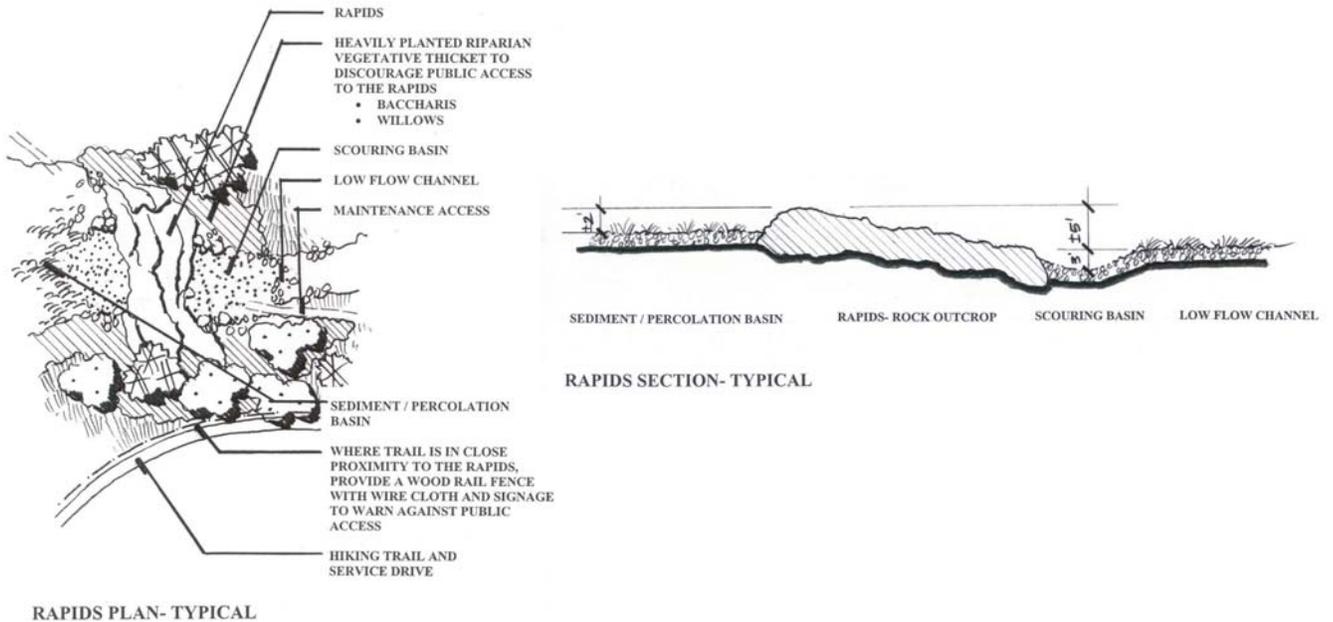
Source: RJM Design Group.

Figure 9.8

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"Rapids" (drop structures) and scouring basin downstream of braided channel. The rapids are a series of shallow sloping rock outcroppings designed to slow the velocity of the storm water flows and retain flows for groundwater re-charge. The scouring basin reduces downstream erosion of the channel and spreads the flows through the braided channel.



## RAPIDS AND SCOURING BASIN

*The Colonies at San Antonio Specific Plan*

Figure 9.9

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the upstream storm water runoff by wetland and riparian vegetation. The braided, sand-bottom channel will be constructed in the basin floor to convey low-volume storm flows and nuisance flows (Figure 9.10). The sand channel will be a suitable substrate for percolation. Additionally, the sand channel and adjacent vegetation will function as a “bio-filter,” removing contaminants from first-flush storm flows and from nuisance flows. Sediment and percolation basins will be established below the braided channels where reduced flow velocities will allow sediments to drop out of suspension and groundwater recharge to take place.

Tributary “bio-swales” will be graded to drain into the basin from the surrounding planning areas (Figure 9.11). The swales will convey runoff from commercial and residential areas and roadways. The swales will be designed and constructed to remove certain contaminants by water filtration and bio-accumulation (uptake of water through native vegetation). The swales will be constructed of either rip-rap or interlocking erosion control pavers, which allow the infiltration of water into the soil. Native vegetation will be planted between the rip-rap or interlocking erosion control pavers, creating water uptake through the plants.

With ultimate improvements, a valved outlet will be constructed at the downstream end of the basin to detain low-volume flows thereby increasing the “treatment time” for wetland vegetation to remove urban contaminants in storm water runoff (Figure 9.12). The controlled outlet will be constructed of local rock, or of artificial rock, to blend with the setting while providing structural functions, and include a valved outlet that is proposed to be operated by the San Bernardino County Flood Control District. Retention of water within the basin will also maintain historic levels of groundwater recharge. Storm flows up to a 100-year event will be discharged through a valved outlet and pipeline outletting into the floodway south of Colonies Parkway. Storm flows in excess of a 100-year storm event will exit Basin A over the spillway or controlled outlet at elevation 1544, discharging into Planning Area 16A.

As a result of the modifications to the central basin area necessitated by the agreement with the San Antonio Water Company and the Cucamonga County Water District settling litigation brought by those agencies against the Colonies at San Antonio EIR, the interim condition described in Chapter 4 would result in a basin with a detention capacity of 582 acre-feet and the ultimate condition would result in a larger detention capacity of 799 acre-feet, both of which would maintain historic rates of groundwater recharge.

When 100-year storm flows overtop the basin outlet, they will be conveyed via an outlet channel through the floodway drainage path, to a pipe at the intersection of Tanglewood Avenue and Hummingbird Lane (Figures 9.13 and 9.14). An overlook at Tanglewood Avenue and Hummingbird Lane will provide the opportunity for a rest area with views upstream toward the “natural” channel (Figure 9.15). The channel will be planted with native wetland and riparian vegetation. The flood waters will be directed through the pipe beneath Tanglewood Avenue and Hummingbird Lane and channeled down toward detention Basin B (in Planning Area 16C) and will then be metered into Cucamonga Creek Channel.

The open space basin design, featuring three sub-basins, and the resulting flow regime will create zones with varying degrees of soil moisture, from the upper braided sand channel sections that will be saturated nearly year-round to sustain wetland and riparian vegetation, to upland areas that will remain dry except for a short duration during and immediately after large flood events, to open wash that will be kept free of vegetation to assist in maintaining historic rates of groundwater recharge. The various riparian/wetlands, grasslands transition, and upland zones will be planted with California native vegetation suited to the particular soil moisture, plant communities, and in accordance with other design elements. Native vegetation that will be established within the basin and adjoining bio-swales will provide wildlife habitat for birds, amphibians, and small mammals. The creation of habitat zones corresponding to the hydrologic regimes in the basin and swales will provide increased habitat diversity and value.

Three distinct plant communities will define the natural habitat of the central open space area: wetland, riparian, and upland Southern California natives. These are described below.

a. **Wetlands.** This plant community consists of a sandy, gravel channel with large boulders planted with native vegetation. Suggested plant materials include the following.

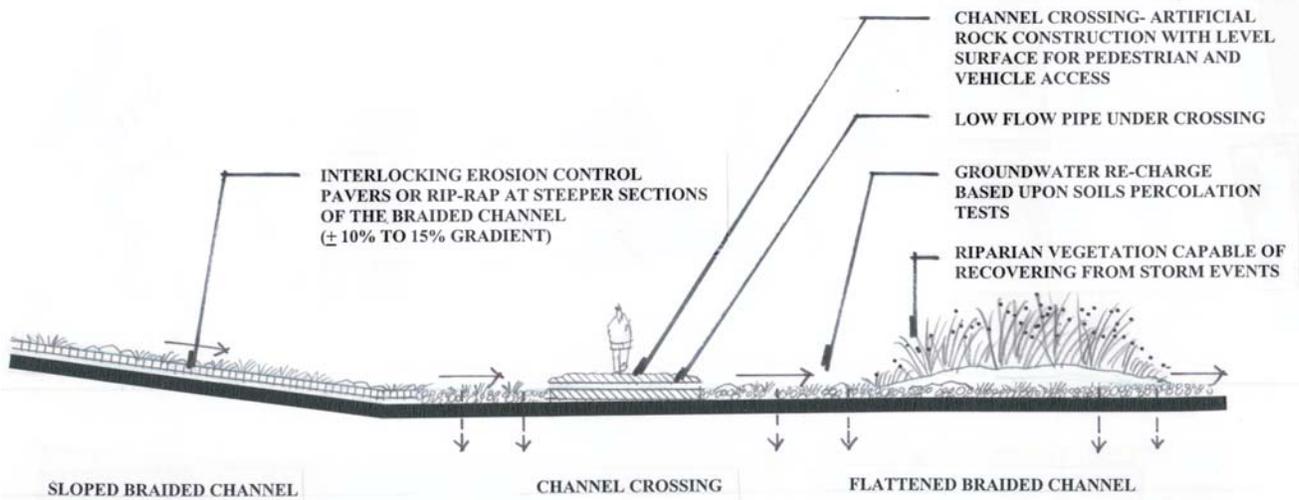
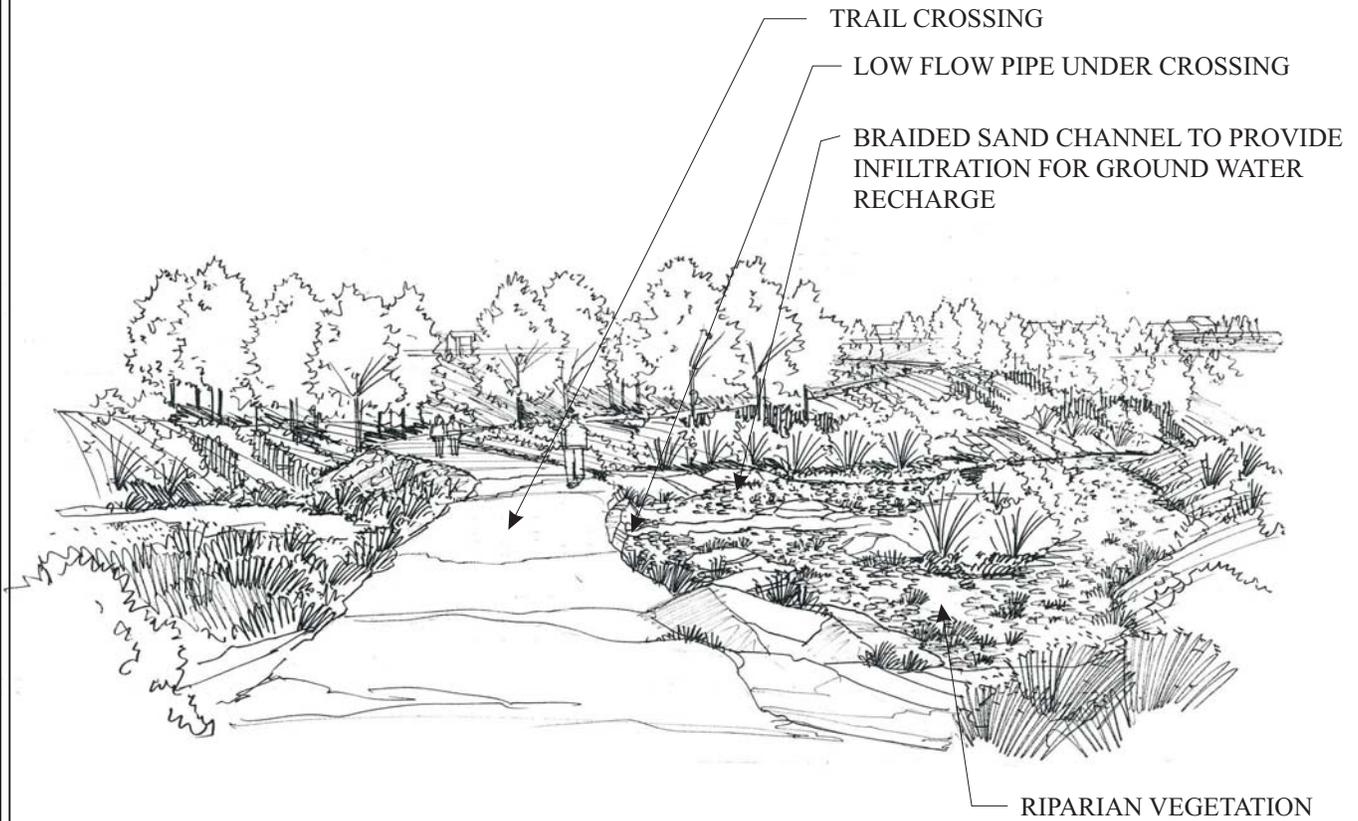
- *Cyperus eragrostis* Tall umbrella sedge
- *Eleocharis geniculata* Spikesedge
- *Juncus balticus* Wire rush
- *Populus fremontii* Cottonwood
- *Salix exigua* Sandbar willow
- *Salix gooddingii* Goodding's willow
- *Scirpus californicus* Bulrush
- *Typha domingensis* Giant cattail
- *Typha latifolia* Cattail

b. **Riparian Channel.** This plant community consists of a sandy, gravel channel with large boulders and rocks planted with native riparian vegetation. Suggested plant materials include the following:

- *Baccarus salifolia* Mule fat
- *Juncus bufonius* Toad rush
- *Juncus mexicanus* Mexican rush
- *Leymus condensatus* Giant wild rye
- *Leymus triticoides* Creeping wildrye
- *Muhlenburgia rigens* Deer grass
- *Platanus racemosa* California sycamore
- *Salix lasiolepis* Arroyo willow

c. **Southern California Natives.** This upland (non-riparian) plant community will display a diverse, native flora creating an efficient, water-conscious landscape of unique character. Suggested plant materials include the following:

- *Alnus rhombifolia* White alder
- *Artemisia californica* California sagebrush
- *Eriogonum fasciculatum* California buckwheat
- *Heteromeles arbutifolia* Toyon
- *Isomeris arborea* Bladderpod
- *Mimulus aurantiacus* Buff monkeyflower
- *Oenothera californica* California evening-primrose
- *Pinus attenuata* Knobcone pine
- *Pinus coulteri* Coulter pine
- *Pinus sabiniana* Foothill pine
- *Platanus racemosa* California sycamore
- *Quercus agrifolia* Coast live oak
- *Rhus integrifolia* Lemonade berry
- *Salvia mellifera* Black sage
- *Sambucus caerulea* Blue elderberry



*Braided Channel Section - Typical*

## CHANNEL CROSSING AND BRAIDED CHANNEL

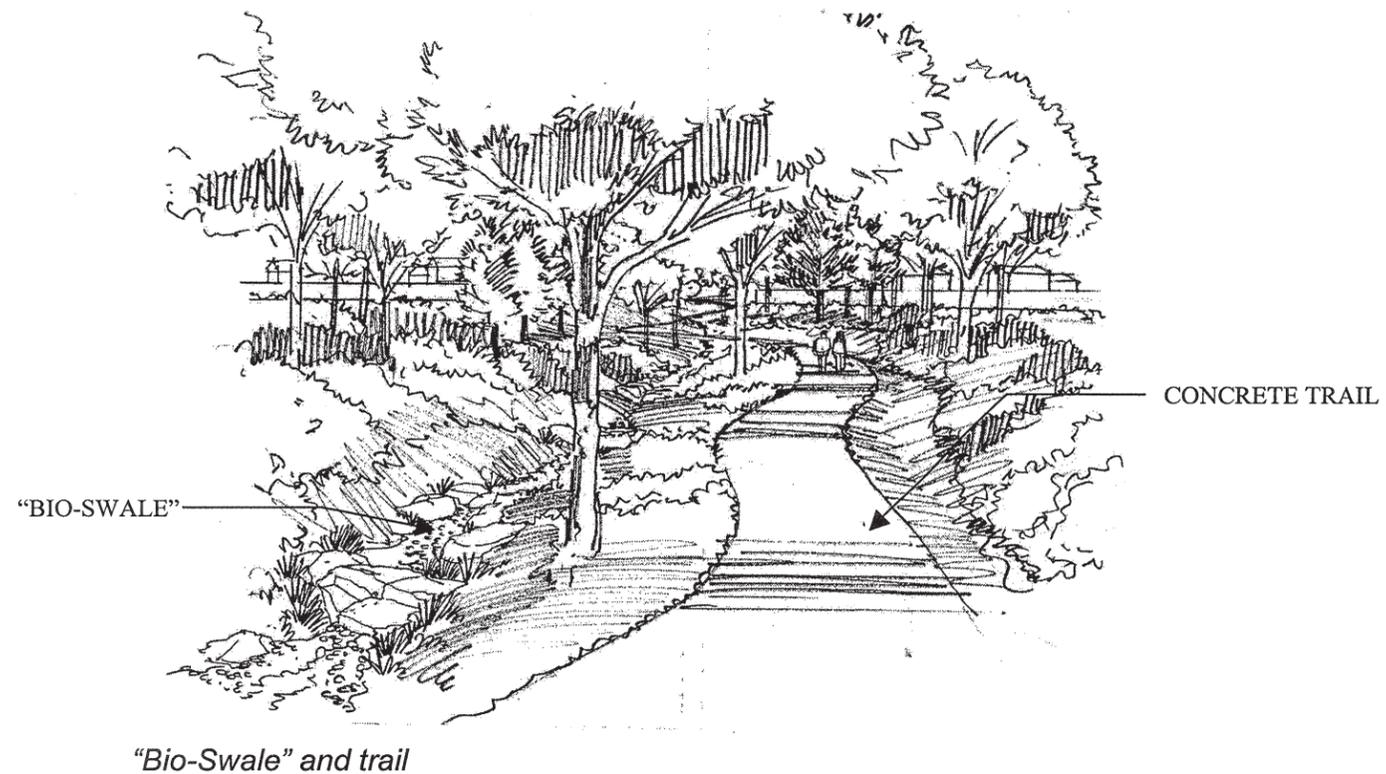
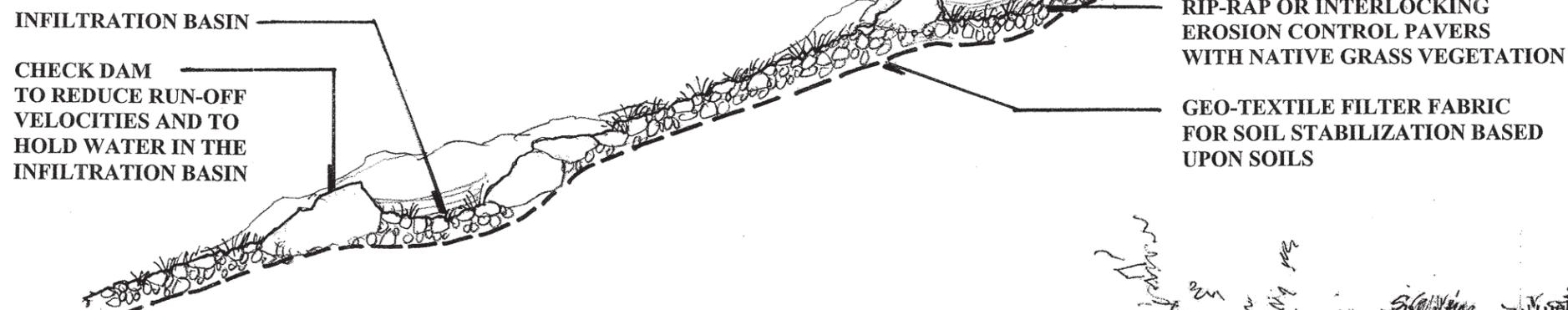
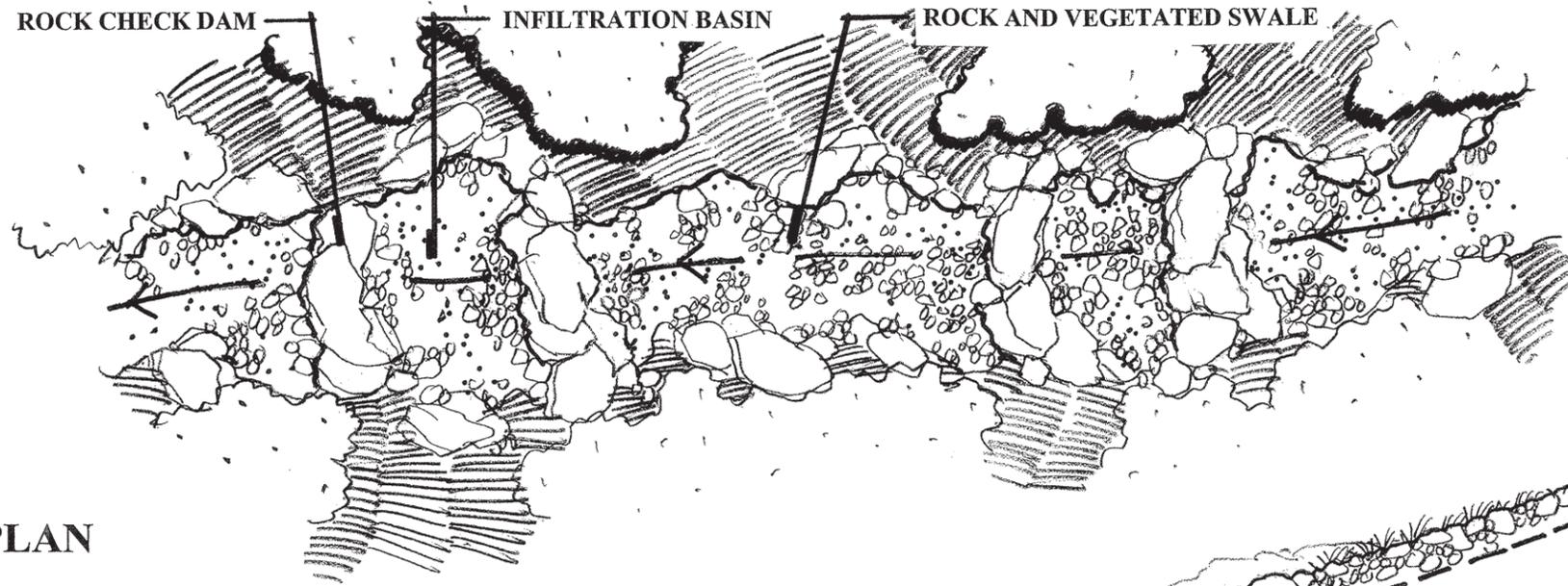
*The Colonies at San Antonio Specific Plan*

Figure 9.10

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# BIO-SWALES

The bio-swales are intended to collect storm water run-off from adjacent developments and remove particulate pollutants through the filtering action of the grasses and infiltration into the subsoil prior to entering the basin. The swales are constructed of either rip-rap or interlocking erosion control pavers, which allow the infiltration into the soil. A series of check dams at periodic intervals will slow the velocity of the run-off down the swale and will store the run-off in a basin behind the dam to increase the time for infiltration.



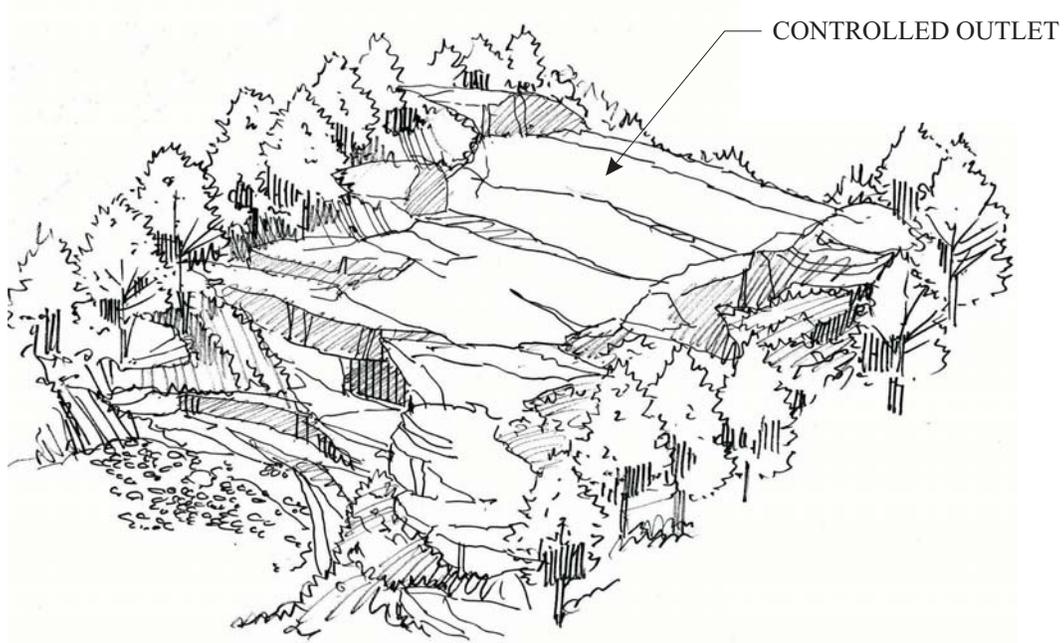
*The Colonies at San Antonio Specific Plan*

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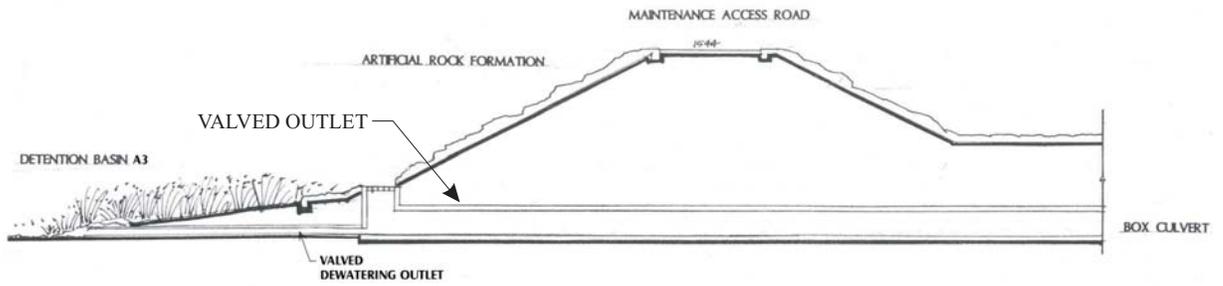
Source: RJM Design Group.

Figure 9.11

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*Controlled outlet/artificial rock outcrop.*



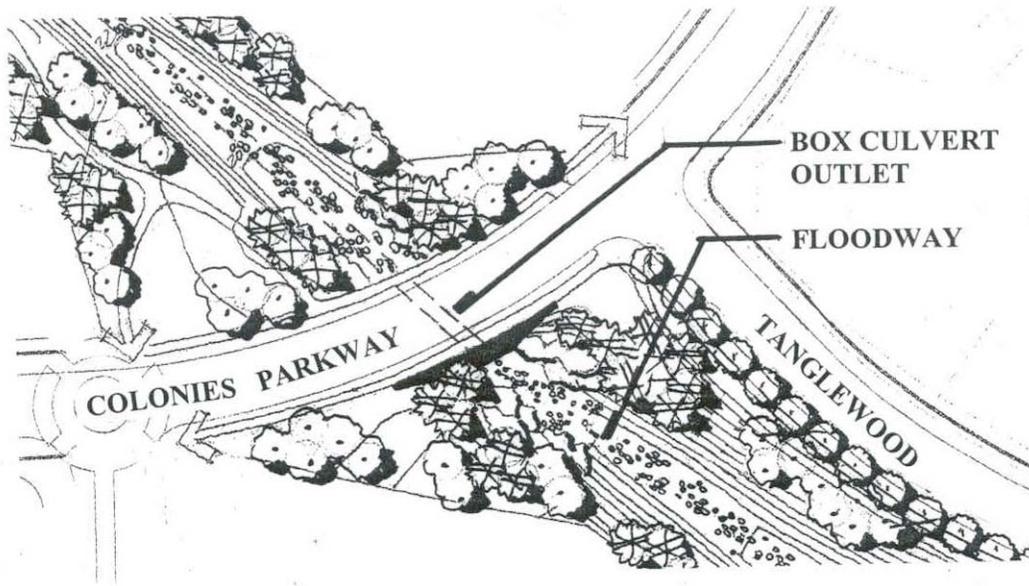
*Controlled outlet section*

## **CONTROLLED OUTLET**

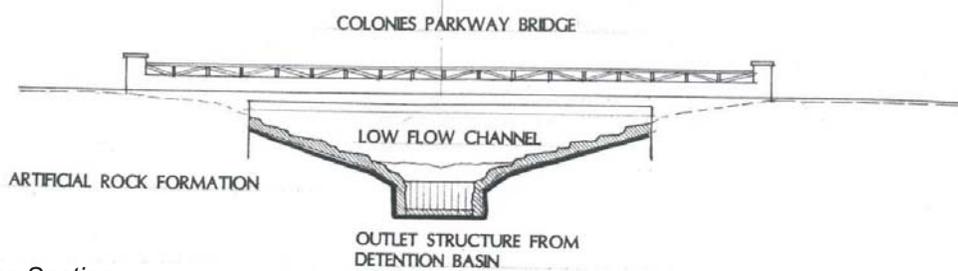
*The Colonies at San Antonio Specific Plan*

Figure 9.12

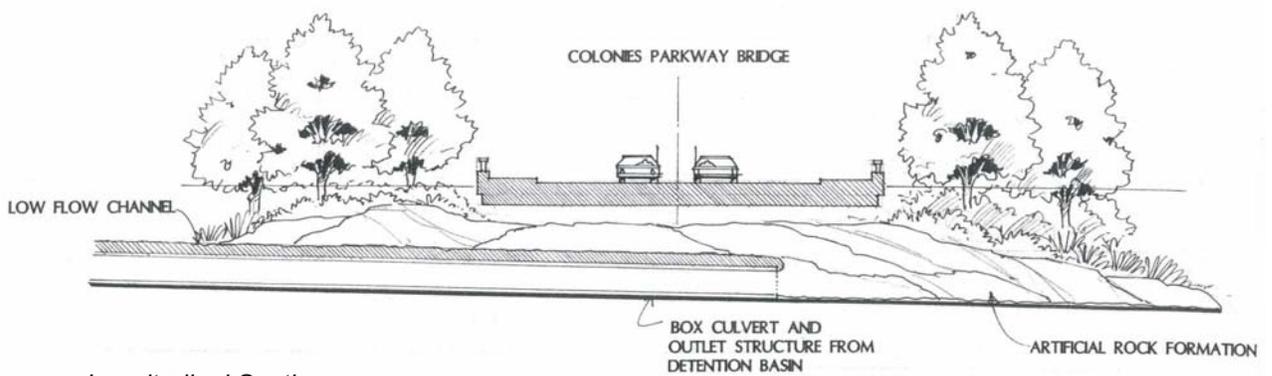
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Plan View



Cross Section



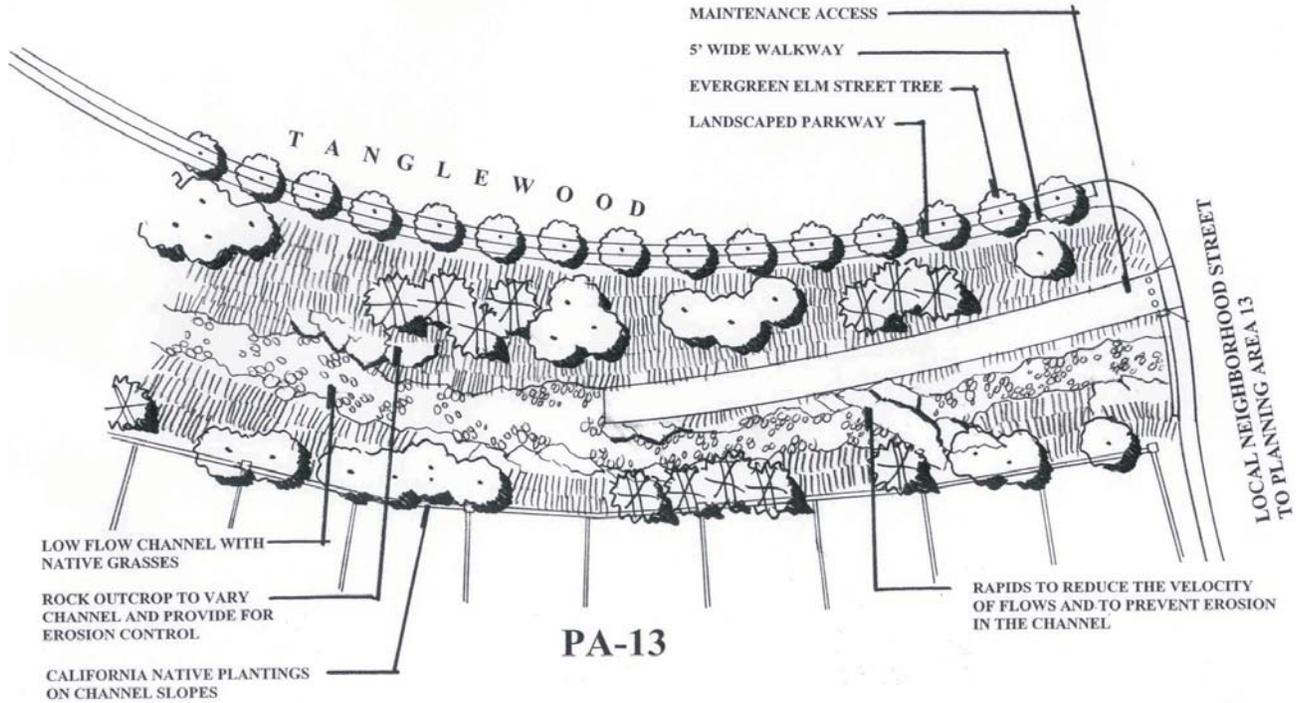
Longitudinal Section

## BOX CULVERT AT COLONIES PARKWAY

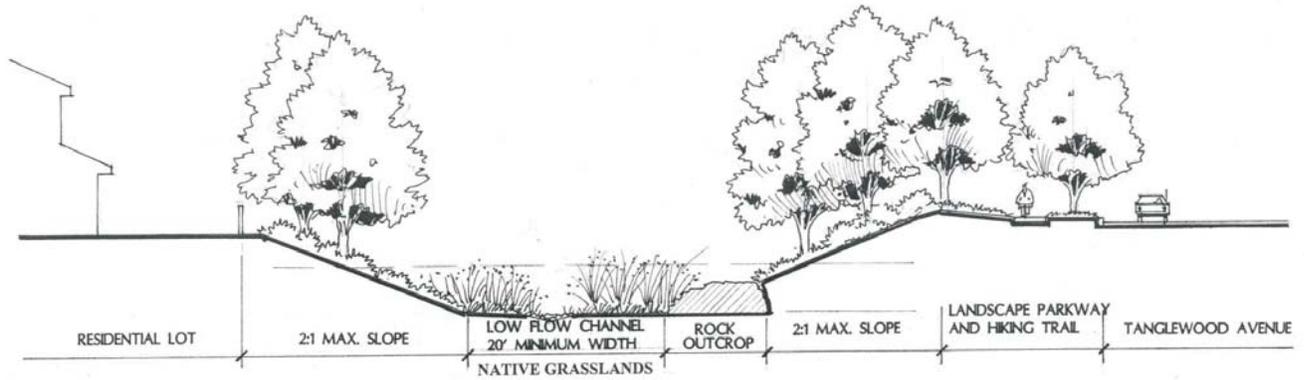
*The Colonies at San Antonio Specific Plan*

Figure 9.13

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*Floodway Plan - Typical*



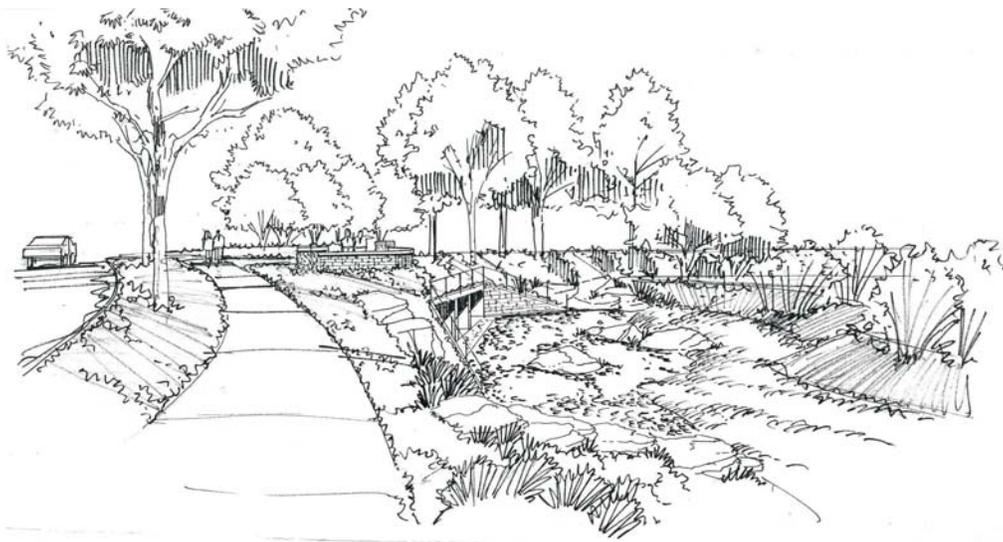
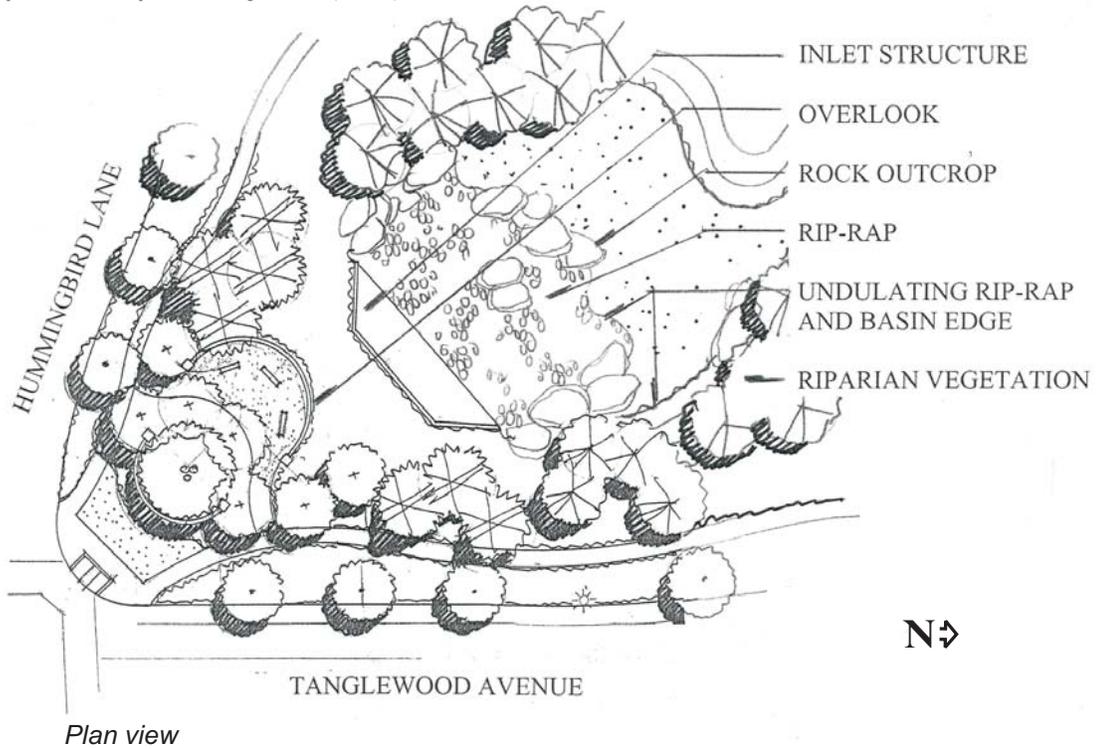
*Floodway Section - Typical*

## DRAINAGE PATH / FLOODWAY

*The Colonies at San Antonio Specific Plan*

Figure 9.14

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## INLET STRUCTURE AND OVERLOOK AT TANGLEWOOD/HUMMINGBIRD

*The Colonies at San Antonio Specific Plan*

Figure 9.15

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## 9.2.2 Trails

The central open space area will be accessed by an interconnected community-accessible trail system, linking the natural environment to the surrounding developed areas and the Cucamonga Creek Channel regional trail. This trail system will provide a passive recreational opportunity that reinforces the natural character and aesthetics of the open space.

Each neighborhood will have direct links to the community trail and greenbelt system (Figure 9.16). Entries to the trail and greenbelt corridors will occur at key locations, primarily at the ends of street cul-de-sacs. Each entry will be identified by a specimen tree to serve as a focal point. The community trail will meander through the greenbelt corridor, and provide links to the central open space area, the Cucamonga Creek Channel regional trail, and other neighborhoods (Figures 9.17 and 9.18). Opportunities will be provided for passive recreation to include seating, picnicking and informal family play. The landscape will provide a buffer between the trail and the adjacent residential lots. Plant material shall be drought tolerant, either native to southern California or adapted to southern California's natural dry climate along the foothills. A list of recommended trees for the community trail and greenbelt areas are provided under Section 9.5.1. The suggested trees will provide continuity and identity between the neighborhoods. A list of shrub materials adapted to the City of Upland are provided in the City's Planning Ordinance No. 17.

Along the southern edge of the project, a 28-foot wide (minimum) utility easement will accommodate the storm drain and sewer utilities as well as serve as a trail connection between neighborhoods and the greenbelt system (Figure 9.19). Access to the trail will be provided at key locations at the end of the cul-de-sacs of local neighborhood streets. The southern property line will be defined by an open wrought iron fence to provide a sense of security by visually opening up views to the adjacent apartment development and Scenic View Street. Farther west along the southern property line, a solid masonry wall will define the edge between the easement and the existing single family residential uses. Contour grading will provide undulation and interest along the landscape edge. Accommodations will be made to allow access to the corridor for maintenance.

The community trails through the neighborhood will provide links to the community open space hiking trail located within the restored central open space/basin, providing access to the wetland/riparian and native upland habitat (Figures 9.20 and 9.21). Integration of the site's natural resources into the community will provide opportunities for hiking, jogging, wildlife viewing, as well as interpretive signs to describe the natural systems, native vegetation, and wildlife habitat. The trail design will:

- Maximize views of the wetland/riparian habitat, rock outcroppings, and natural vegetation; provide safe pedestrian travel by minimizing conflicts with vehicular traffic;
- Enable users of the trail system to experience a variety of landscapes and habitat;
- Be comprised of trail material consisting of concrete with a maximum width of 10 feet;
- Be designed to comply with ADA and Title 24 requirements and accommodate emergency and maintenance vehicles at convenient access points; and
- Incorporate distance markers and interpretive trail signage to encourage trail usage and interaction within the community.

The conceptual location of signage in the central open space area, including trail head entry signage, trail marker signage, and interpretive signage for describing the natural features of the central open space area is shown in Figure 9.22. Open space signage design concepts feature the project logo and text on a sign panel, mounted on an aluminum post (Figure 9.23). The trail head entry monuments include a stone base.

Site furnishings within the open space area and along trails provide seating at view areas, rest areas, picnic locations, and conveniences such as trash receptacles. Pedestrian walkway lighting will be provided at approximately 80 feet on center (Figure 9.24). Site furniture designs and conceptual layout are shown in Figures 9.25 and 9.26.

### 9.3 STREETS CAPES

Streetscape design will establish visual continuity throughout the Colonies at San Antonio, creating a distinctive community identity. Streetscape design standards will be reminiscent of the historic character and rural quality of Upland’s heritage. Citrus accent trees will be installed at the primary intersections/monument signs to reflect the past significance of the citrus industry in the foothill region. Street rights-of-way, pavement, parkway, and median conditions are design factors that will be integrated into the development of streetscape design guidelines. Special landscape design treatments will be implemented throughout the site, utilizing consistent tree species, accent planting, enriched hardscape materials, walls, and signage.

The hierarchy of street intersection design treatments is based on four major entry treatments: primary, secondary, commercial intersections, and neighborhood entries. The landscape design concept for each will express an overall unifying sense of community identity. Figure 9.1 presents the locations of these intersections and entries.

#### 9.3.1 Primary Intersections and Monumentation

The key entries to the Colonies at San Antonio, as well as key intersections within the Specific Plan area have been designated as “Primary Intersections.” These include the intersection of Campus Avenue and Colonies Parkway at the western edge of the project site, the south side of the intersection of 19<sup>th</sup> Street and Colonies Parkway (which also serves as a commercial intersection on the north side), and Tanglewood Avenue at the southern edge of the project site. The design for the intersections is illustrated in Figure 9.27 (see sign standards in Section 7.7 for design requirements adjacent to Planning Areas 17 through 21).

- Project Monument Sign and Logo: Random, split-face concrete block and river cobble veneered walls with concrete block cap. Precast concrete sign and logo panels with sand blasted and painted letters and graphic.
- Trees: Citrus (Valencia Orange), *Schinus molle*, *Pinus eldarica*

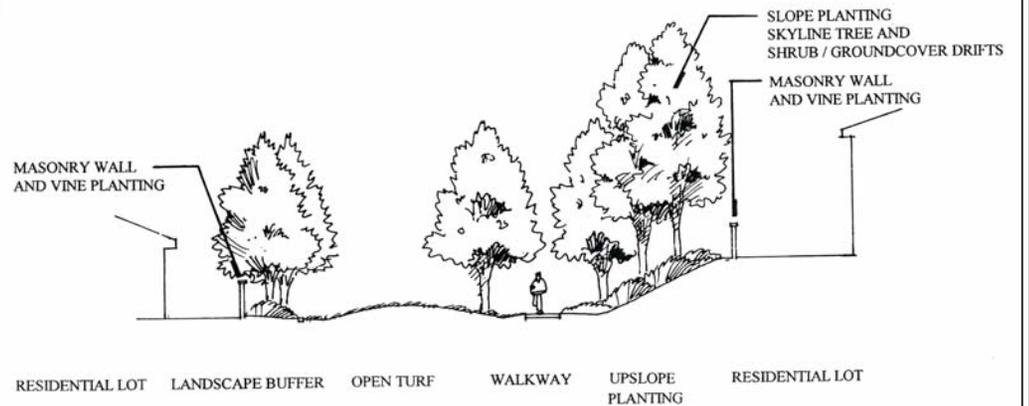
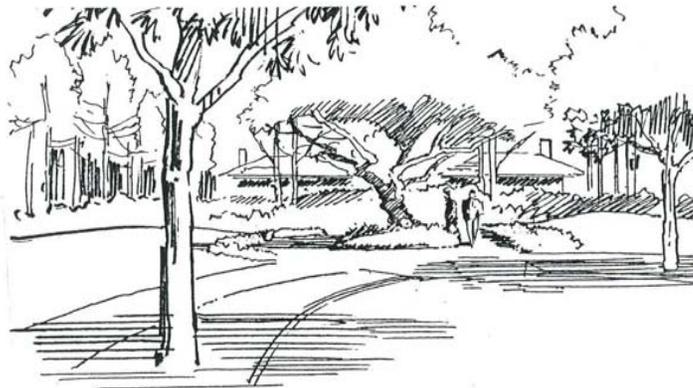
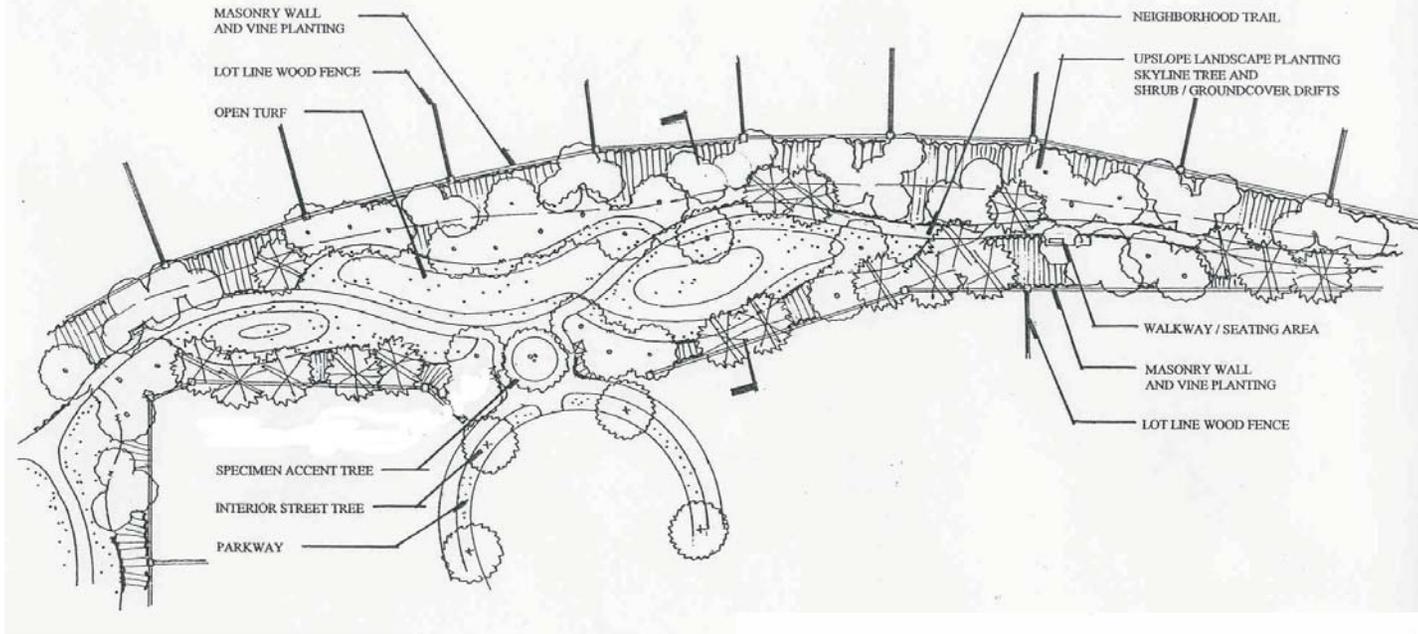
#### 9.3.2 Secondary Intersections and Monumentation

Secondary intersections within the Colonies at San Antonio Specific Plan include the intersection of Colonies Parkway and Tanglewood Avenue, and the westbound I-210/Campus Avenue on- and off-ramps. The design for these intersections is illustrated in Figure 9.28 (see sign standards in Section 7.7 for design requirements adjacent to Planning Areas 17 through 21).

Monumentation similar to the hierarchy and design of the secondary intersections is located at the easterly project entry on 19<sup>th</sup> Street at the eastern boundary of the project area (Figure 9.29).

- Secondary/Entry Monument: Random split-face concrete block with river cobble veneered pilaster with concrete block cap. Precast concrete sign and logo panels with sand blasted and painted letters and graphic.
- Trees: Citrus (Valencia Orange), *Schinus molle*, *Pinus eldarica*, *Washington sp.*, *Phoenix sp.*

# COMMUNITY TRAIL AND GREENBELT

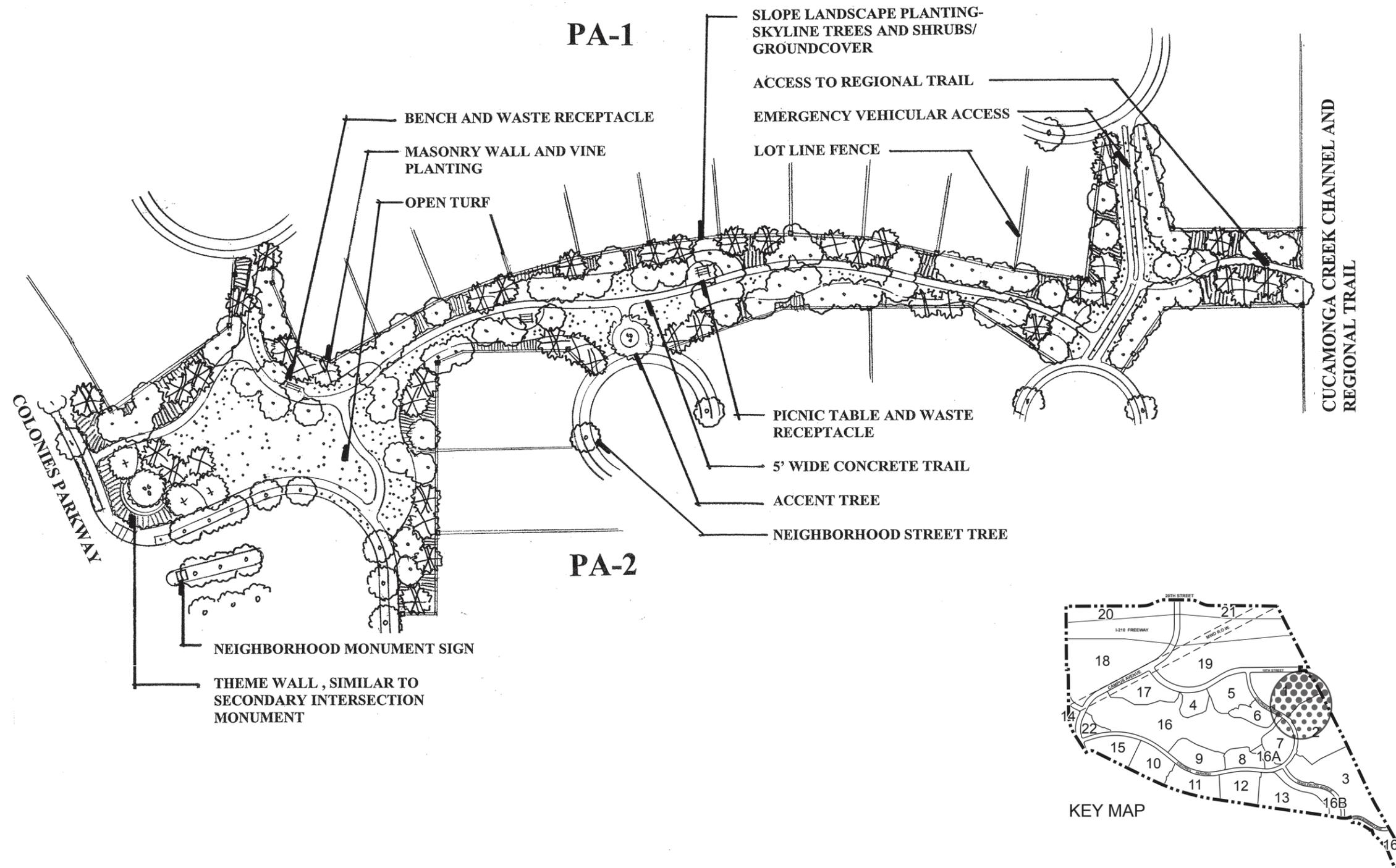


*The Colonies at San Antonio Specific Plan*

Figure 9.16

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# COMMUNITY TRAIL AND GREENBELT AT PLANNING AREAS 1 AND 2



*The Colonies at San Antonio Specific Plan*

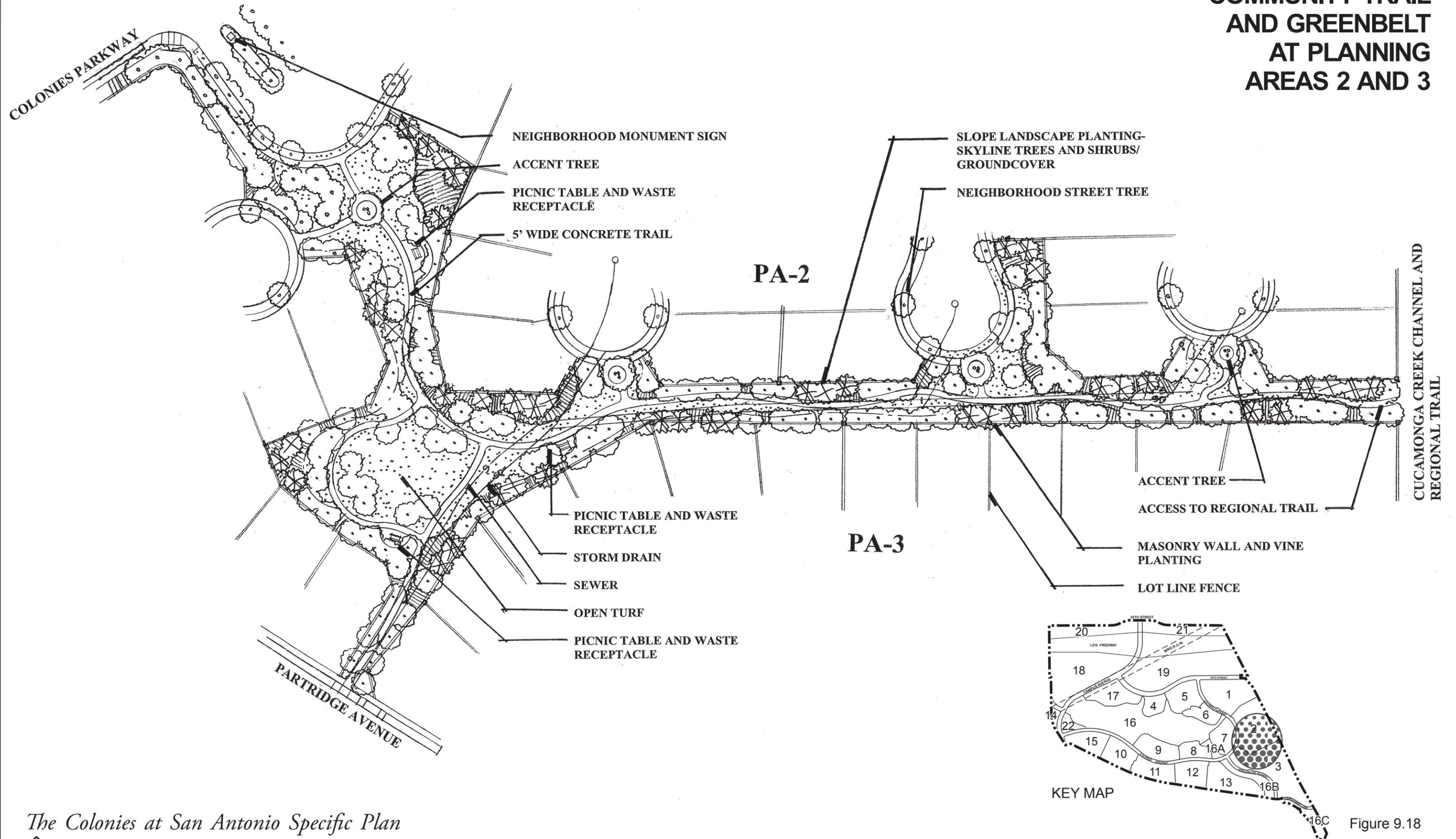
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N Not to Scale

Source: RJM Design Group.

Figure 9.17

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# COMMUNITY TRAIL AND GREENBELT AT PLANNING AREAS 2 AND 3



*The Colonies at San Antonio Specific Plan*

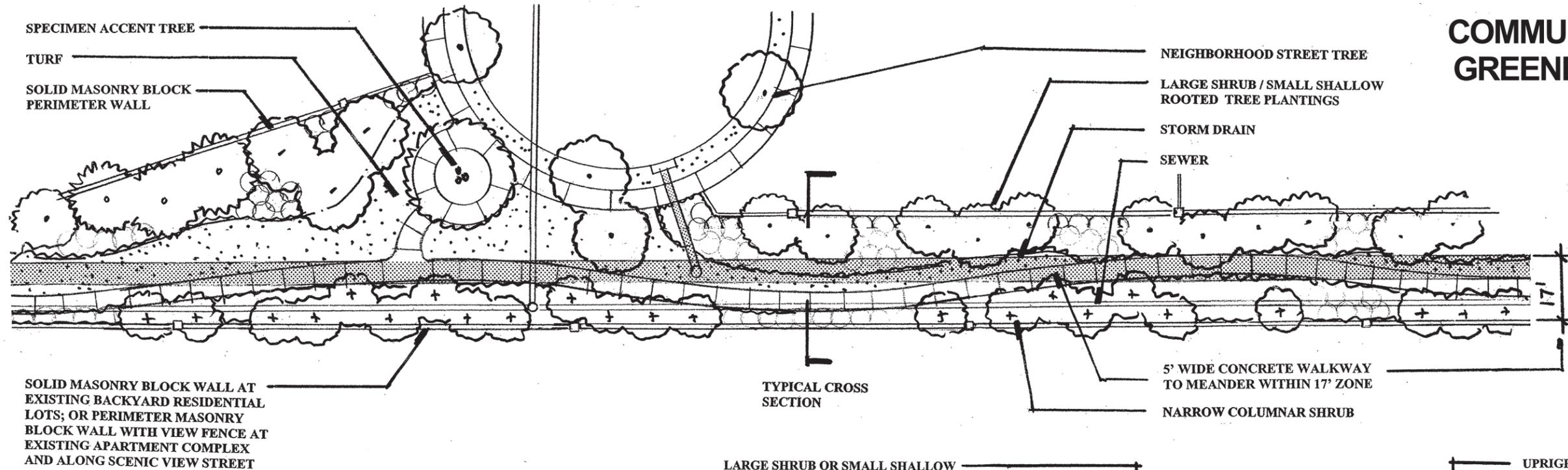
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Source: RJM Design Group.

Figure 9.18

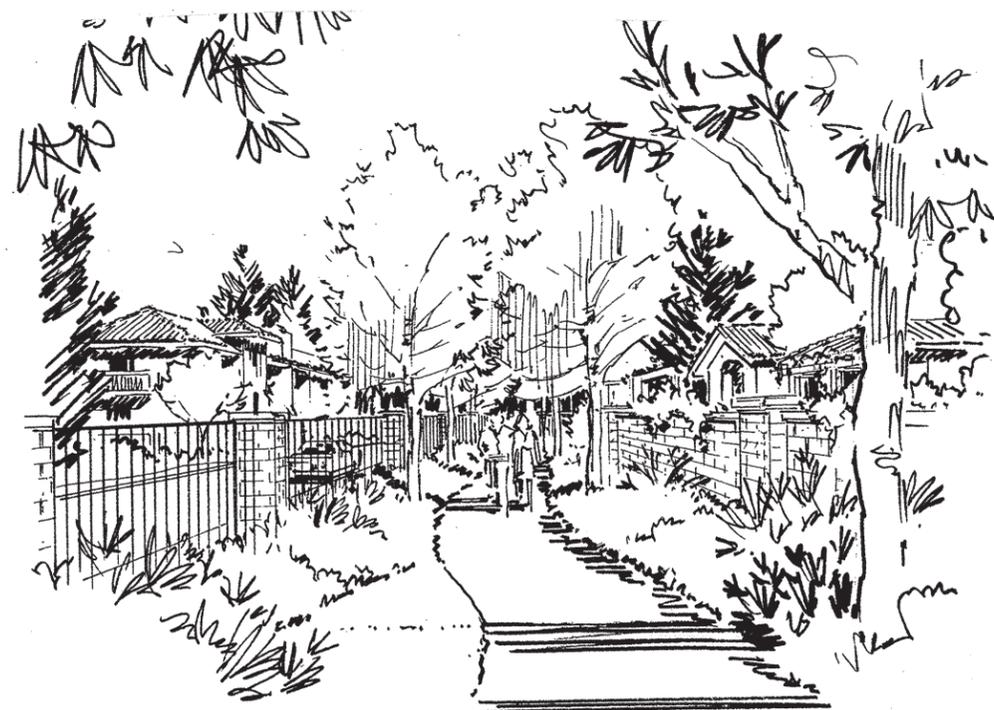
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# COMMUNITY TRAIL AND GREENBELT AT UTILITY EASEMENT



TYPICAL CROSS SECTION

Plan

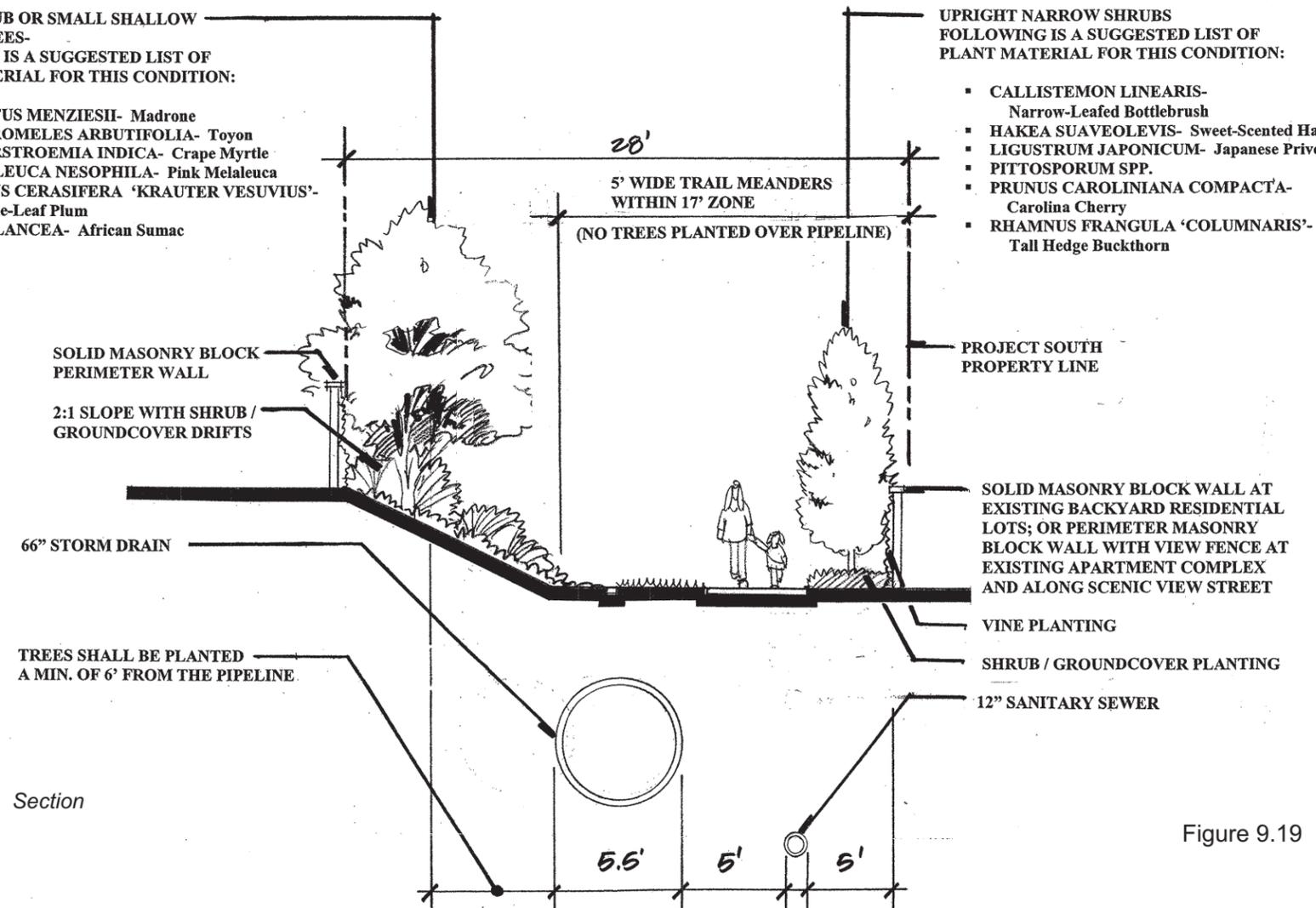


LARGE SHRUB OR SMALL SHALLOW ROOTED TREES-  
FOLLOWING IS A SUGGESTED LIST OF PLANT MATERIAL FOR THIS CONDITION:

- ARBUTUS MENZIESII- Madrone
- HETEROMELES ARBUTIFOLIA- Toyon
- LAGERSTROEMIA INDICA- Crape Myrtle
- MELALEUCA NESOPHILA- Pink Melaleuca
- PRUNUS CERASIFERA 'KRAUTER VESUVIUS'- Purple-Leaf Plum
- RHUS LANCEA- African Sumac

UPRIGHT NARROW SHRUBS  
FOLLOWING IS A SUGGESTED LIST OF PLANT MATERIAL FOR THIS CONDITION:

- CALLISTEMON LINEARIS- Narrow-Leaved Bottlebrush
- HAKEA SUAVEOLEVIS- Sweet-Scented Hakea
- LIGUSTRUM JAPONICUM- Japanese Privet
- PITTOSPORUM SPP.
- PRUNUS CAROLINIANA COMPACTA- Carolina Cherry
- RHAMNUS FRANGULA 'COLUMNARIS'- Tall Hedge Buckthorn



Section

The Colonies at San Antonio Specific Plan

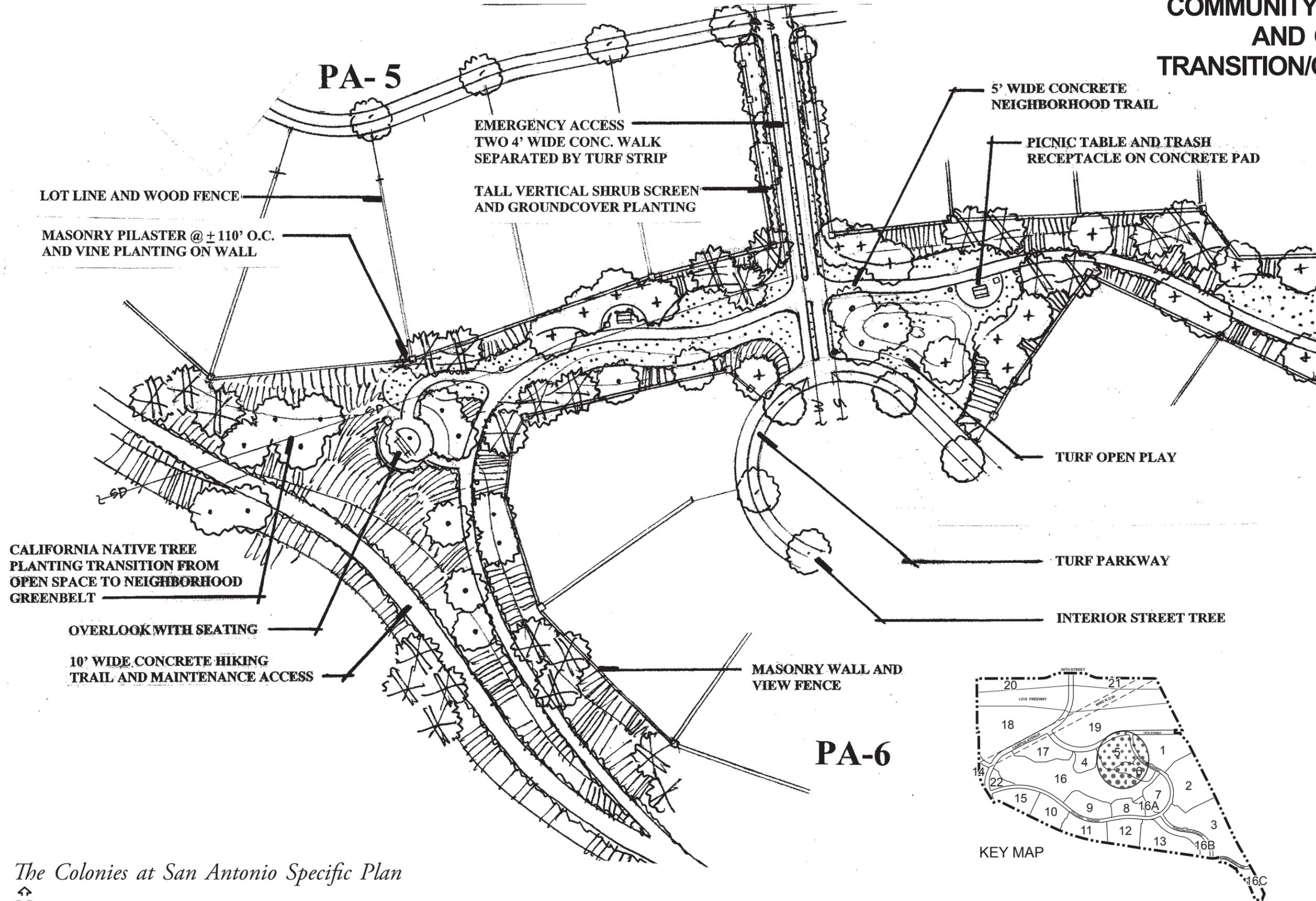
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Source: RJM Design Group.

Figure 9.19

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# COMMUNITY GREENBELT AND OPEN SPACE TRANSITION/CONNECTION



*The Colonies at San Antonio Specific Plan*

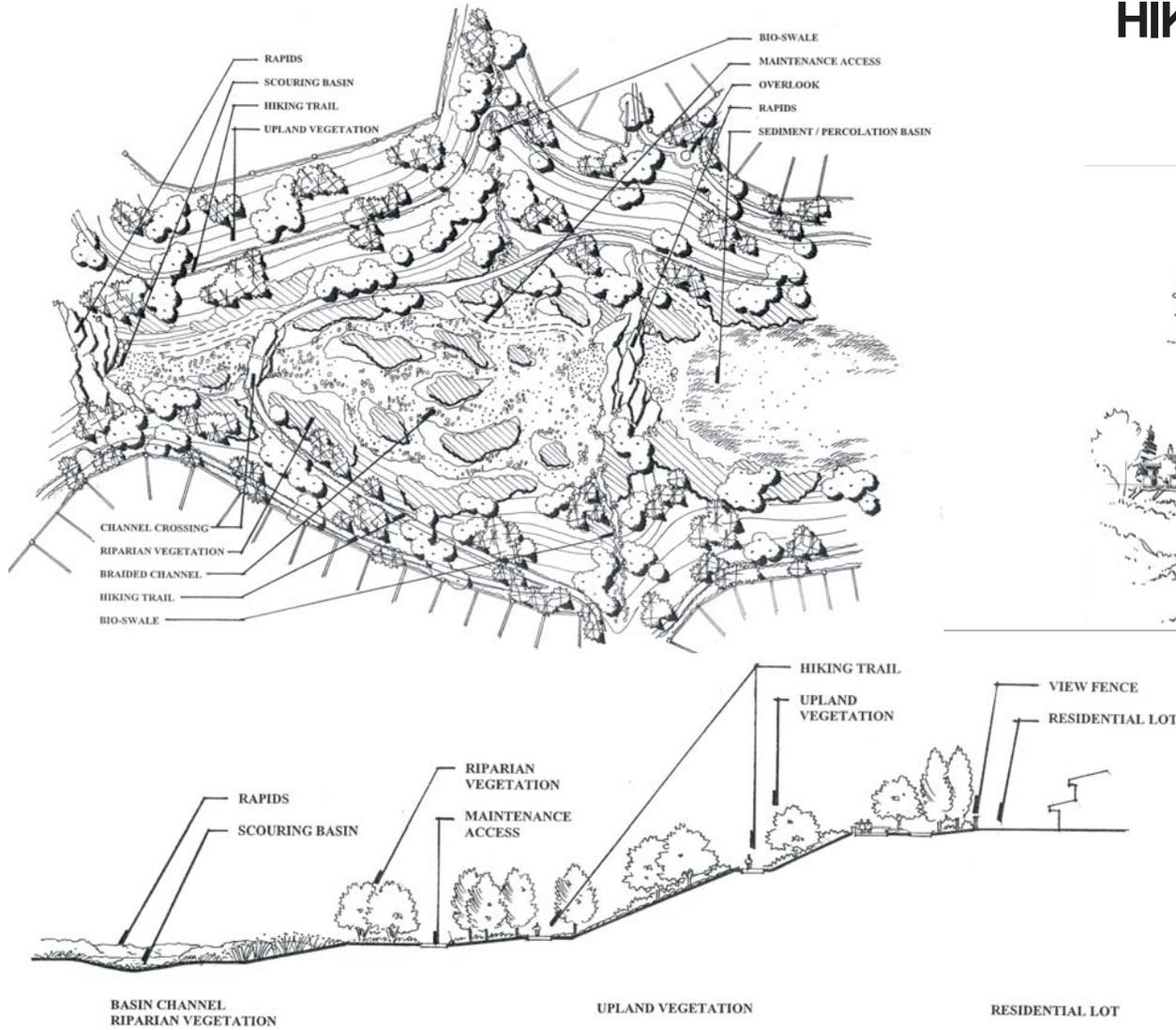
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Source: RJM Design Group.

Figure 9.20

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# COMMUNITY OPEN SPACE HIKING TRAIL CONCEPT



*The Colonies at San Antonio Specific Plan*

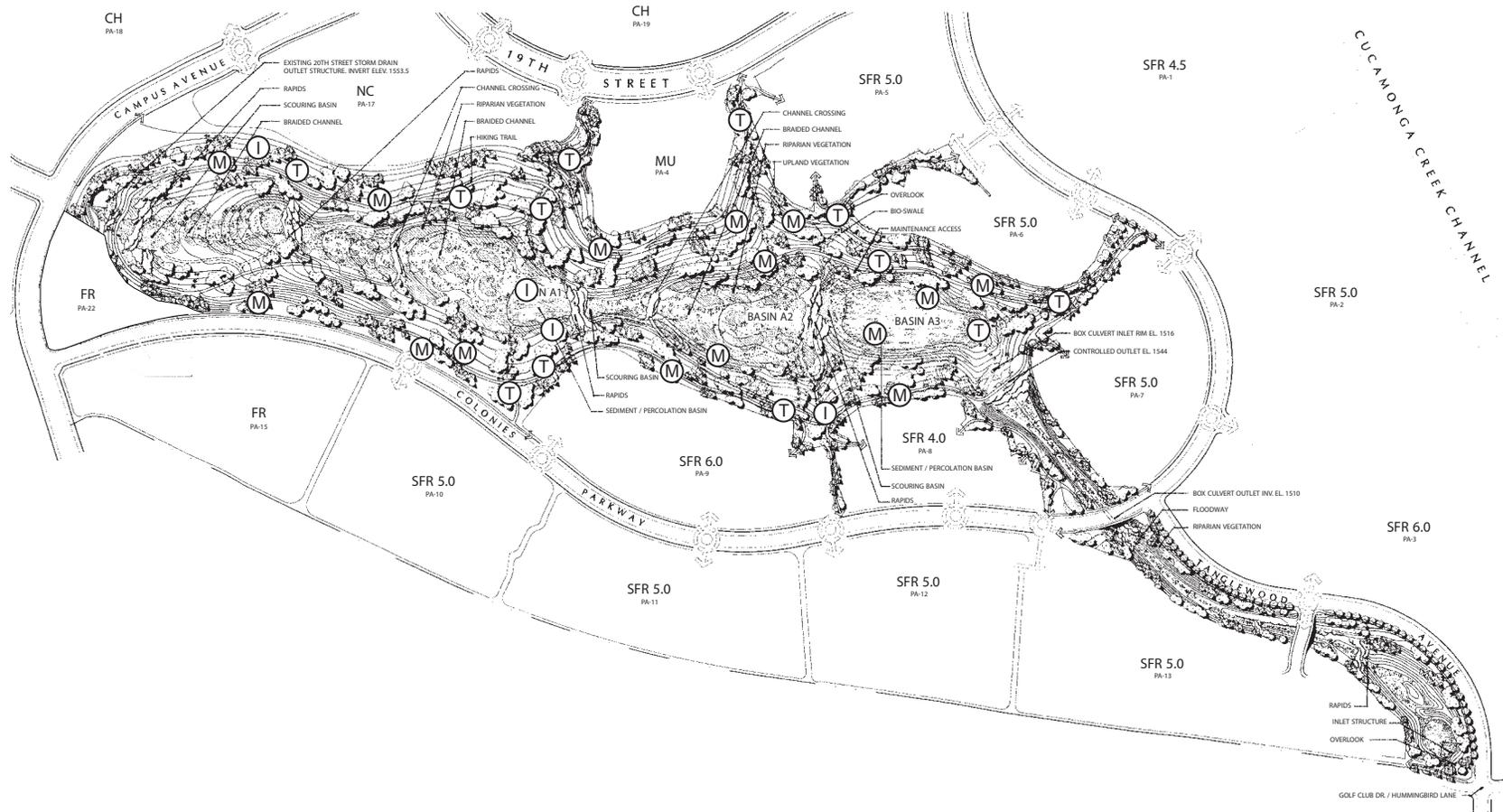
Figure 9.21

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# OPEN SPACE SIGNAGE CONCEPTUAL LOCATION

## LEGEND

-  Trail Head Entry Signage
-  Trail Marker Signage
-  Interpretive Signage



*The Colonies at San Antonio Specific Plan*  
Not to Scale

Figure 9.22

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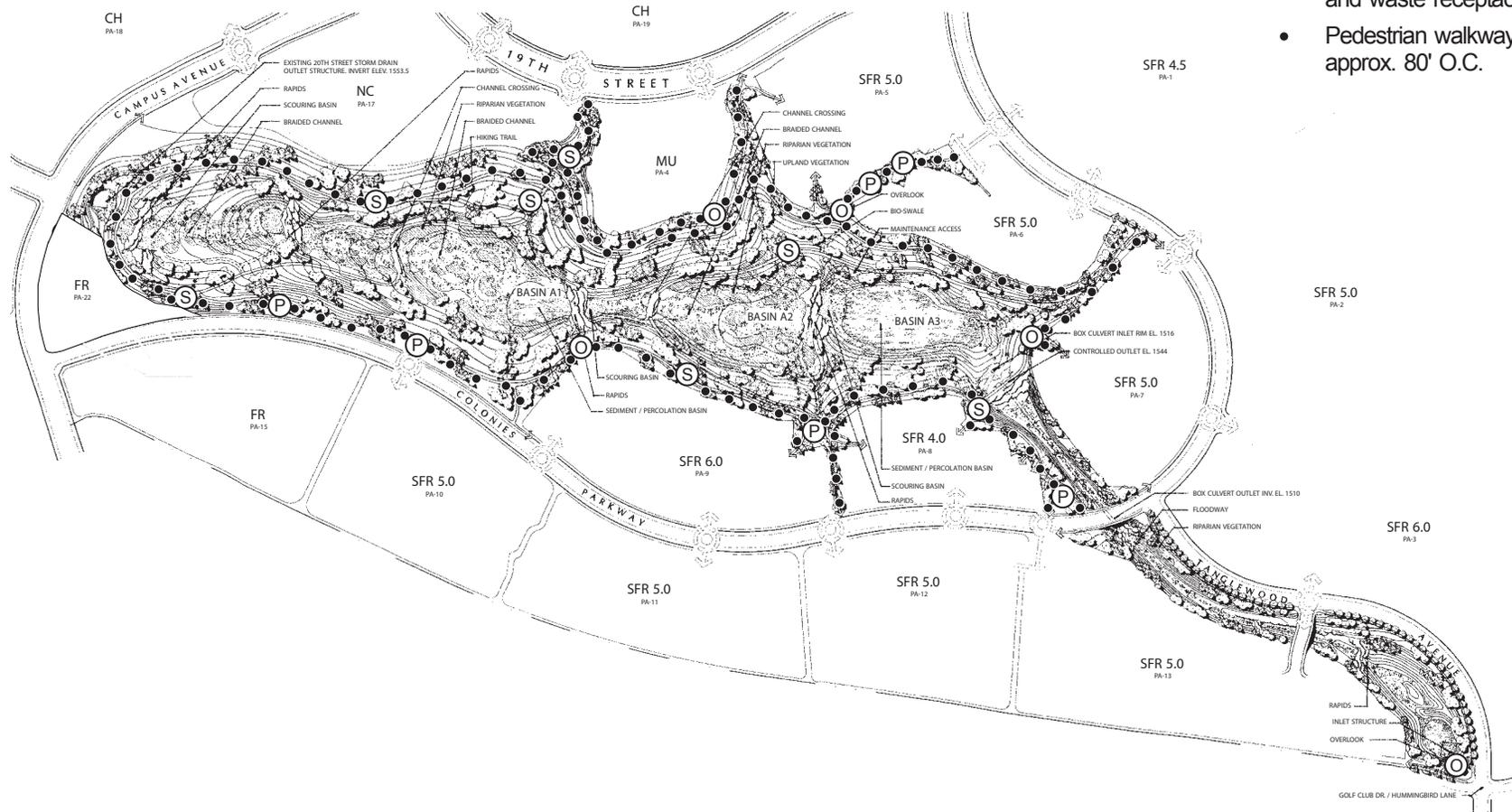


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# SITE FURNISHINGS CONCEPTUAL LOCATION

## LEGEND

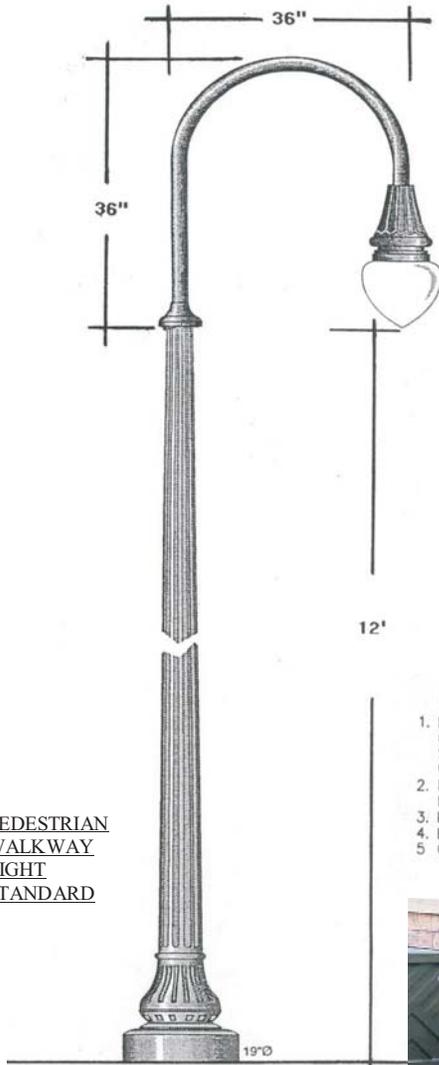
- ⊙ Overlook with bench and waste receptacle
- Ⓢ Seating area with bench and waste receptacle
- Ⓟ Picnic pad with picnic table and waste receptacle
- Pedestrian walkway light at approx. 80' O.C.



*The Colonies at San Antonio Specific Plan*  
Not to Scale

Figure 9.24

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SIMILAR PRODUCTS OF OTHER MANUFACTURERS MATCHING THE PHYSICAL PROPERTIES DESCRIBED HEREIN MAY BE ALLOWED, SUBJECT TO THE APPROVAL OF THE COMMUNITY DEVELOPMENT DIRECTOR.

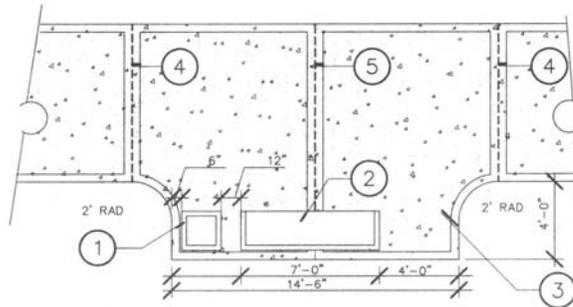
PEDESTRIAN WALKWAY LIGHT STANDARD

CLEAR POLY CARBONATE LENS WITH A CAST ALUMINIUM GLOBE HOLDER.

PRE-CAST CONCRETE POLE WITH "SALT & PEPPER" FINISH.

POWDER COAT OVER ZINC CHROMATE PRIMER COLOR: GREEN

BENCH AND WASTE RECEPTACLE PAD



LEGEND:

1. PRECAST CONCRETE, WASTE RECEPTACLE, MODEL NO. QS-VC2436 W VANCOUVER SERIES. COLOR: C-1, TEXTURE: T-7 LID MODEL # 0-A-LC6 CUSTOM COLOR TO MATCH BENCH. SEALER: STANDARD. MANUFACTURED BY: QUICKCRETE PRODUCTS CORP. OR APPROVED EQUAL.
2. METAL BENCH MODEL NO. 117-60, S-2, COLOR: GREEN. MANUFACTURED BY: DUMOR, INC. DISTRIBUTOR: THE WAKEFIELD CO. (949) 552-1130.
3. PLACE BENCH ON CONCRETE PAD PER MANUFACTURES SPECIFICATIONS
4. EXPANSION JOINT
5. CONTROL JOINT

PEDESTRIAN  
WALKWAY  
LIGHT  
STANDARD

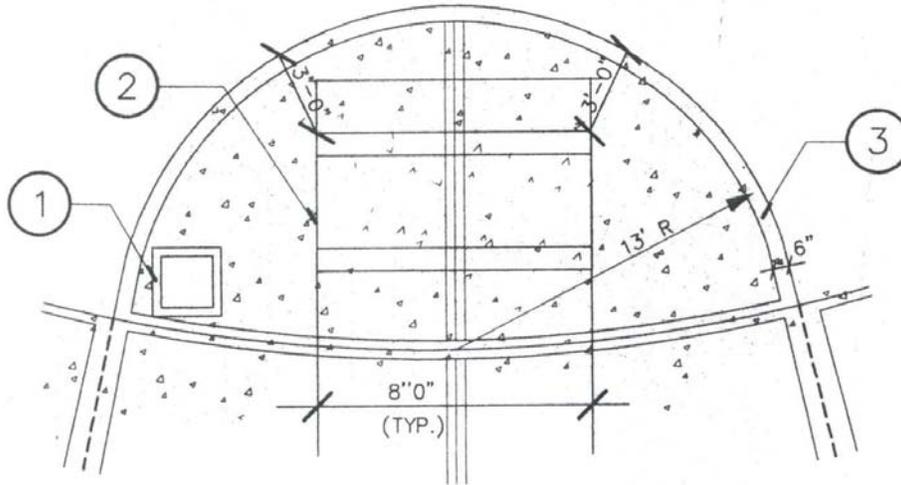


## SITE FURNITURE 1

*The Colonies at San Antonio Specific Plan*

Figure 9.25

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PICNIC TABLE PAD

SIMILAR PRODUCTS OF OTHER MANUFACTURERS MATCHING THE PHYSICAL PROPERTIES DESCRIBED HEREIN MAY BE ALLOWED, SUBJECT TO THE APPROVAL OF THE COMMUNITY DEVELOPMENT DIRECTOR.

LEGEND:

1. PRE CAST CONCRETE, WASTE RECEPTACLE, MODEL NO. QS-VC2436 W VANCOUVER SERIES. COLOR: C-1, TEXTURE: T-7 LID MODEL # 0-A-LC6 CUSTOM COLOR TO MATCH BENCH SEALER: STANDARD  
DISTRIBUTED BY/ MANUFACTURED BY:  
QUICKCRETE PRODUCTS CORP. OR APPROVED EQUAL.
2. PRE CAST CONCRETE PICNIC TABLE, MODEL Q-LBT-96PT, 8'L X40" W, COLOR C-1, TEXTURE: T-7 (SIDES), T-1 (TOP) WITH WOOD TEXTURE TABLE TOP.
3. PLACE TABLE ON CONCRETE PAD. PAD SHALL RECEIVE MEDIUM BROOM FINISH.



PRECAST CONCRETE PICNIC TABLE



PRECAST CONCRETE WASTE RECEPTACLE

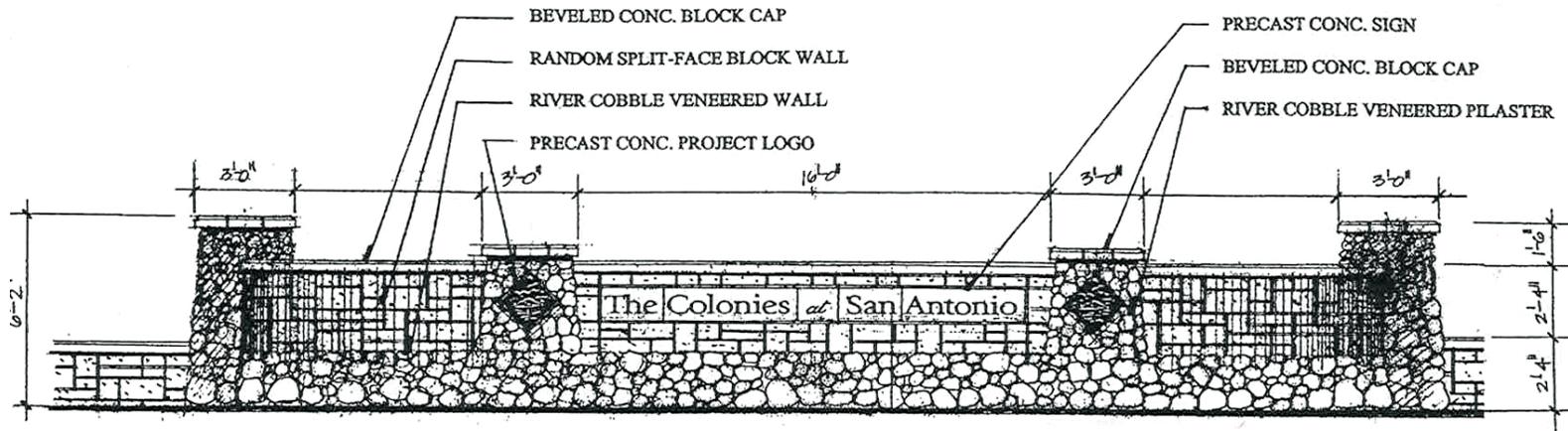
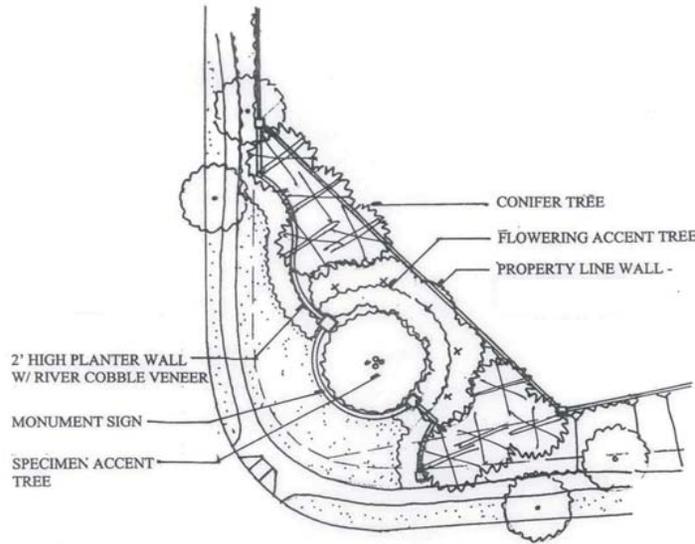
## SITE FURNITURE 2

*The Colonies at San Antonio Specific Plan*

Figure 9.26

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# PRIMARY INTERSECTION AND MONUMENT SIGN



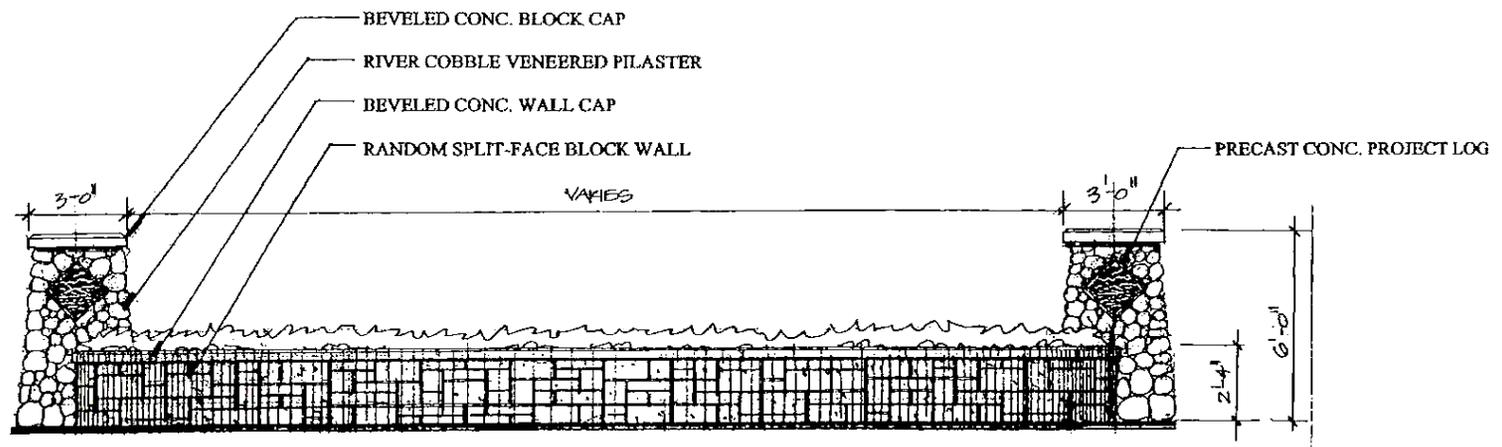
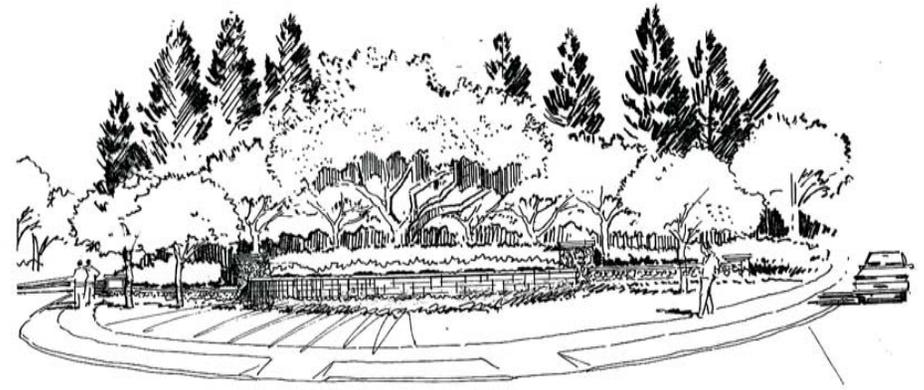
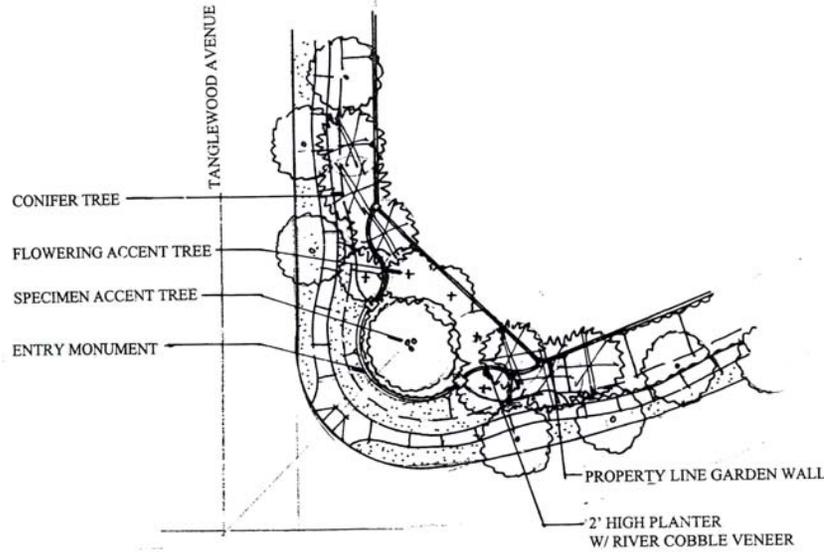
NOTE: COMMERCIAL AREA MONUMENTS WILL REFLECT MATERIALS AND DESIGNS CONSISTENT WITH THE COMMERCIAL ARCHITECTURE AND WILL BE LANDSCAPED WITH MATERIALS CONSISTENT WITH COMMERCIAL PALETTE.

*The Colonies at San Antonio Specific Plan*

Figure 9.27

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# SECONDARY INTERSECTION AND MONUMENT SIGN



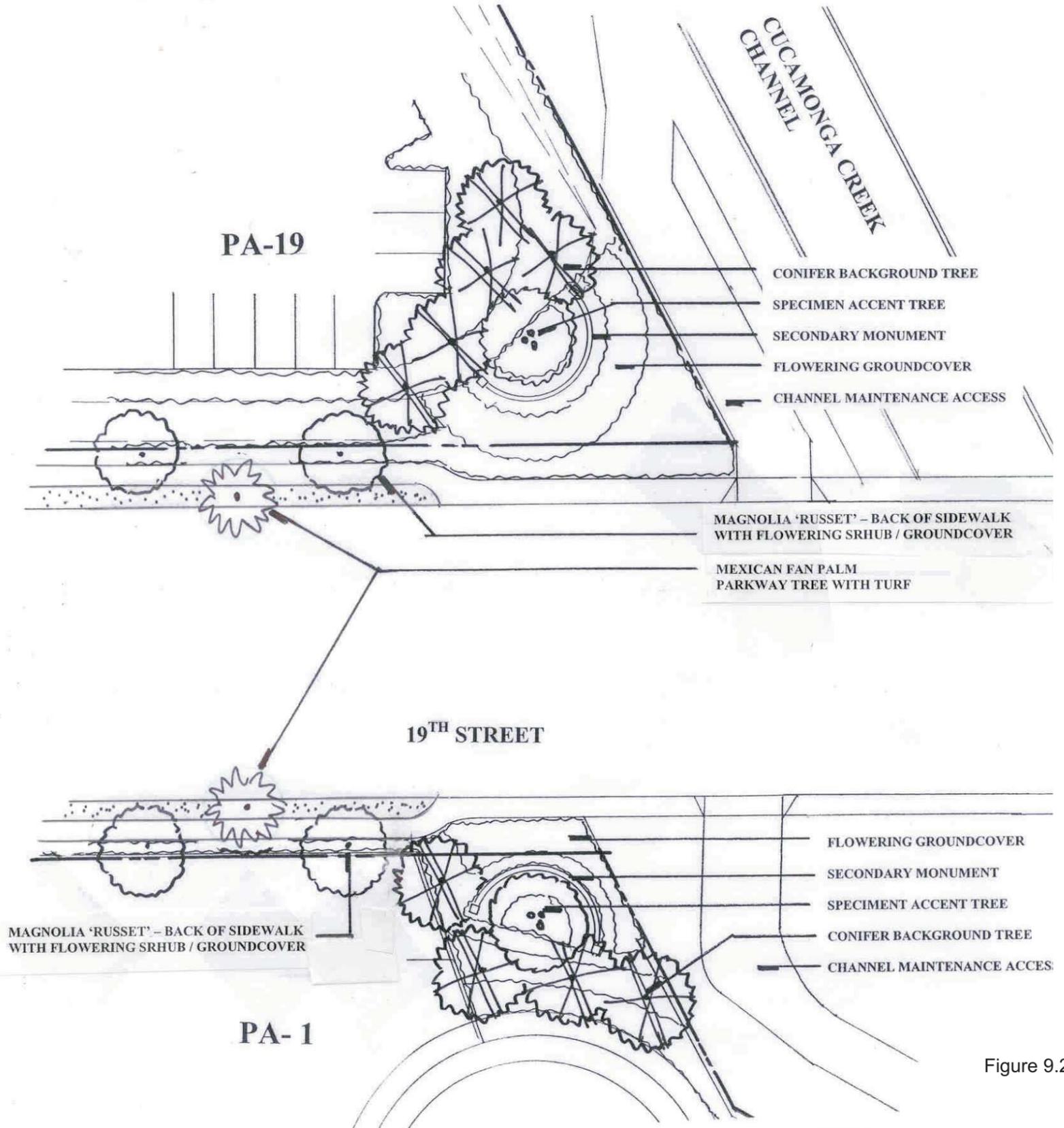
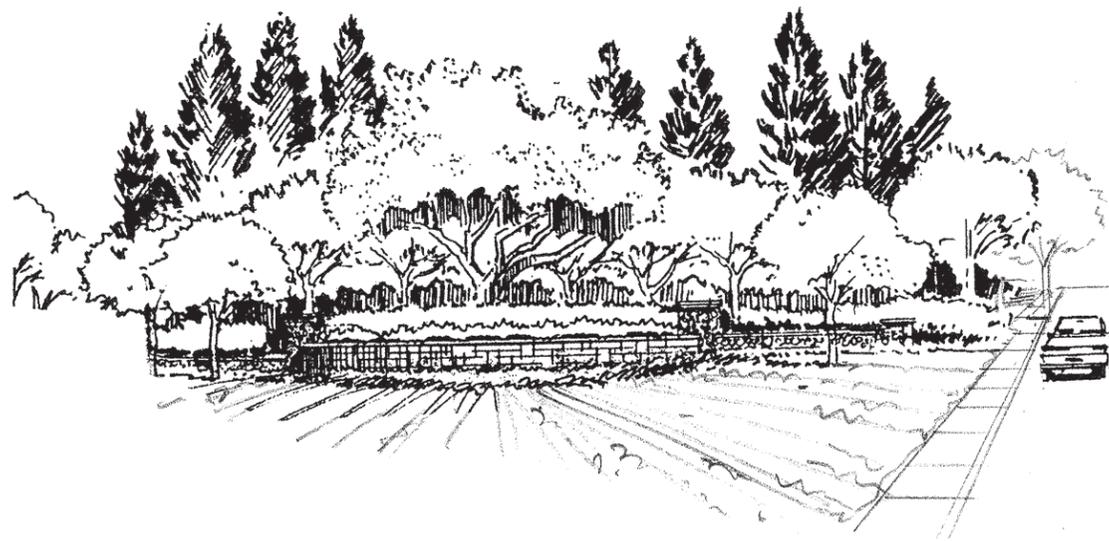
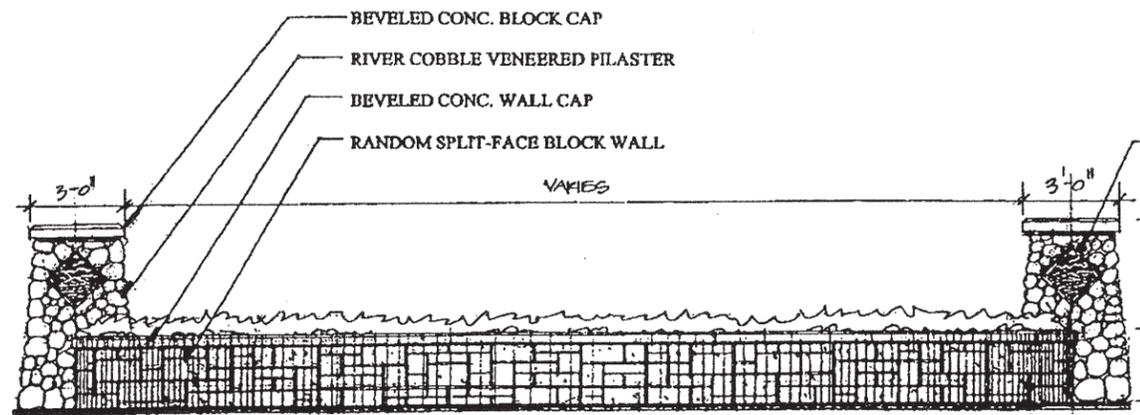
NOTE: COMMERCIAL AREA MONUMENTS WILL REFLECT MATERIALS AND DESIGNS CONSISTENT WITH THE COMMERCIAL ARCHITECTURE AND WILL BE LANDSCAPED WITH MATERIALS CONSISTENT WITH COMMERCIAL PALETTE.

*The Colonies at San Antonio Specific Plan*

Figure 9.28

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# EASTERLY PROJECT ENTRY



*The Colonies at San Antonio Specific Plan*

↑  
 N Not to Scale

Source: RJM Design Group.

Figure 9.29

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### 9.3.3 Commercial and Mixed Use Intersection and Monumentation

Special design treatments are proposed at the intersection of Campus Avenue and 19<sup>th</sup> Street, on the north side of the intersection of 19<sup>th</sup> Street and Colonies Parkway, and at commercial and mixed use entries and pad buildings on 19<sup>th</sup> Street and Campus Avenue in proximity to the Colonies at San Antonio commercial centers (Figure 9.30). Commercial and mixed use monuments will reflect materials and designs consistent with the nearby commercial architecture. Landscaping will include *Washington* sp. and *Phoenix* sp.

### 9.3.4 Neighborhood Entries and Monument Signs

Special landscaping is proposed at entries to individual residential neighborhoods along Colonies Parkway and Tanglewood Avenue (Figure 9.31).

- Neighborhood Monument Sign: River cobble veneered pilaster with concrete block cap.
- Tree: *Jacaranda mimosifolia*

### 9.3.5 Streetscape Landscaping

Designated street tree and groundcover plantings throughout the Specific Plan area will work together with the entry monumentation to define circulation patterns and create individual street identity. The following tree species may be subject to change, based upon availability of the tree material.

**a. Colonies Parkway.** The proposed streetscape for Colonies Parkway is illustrated in Figure 9.32.

- Parkway Tree: *Magnolia grandiflora* Magnolia
- Median Tree: *Phoenix canariensis* Canary Island Date Palm
- Parkway Groundcover: *Lantana* ‘Spreading Sunset’ Lantana
- Median Groundcover: *Hemerocallis* spp. Daylily

**b. Campus Avenue.** The proposed streetscape for Campus Avenue is illustrated in Figure 9.33.

- Groundcover: water saver sod turf Fescue Blend
- Median Groundcover: *Hemerocallis* spp. Day Lily

**c. 19<sup>th</sup> Street.** The proposed streetscape for 19<sup>th</sup> Street is illustrated in Figure 9.33.

- Parkway Tree: As approved in landscape plans.
- Groundcover: *Gazania* ‘Mitsuwa Orange’ Gazania  
water saver sod turf Fescue Blend  
*Trachelospermum jasminoides* Star Jasmine

**d. Tanglewood Avenue.** The proposed streetscape for Tanglewood Avenue is illustrated in Figure 9.34.

- Parkway Tree: *Ulmus parvifolia* ‘True Green’ Evergreen Elm
- Median Tree: *Phoenix canariensis* Canary Island Date Palm
- Parkway Groundcover: *Lantana* ‘Spreading Sunset’ Lantana
- Median Groundcover: *Hemerocallis* spp. Daylily

e. **20th Street.** The proposed streetscape for 20<sup>th</sup> Street is illustrated in Figure 9.34.

- Parkway Tree: *Ulmus parvifolia* ‘True Green’ Evergreen Elm
- Parkway Groundcover: *Rosmarinus officinalis prostratus* Rosemary

f. **Hummingbird Lane.** The proposed streetscape for Hummingbird Lane is illustrated in Figure 9.35.

- Parkway Tree: *Ulmus parvifolia* ‘True Green’ Evergreen Elm
- Parkway Groundcover: *Lantana* ‘Spreading Sunset’ Lantana

## 9.4 COMMERCIAL AND MIXED USE LANDSCAPE CONCEPTS

An important part of the Colonies at San Antonio is the proposed retail development, which will be highly visible from the I-210 freeway. Landscape design for these commercial and mixed use areas will enhance the circulation patterns and add spatial organization in the parking areas. Parking lots and service areas will be screened from view with landscape plantings or screen walls. Building entries, plazas, and other pedestrian zones will be articulated and enhanced with more refined textures and colors in the hardscape and landscape planting.

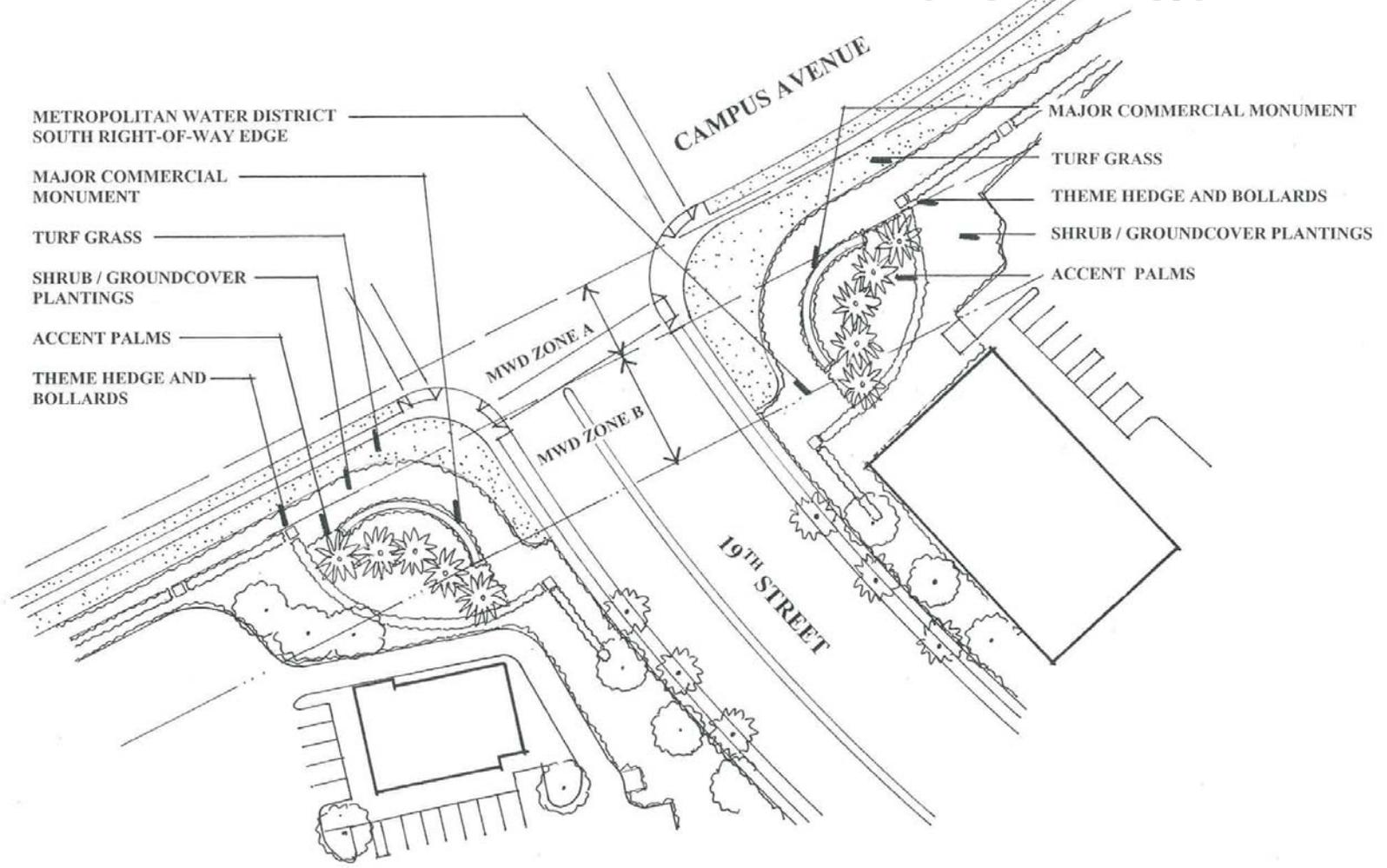
The design for all entry monumentation, signage, site furnishings, and fixtures works together to provide a unique signature for the overall community character of The Colonies at San Antonio.

The landscape within commercial and mixed use areas should be developed in an organized manner to help define pedestrian and vehicular circulation patterns, with accent trees to identify project entries and key focal points.

The following plants are approved for use in the Specific Plan’s commercial and mixed use areas. Plants other than those listed may be used to satisfy design or horticultural needs consistent with the project’s objectives if approved by the City.

- *Arbutus* ‘Marina’ – Madrone
- *Cedrus deodara* – Deodar Cedar
- *Cupaniopsis anacardioides* – Carrot Wood
- *Eriobotrya* ‘Coppertone’ – Loquat
- *Geijera parvifolia* – Australian Willow
- *Koelreuteria paniculata* – Golden Rain Tree
- *Lagerstroemia indica* ‘Varieties’ – Crape Myrtle
- *Laurus nobilis* – Sweet Bay
- *Liquidambar styraciflua* ‘Palo Alto’ and ‘Burgundy’ – Sweet Gum
- *Magnolia grandiflora* ‘Russet’ – Magnolia
- *Magnolia grandiflora* ‘Samuel Sommer’ – Magnolia
- *Magnolia grandiflora* ‘San Marino’ – Magnolia
- *Magnolia grandiflora* – Magnolia
- *Olea europaea* – Olive
- *Phoenix canariensis* – Canary Island Date Palm
- *Phoenix dactylifera* – Date Palm
- *Pinus canariensis* – Canary Island Pine
- *Pinus eldarica* – Mondell Pine
- *Pinus pinea* – Italian Stone Pine
- *Pistacia chinensis* – Chinese Pistache
- *Platanus acerifolia* – London Plane Tree

# COMMERCIA AND MIXED USE INTERSECTION AND MONUMENT SIGN

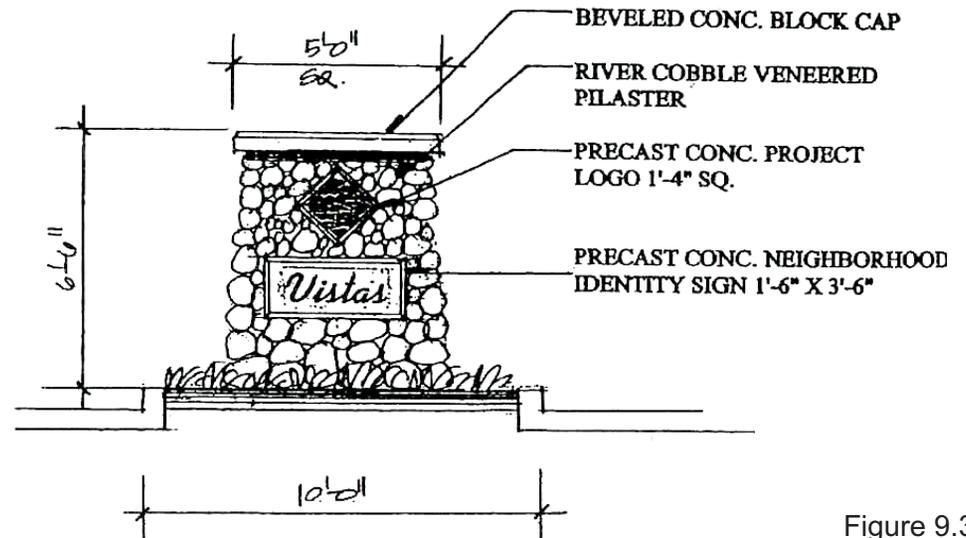
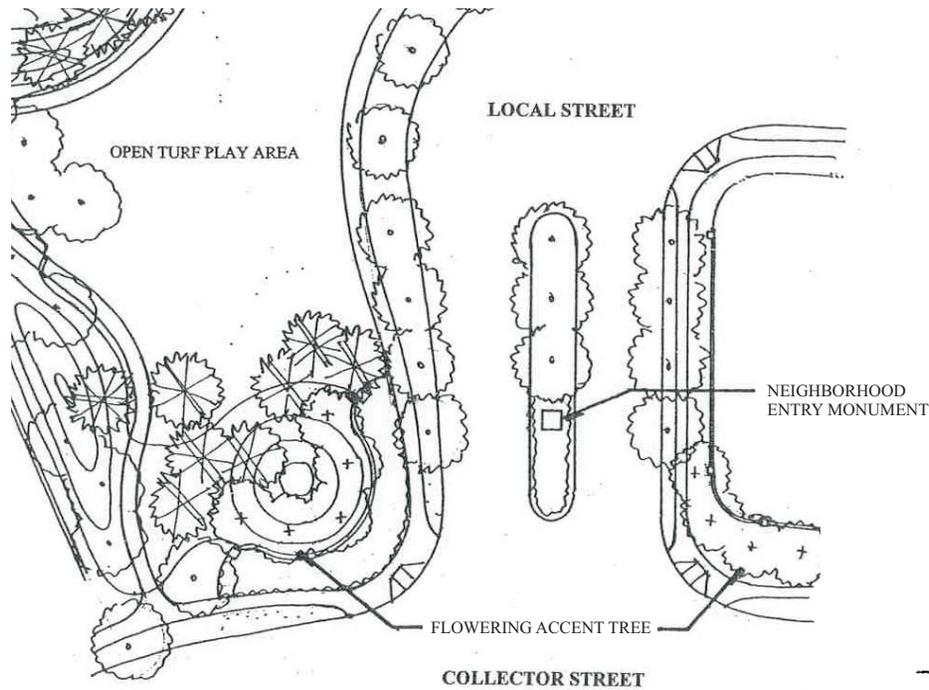


*The Colonies at San Antonio Specific Plan*

Figure 9.30

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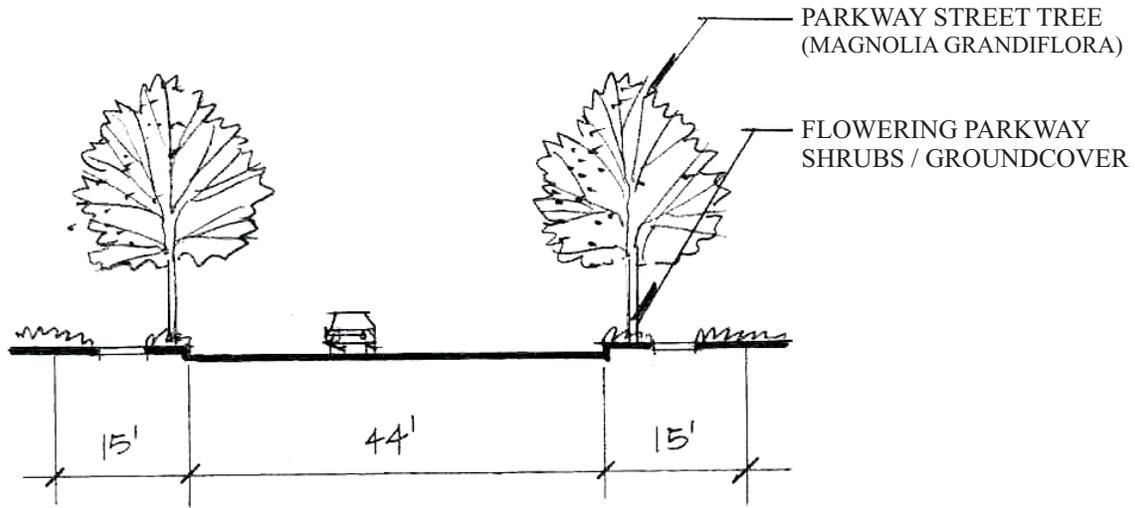
# NEIGHBORHOOD INTERSECTION AND MONUMENT SIGN



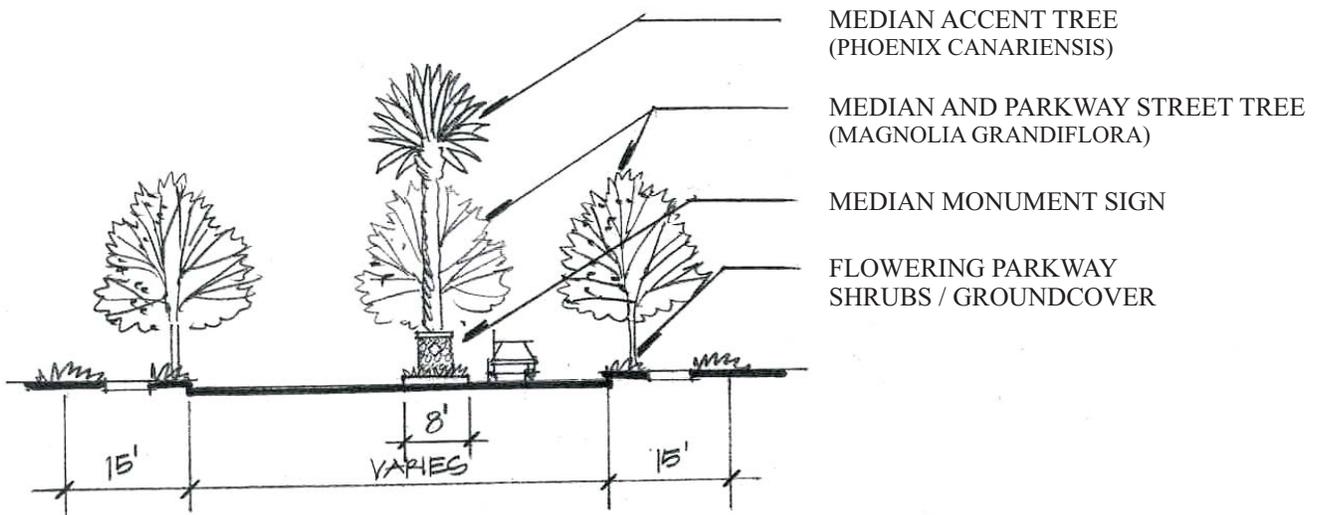
*The Colonies at San Antonio Specific Plan*

Figure 9.31

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*Colonies Parkway*



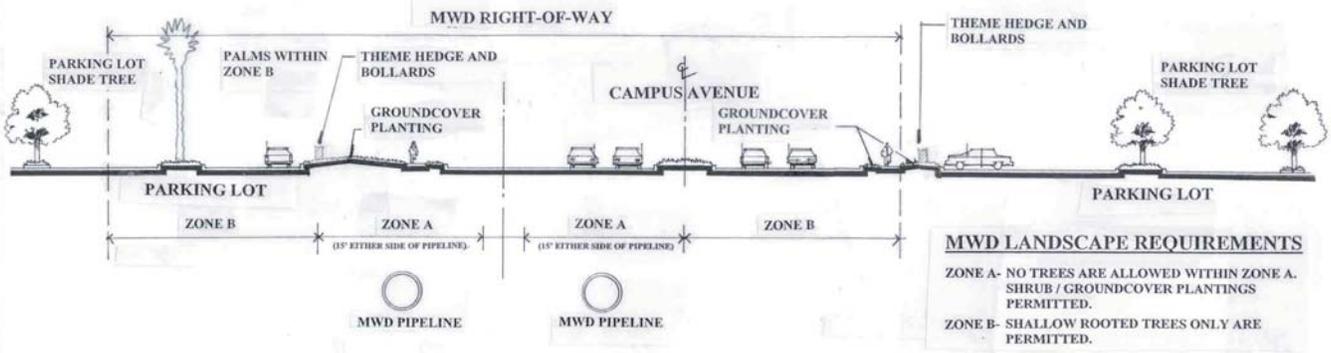
*Colonies Parkway with median.*

## ROADWAY LANDSCAPE COLONIES PARKWAY

*The Colonies at San Antonio Specific Plan*

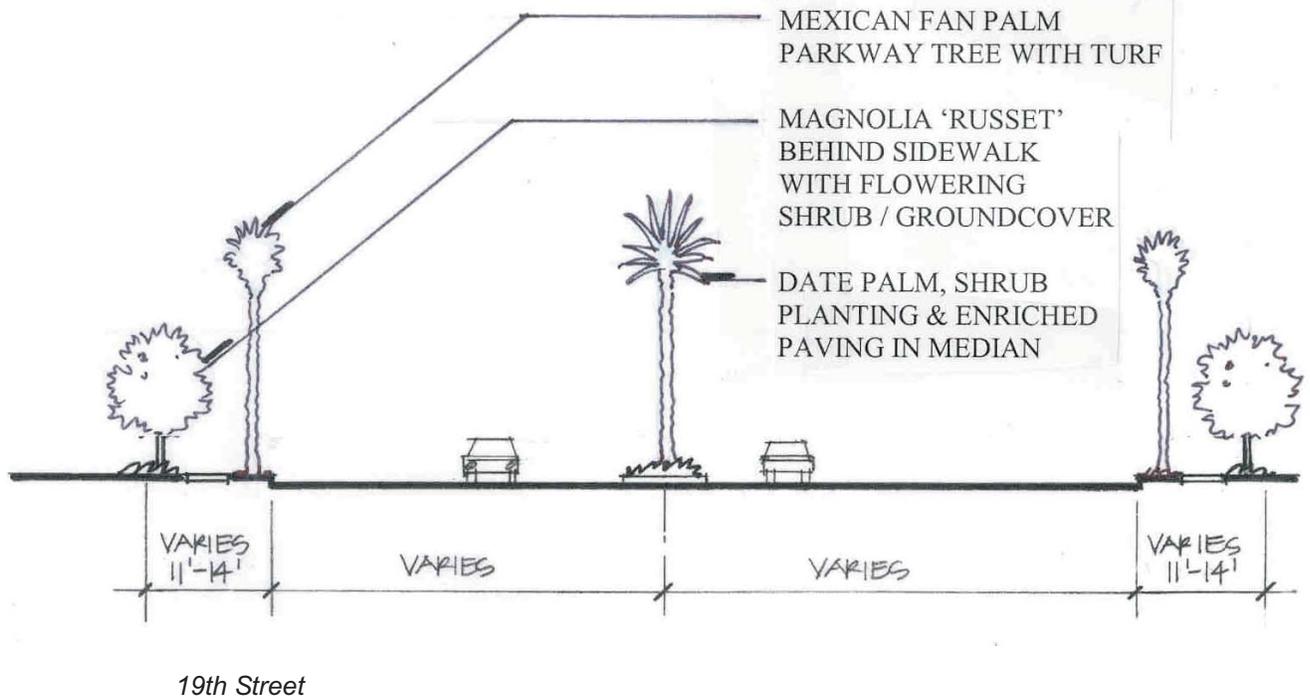
Figure 9.32

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**Campus Avenue**

Note: Portions of Campus Avenue lie within the Metropolitan Water District right-of-way. All planting shall comply with MWD requirements and approval.

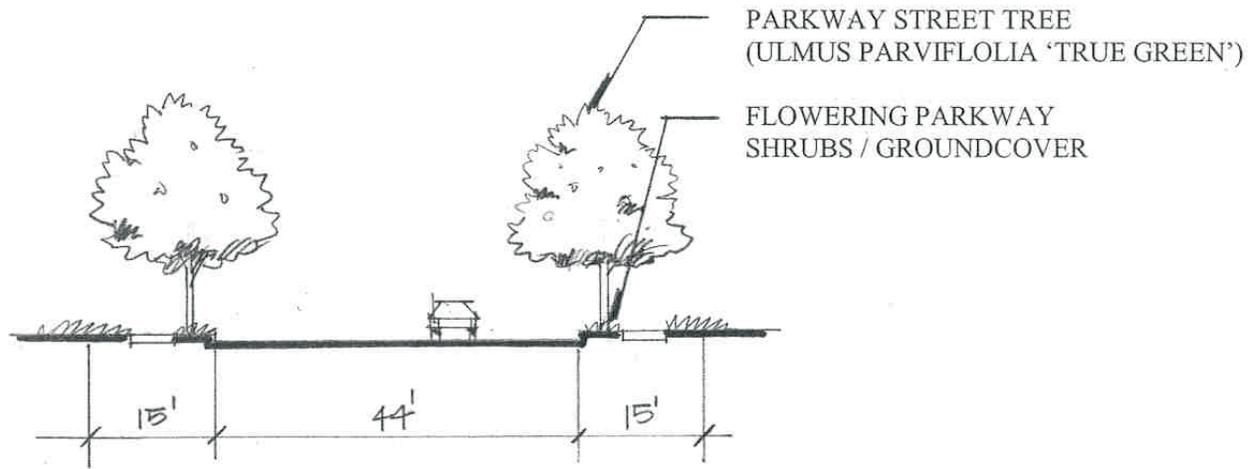


**ROADWAY LANDSCAPE  
19TH STREET EAST OF CAMPUS AVENUE**

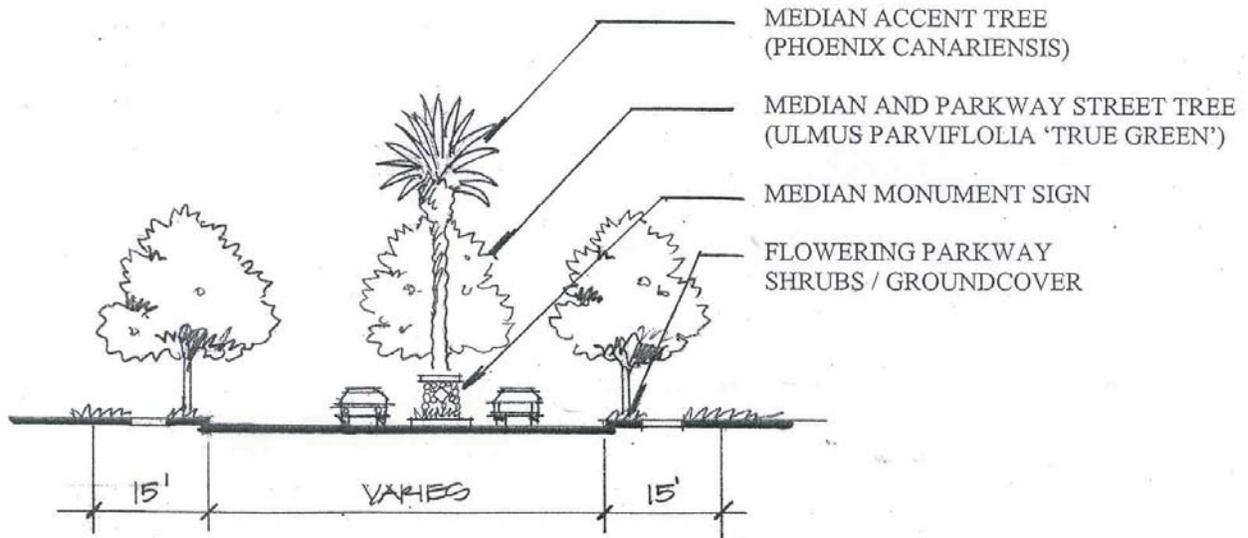
*The Colonies at San Antonio Specific Plan*

Figure 9.33

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*Tanglewood Avenue/20th Street*



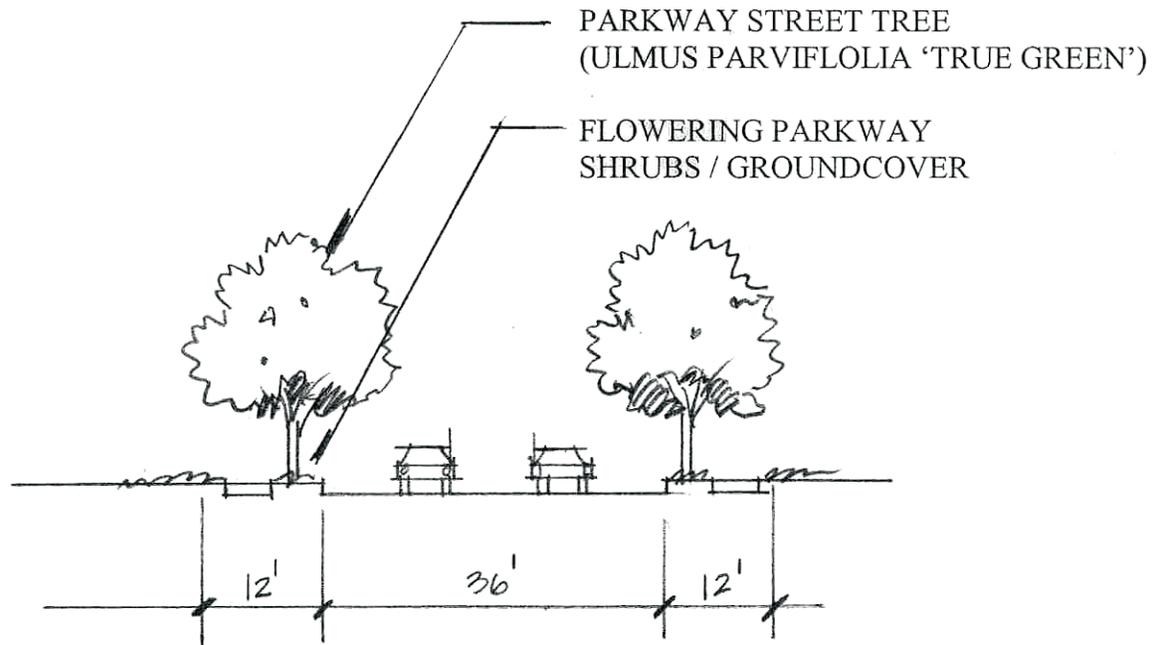
*Tanglewood Avenue with median.*

## ROADWAY LANDSCAPE TANGLEWOOD AVENUE AND 20TH STREET

*The Colonies at San Antonio Specific Plan*

Figure 9.34

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*Hummingbird Lane*

## ROADWAY LANDSCAPE HUMMINGBIRD LANE

*The Colonies at San Antonio Specific Plan*

Figure 9.35

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- *Prunus cerasifera* ‘Krauter Vesuvius’ – Purple Leaf Plum
- *Prunus cerasifera* ‘Purple Pony Plum’ – Purple Pony Plum
- *Pyrus calleryana* – Bradford Pear
- *Rhus lancea* – African Sumac
- *Schinus molle* – California Pepper Tree
- *Tabebuia chrysotricha* - Golden Trumpet Tree
- *Tabebuia ipe* – Pink Trumpet Vine
- *Tristania conferta* – Brisbane Box
- *Tristania laurina* - Water Gum
- *Washingtonia robusta* – Mexican Fan Palm

The following lists serve to categorize the above trees according to characteristics that make them suitable for various purposes. These lists also identify plants appropriate for use as shrubs and hedges, accents, and ground covers. Some species are multi-purpose and appear in more than one category.

#### 9.4.1 Vertical Trees

- *Phoenix* sp.
- *Washingtonia* sp.

#### 9.4.2 Parking Area or Small/Medium Trees

- *Arbutus* m.
- *Cupaniopsis* a.
- *Eriobotrya* d.
- *Geijera* p.
- *Laurus* n.
- *Magnolia* sp.
- *Olea* sp.
- *Prunus* sp.
- *Pyrus* c.
- *Rhus* l.
- *Tristania* l.

#### 9.4.3 Buffer/Screen Trees

- *Cedrus* d.
- *Cupaniopsis* a.
- *Lagerstroemia*
- *Magnolia* sp.
- *Olea* sp.
- *Pinus* sp.
- *Pistacia* c.
- *Platanus*
- *Prunus* sp.
- *Tristania* c.

#### **9.4.4 Shrubs and Hedges**

- *Buxus*
- *Eleagnus*
- *Escallonia*
- *Euonymous*
- *Ligustrum*
- *Prunus*
- *Rhaphiolepis*
- *Xylosma*

#### **9.4.5 Accent Plants**

- *Agapanthus*
- *Dietes*
- *Hemerocallis*
- *Philodendron*
- *Phormium*
- *Tulbaghia*

#### **9.4.6 Ground Covers**

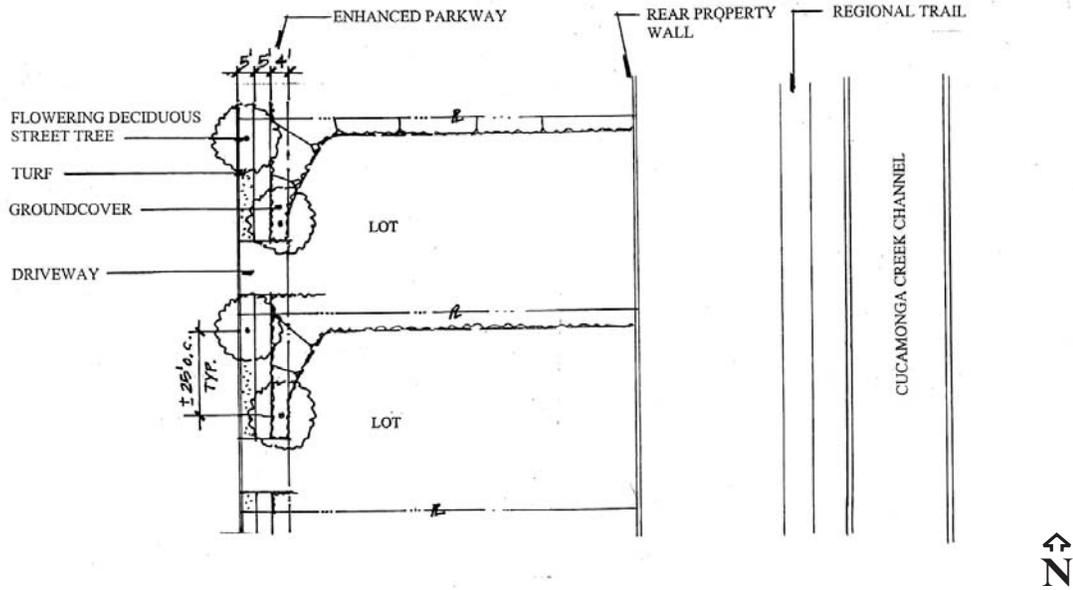
- *Hedera*
- *Hemerocallis*
- *Lonicera*
- *Myoporum*
- *Rhaphiolepis*
- *Trachelospermum*
- *Tulbaghia*
- Turf

### **9.5 RESIDENTIAL LANDSCAPE CONCEPTS**

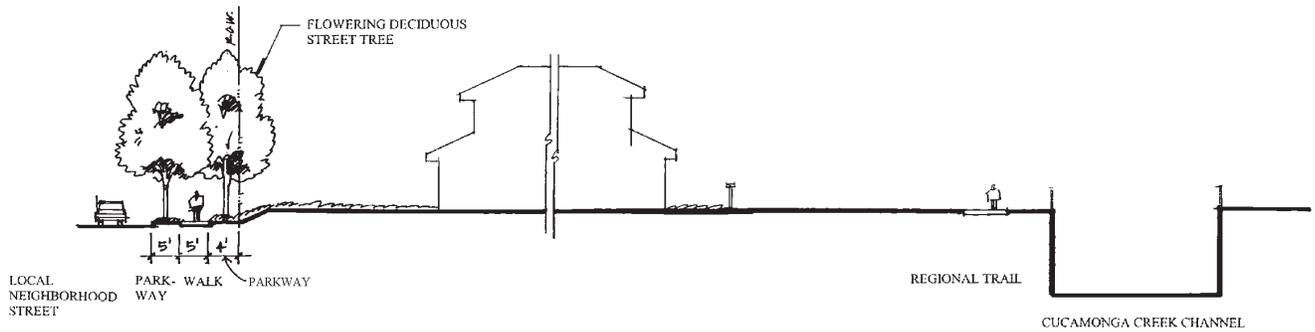
The Colonies at San Antonio residential community is defined by neighborhood entry landscape treatments and traditional streetscape planting. Streets that serve individual neighborhoods should emphasize cul-de-sacs or loop roads to reduce through traffic and promote the sense of neighborhood.

Each neighborhood will have direct links to the community trail and greenbelt system. Entries to the trail and greenbelt corridors will occur at key locations, primarily at the ends of street cul-de-sacs. Each entry will be identified by a specimen tree to serve as a focal point. The community trail will meander through the greenbelt corridor and provide a link to the detention basin natural open space, to the Cucamonga Creek Channel regional trail and to other neighborhoods. Opportunities will be provided for passive recreation to include seating, picnicking and informal family play. Landscaped areas will provide a buffer between the trail and the adjacent residential lots. Neighborhood trail links to the open space of Planning Area 16 will bring the site's natural resources into the community.

Within Planning Areas 2 and 3 an enhanced parkway provides a link from the neighborhood to the regional trail (Figure 9.36). The enhanced parkway will occur on the east side of the local north/south neighborhood street, which fronts the residential lots along the Cucamonga Creek Channel right-of-way.



Plan View



Elevation

## ENHANCED PARKWAY

*The Colonies at San Antonio Specific Plan*

Figure 9.36

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The enhanced parkway will provide a 5-foot wide parkway, 5-foot wide sidewalk, and 4-foot wide parkway to allow for a double row of flowering canopy trees (*Pyrus calleryana* 'Bradford') in order to create a visual link and provide direction to the community trail/greenbelt and regional trail along the Cucamonga Creek Channel.

Drought tolerant plant selections will reflect an awareness of the natural environment in regard to water conservation.

### 9.5.1 Residential Plant Palette

The landscape treatment in each of the residential neighborhoods shall provide a sense of identity and continuity between neighborhoods. Following is a list of recommended trees for the project's residential areas, including community greenbelts and trails.

- *Alnus rhombifolia* - White Alder
- *Arbutus menziesii* - Madrone
- *Bauhinia variegata* - Purple Orchid Tree
- *Callistemon linearis* - Narrow-Leafed Bottlebrush
- *Chionanthus retusus* - Chinese Fringe Tree
- *Cedrus deodara* - Deodar Cedar
- Citrus - Valencia Orange
- *Geijera parvifolia* - Australian Willow
- *Hakea suaveolevis* - Sweet-Scented Hakea
- *Heteromeles arbutifolia* - Toyon
- *Jacaranda mimosifolia* - Jacaranda
- *Koelreuteria paniculata* - Golden Rain Tree
- *Lagerstroemia indica* - Crape Myrtle varieties
- *Ligustrum japonium* - Japanese Privet
- *Liquidambar styraciflua* - American Sweet Gum 'Burgundy'
- *Melaleuca nesophila* - Pink Melaleuca
- *Olea europaea* - Olive
- *Pinus eldarica* - Mondell Pine
- *Pinus halepensis* - Aleppo Pine
- *Pistachia chinensis* - Chinese Pistache
- *Pittosporum* spp. - No common name
- *Platanus racemosa* - California Sycamore
- *Prunus cerasifera* 'Newport' - Flowering Plum
- *Prunus cerasifera* 'Krauter Vesuvius' - Purple Leaf Plum
- *Prunus cerasifera* 'Purple Pony Plum' - Purple Pony Plum
- *Pyrus calleryana* 'Bradford' - Bradford Pear
- *Quercus ilex* - Holly Oak
- *Rhus lancea* - African Sumac
- *Tabebuia chrysotricha* - Golden Trumpet Tree
- *Tabebuia ipe* - Pink Trumpet Tree
- *Tristania conferta* - Brisbane Box
- *Tristania laurina* - Water Gum

## 9.5.2 Residential Streetscape

Each residential planning area shall feature a selected street tree type, to differentiate the neighborhood and provide its own sense of identity. No tree type shall be used in more than three residential planning areas and the same tree type shall not be used in contiguous planning areas. Following is a list of acceptable trees.

- *Agonis flexuosa* - Peppermint Willow
- *Bauhinia variegata* - Purple Orchid Tree
- *Chionanthus retusus* - Chinese Fringe Tree
- *Geijera parvifolia* - Australian Willow
- *Ginkgo biloba* 'Autumn Gold' - Maidenhair Tree
- *Lagerstroemia indica* 'Varieties' - Crape Myrtle
- *Pittosporum phillyraeoides* - Willow Pittosporum
- *Pistachia chinensis* - Chinese Pistache
- *Prunus cerasifera* 'Varieties' – Purple Leaf Plum
- *Rhus lancea* - African Sumac
- *Tabebuia chrysotricha* – Golden Trumpet Tree
- *Tabebuia ipe* – Pink Trumpet Tree

## 9.6 EDGE TREATMENTS

Landscape edge treatments will be employed to provide a transition between differing adjacent uses, such as the commercial planning areas adjacent to the Caltrans right-of-way of I-210, and the commercial areas adjacent to the Cucamonga Creek Channel. These edge treatments will respond to grading and slope requirements, and provide a buffer between adjacent uses.

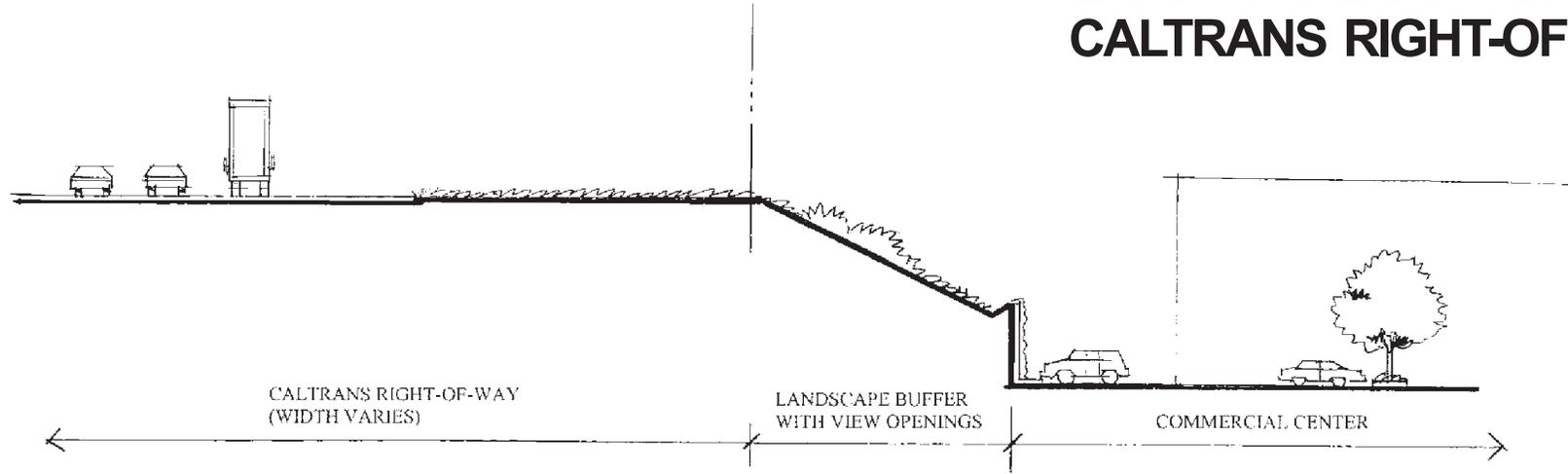
The specific requirements for each planning area will dictate the extent, type and manner of edge treatment. For example, edge treatments between commercial areas adjacent to I-210 will require the installation of landscaping which enhances the appearance and visibility of commercial uses, while edge treatments between commercial and mixed use and public uses will emphasize landscaping which screens, and limits access.

Along the I-210 freeway edge, any retaining walls shall be either textured and planted with vines; “keystone” or “crib-wall” to provide planting pockets; or veneered with artificial rock formation. Plantings shall be arranged in groupings to screen service areas, but allow views from the freeway to commercial building entrances and signage (Figure 9.37).

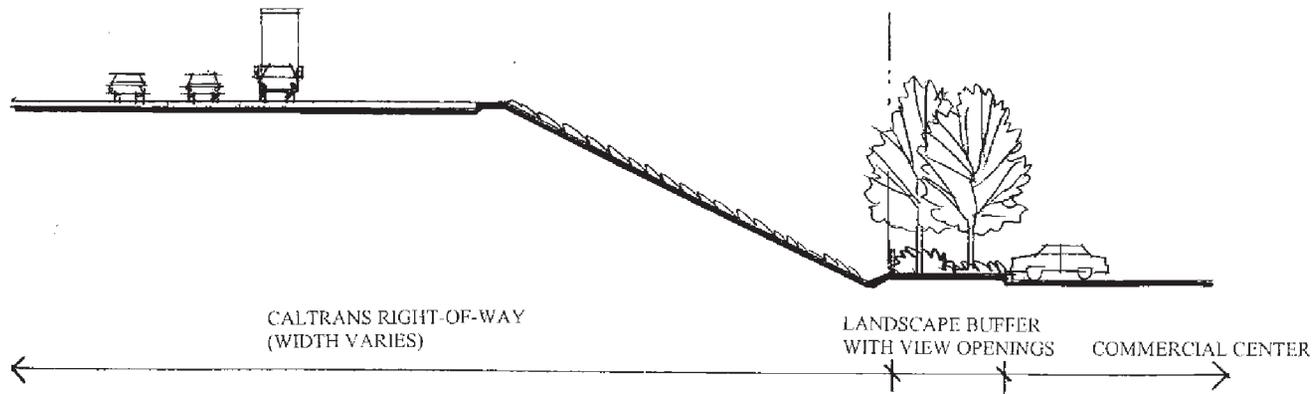
Within Planning Areas 19 and 21, a landscaped buffer located at the eastern edge of the Planning Areas adjacent to the Cucamonga Creek Channel shall screen commercial development (Figure 9.38).

- A landscaped buffer at least 25 feet wide shall run the entire eastern boundary of Planning Areas 19 and 21.
- Evergreen trees and shrubs shall be placed within the landscaped buffer so as to form upon plant maturity a continuous screen of commercial development as viewed from east of the Cucamonga Creek Channel.
- The use of deciduous trees and ground cover in addition to the evergreen tree/shrub screen is encouraged.
- Trees used in the evergreen screen shall be at least 24-inch box in size.
- Retaining walls may be located within the landscaped buffer as necessary.

# LANDSCAPE BUFFER AT CALTRANS RIGHT-OF-WAY



*Retaining condition*



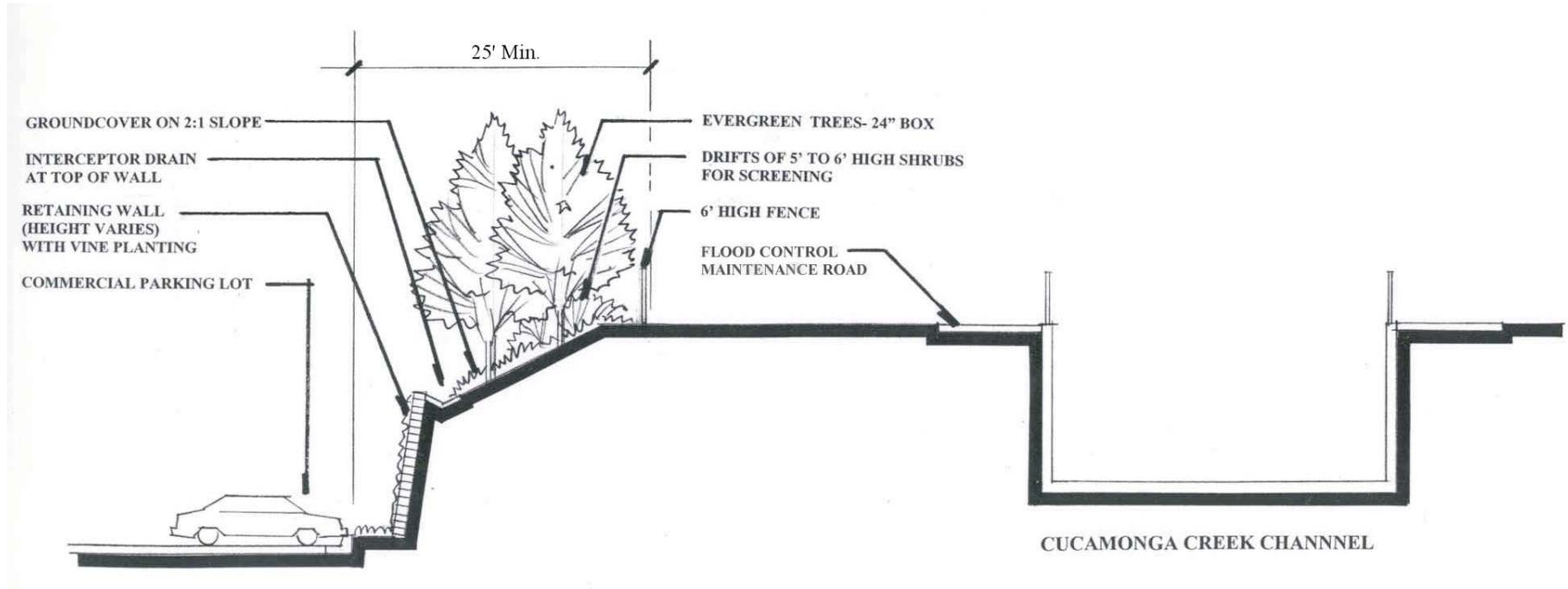
*Non-retaining condition*

*The Colonies at San Antonio Specific Plan*

Figure 9.37

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# LANDSCAPE BUFFER ALONG CUCAMONGA CREEK CHANNEL



*The Colonies at San Antonio Specific Plan*

Figure 9.38

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## **9.7 LANDSCAPING PLAN DEVELOPMENT STANDARDS**

The landscaping design for the Colonies at San Antonio will include plant material choices and irrigation design that are water efficient. Thematic continuity will be achieved in the landscaping programs developed for residential, commercial and mixed use, open-space areas, and the flood control corridor. Each land use component will be planted so that it transitions to adjacent land use areas.

Project entry statements will be designed with landscaping and architectural treatments that project a high quality image for the mixed-used development.

Any private common open space that may be located within each planning area, will be devoted to passive open space uses.

All detailed landscaping programs for planning areas and roadways will be prepared by a qualified landscape architect for review by the Community Development Director. The landscape improvement plans for each Planning Area shall be approved by the City in conjunction with, and at the same time as, City approval of the appropriate tract.

Landscaping within commercial and mixed use Planning Areas shall encompass a minimum of 10 percent of the square footage of the Planning Area. Required parking lot landscaping and landscaping within required setback areas shall count toward this requirement.

The improvement plans for each Planning Area shall include, as a minimum, the following.

- Final grading plan of Planning Area drawn to scale.
- Irrigation plans of all landscaping areas within each Planning Area. All irrigation plans shall be drawn to scale and certified by a landscape architect.
- A planting plan of the Planning Area that specifies landscape planting to include locations, type, size, quantity and installation methods.
- A hardscaping plan with location and finishes.
- Fence and wall treatment plans.

At the time of recordation of any tentative subdivision which contains a common private greenbelt or open-space areas, the subdivision shall have those common areas conveyed to the master homeowners association, an individual homeowners association or appropriate public maintenance agency, which shall then be responsible for maintenance.

The developer of each Planning Area shall be responsible for the maintenance of all slope plantings, common landscaped areas, and irrigation systems within the Planning Area until such time as these operations are the responsibility of other parties.

### **9.7.1 Parking Lot Landscaping**

The following standards shall be applied to parking lot landscaping in all commercial and mixed use, public facilities, and luxury attached housing Planning Areas.

- Parking should be buffered from adjacent public plazas, streets, and driveways with a combination of earth berms and landscape or with a 3-foot high landscape hedge.
- Provide trees within the vehicular use areas at a ratio of one tree for every 14 parking stalls .

- Parking areas should be designed in a manner which links the building to the street sidewalk system, as an extension of the pedestrian environment. This can be accomplished by using design features such as walkways with enhanced paving, trellis structures, and/or landscape treatments.
- All storage and refuse structures shall be provided with a solid wall of sufficient height to screen it from view, and landscaping.
- Planter areas shall have a minimum inside width of 3 feet and an average inside width of 7 feet, bounded on the outside by a concrete curb (or its equivalent) having a minimum height of 6 inches.
- Provide an end cap planter island at the end of all parking rows. End caps shall be a minimum 6-foot wide including curbs. Planters shall have a minimum length equal to the longest abutting parking stall, inclusive of curbing.
- Diamond planters located within parking rows shall have a minimum dimension of 5 feet by 5 feet interior clear area.
- Commercial loading docks that are visible from I-210, a public street, or the main parking area of a commercial center shall be screened from view using an 8-foot wall that is compatible with the architecture, materials, and color of the main building. The wall shall be visually softened through the use of landscaping.

### **9.7.2 Planting**

The following standards shall apply toward plant selection and placement along streetscapes and within all commercial and mixed use and public facilities Planning Areas.

- Planting and irrigation design shall be water efficient.
- All required trees shall be a minimum 15-gallon in size; 20 percent of the trees provided shall be minimum 24-inch box in size.
- Linear root barriers shall be installed where trees occur within 5 feet of any concrete flatwork, curb and gutter, utilities, structures, etc.
- Tree species are to be selected from the commercial palette and for their ability to thrive in the area's planting conditions.
- All groundcover at 1-gallon size shall be triangularly spaced at a dimension sufficient to fill the area within 24 months.
- Flatted material shall be triangularly spaced at a maximum of 12 inches on center unless variety allows for further spacing.
- All slope areas adjacent to streets shall have a gradient not exceeding the City of Upland's engineering standards.

### **9.7.3 Commercial and Mixed Use Site Furniture**

Site furniture, such as benches, waste receptacles, drinking fountains, bollards, tree grates, planters, and architectural lighting shall be provided, as appropriate, within Planning Areas 17, 18, 19, 20, and 21 to provide scale and conveniences for the pedestrian. These features should be located in plaza areas and in key areas along the pedestrian linkages. The design of street furniture shall reflect the style and characteristics of the building architecture, and shall be vandal-resistant.

#### **9.7.4 Residential Walls and Fences**

Walls and fences are to be designed as an integral component and extension of the building design and the surrounding landscape (Figures 9.39 and 9.40). Walls along the perimeter of residential areas that border on the open space may be designed, if desired, to allow views of the open space from private yards. Figure 9.41 illustrates typical locations for the various types of wall and fence designs.

Prohibited wall and fencing materials include barbed wire, wire, electrically charged fences, plastic materials, corrugated metal, chain link, and grapestake.

- Front yard walls or fencing, and retaining walls that are visible from the street, shall be constructed of materials that are designed and colored to be compatible with the adjacent architecture.
- Walls and fences shall be designed to preserve open space views. Tubular steel should be used where views are present (see Figure 9.40).
- Walls along perimeters of planning areas shall be designed as noted in Figure 9.39.
- Wood fencing may be used between residential lots (see Figure 9.40).

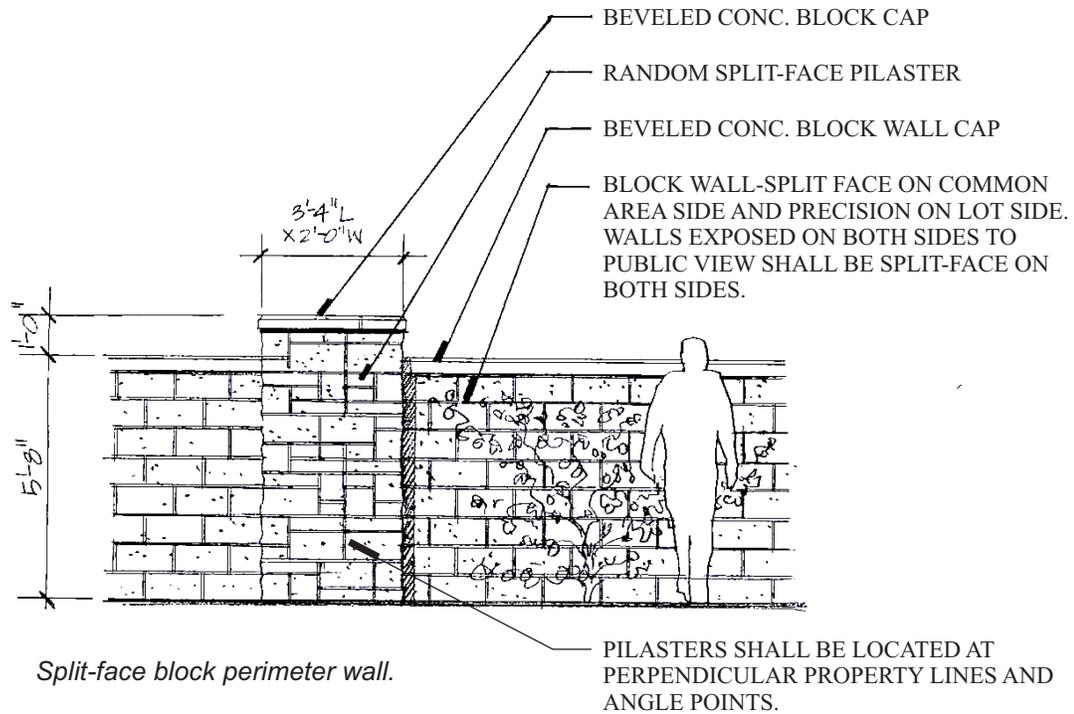
#### **9.7.5 Buffers**

Particular care will be taken to design commercial and mixed use developments that are compatible with surrounding residential areas. Grade separations, open space areas, landscaping, and walls and fences shall be used as buffers. In addition, all storage and refuse areas shall be screened from view by 6-foot high opaque walls or fences. Earth berms, hedges, or walls shall be used for segregating vehicular and pedestrian circulation patterns, as well as for visually screening parking lots and other undesirable views from nearby streets, public plazas, and residential areas.

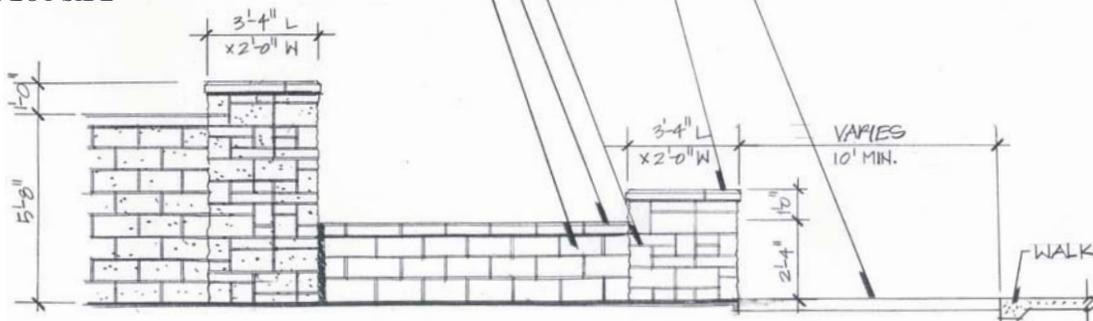
#### **9.7.6 Paving**

Paved areas throughout the Colonies at San Antonio should be designed as an integral component of the surrounding architecture. Decorative paving material is encouraged in high visibility and high use areas such as in public plazas and at key vehicular entry drives. Decorative paving materials may include colored textured concrete or interlocking concrete pavers.

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CONCRETE MOW STRIP TO SEPARATE MAINTENANCE RESPONSIBILITIES BETWEEN COMMON AND PRIVATE AREA LANDSCAPE, WHERE OCCURS  
 BEVELED CONC. BLOCK WALL CAP  
 RANDOM SPLIT-FACE PILASTER  
 BEVELED CONC. BLOCK WALL CAP  
 BLOCK WALL STEM WALL-SPLIT FACE ON BOTH COMMON AREA SIDE AND ON LOT SIDE



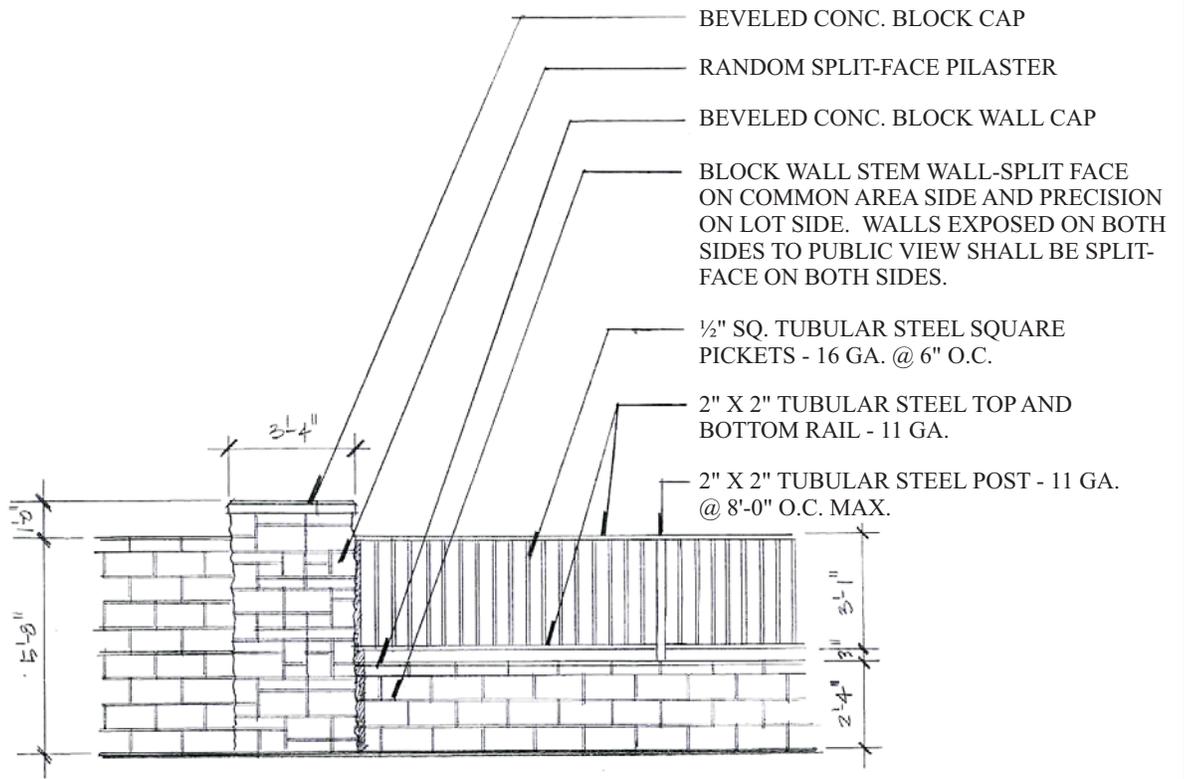
Low split-face block wall.

# RESIDENTIAL WALLS AND FENCES 1

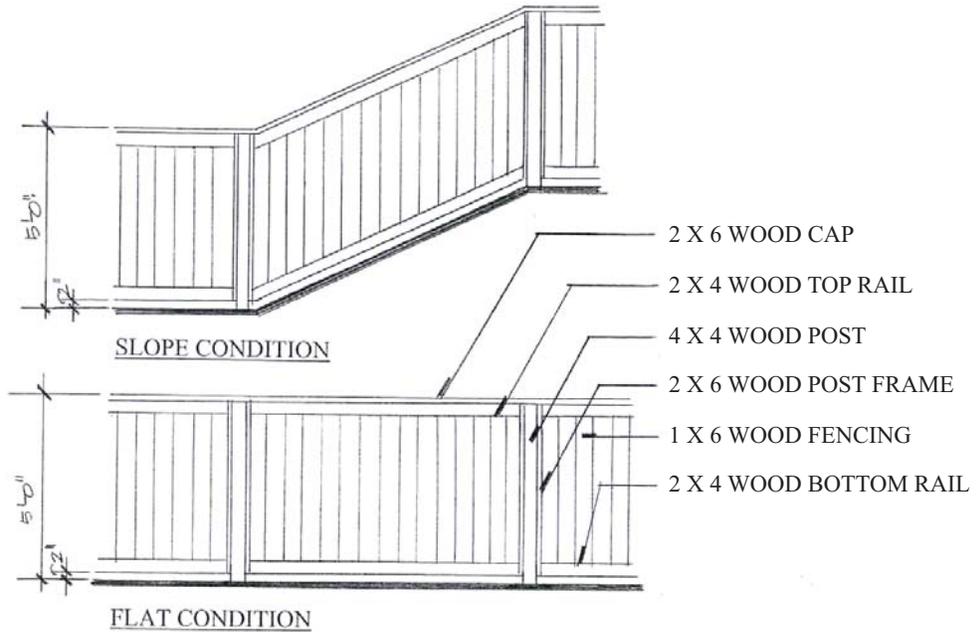
*The Colonies at San Antonio Specific Plan*

Figure 9.39

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Split-face block wall and view fence.



Wood fencing between residential lots.

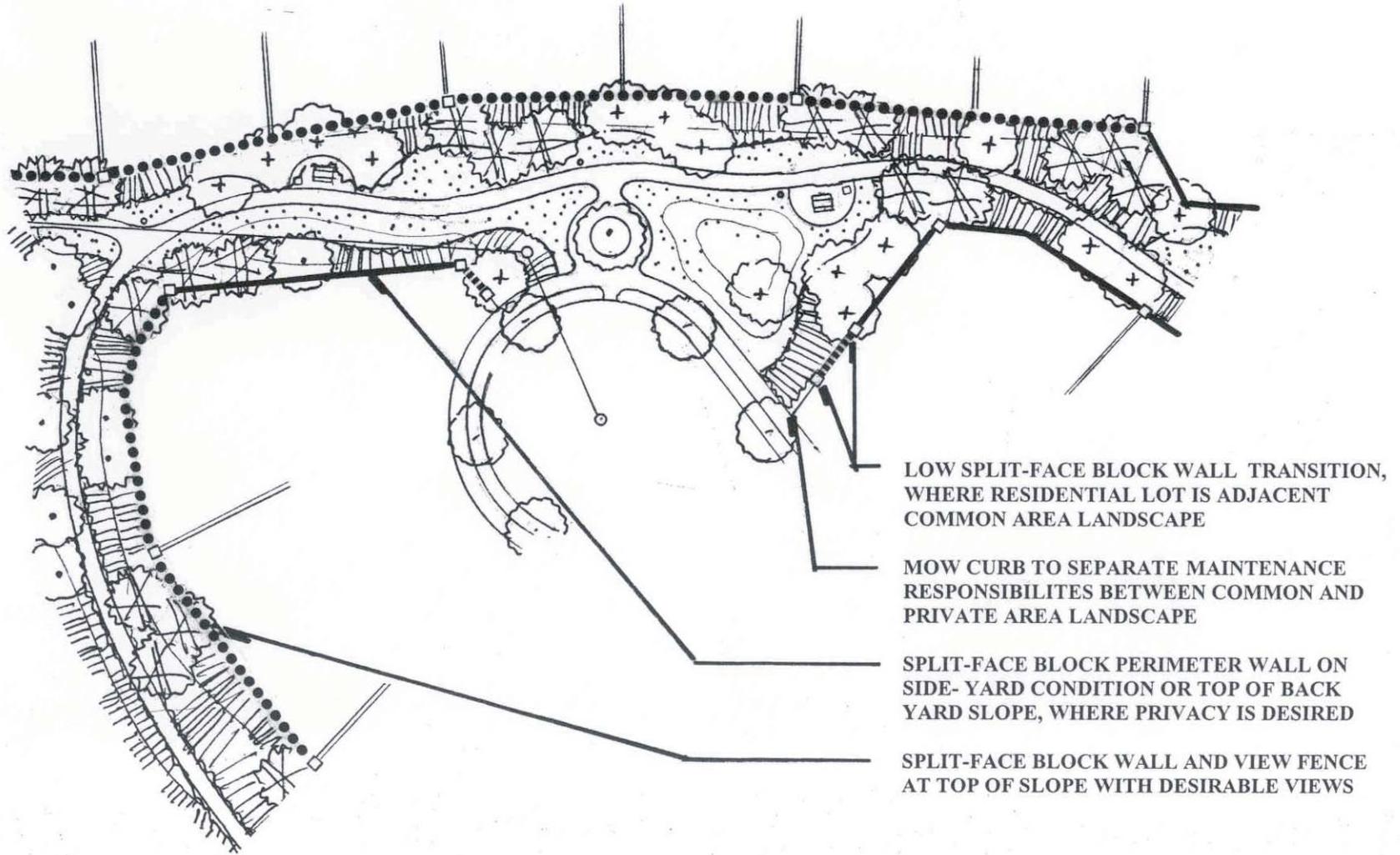
## RESIDENTIAL WALLS AND FENCES 2

*The Colonies at San Antonio Specific Plan*

Figure 9.40

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## RESIDENTIAL WALL AND FENCE TYPICAL LOCATIONS



*The Colonies at San Antonio Specific Plan*

Figure 9.41

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## **10.0 DEVELOPMENT AND INFRASTRUCTURE PHASING**

### **10.1 PURPOSE AND INTENT**

Because the build out of large-scale planned communities, such as the Colonies at San Antonio, occurs over a number of years. These communities are typically designed to allow their master developer the ability to offer individual portions of the community to merchant homebuilders and commercial developers in response to changing market conditions. Thus, the project's master developer is responsible for constructing the project's backbone infrastructure and community amenities, while merchant builders construct and sell homes within individual residential neighborhoods, and commercial developers construct and manage the community's commercial planning areas.

The order in which merchant builders and commercial developers will want to purchase and develop individual portions of a planned community is difficult to predict. As a result, it is necessary to provide flexibility regarding the order in which individual planning areas within a planned community will be developed. However, with such flexibility comes the need to ensure that infrastructure, as well as community facilities and amenities are constructed and made available in a timely manner. It is the purpose of this chapter to set forth a development phasing program for the Colonies at San Antonio that provides sufficient flexibility regarding the order in which individual planning areas are developed, while ensuring the ongoing adequacy of community infrastructure, facilities, and amenities.

### **10.2 CONCEPTUAL DEVELOPMENT PROGRAM**

The Colonies at San Antonio Specific Plan is anticipated to be developed in three general "phases," which will overlap in time (see Figure 10.1). Because utilities are generally available along the eastern boundary of the Specific Plan area, the three most easterly residential neighborhoods (Planning Areas 1, 2, and 3) encompass the first phase of development. The second phase of development consists of the residential neighborhoods clustered along Colonies Parkway and the central open space area, and the commercial planning areas along the I-210 freeway.

### **10.3 DEVELOPMENT AND INFRASTRUCTURE SEQUENCING**

#### **10.3.1 Sequencing of Land Development**

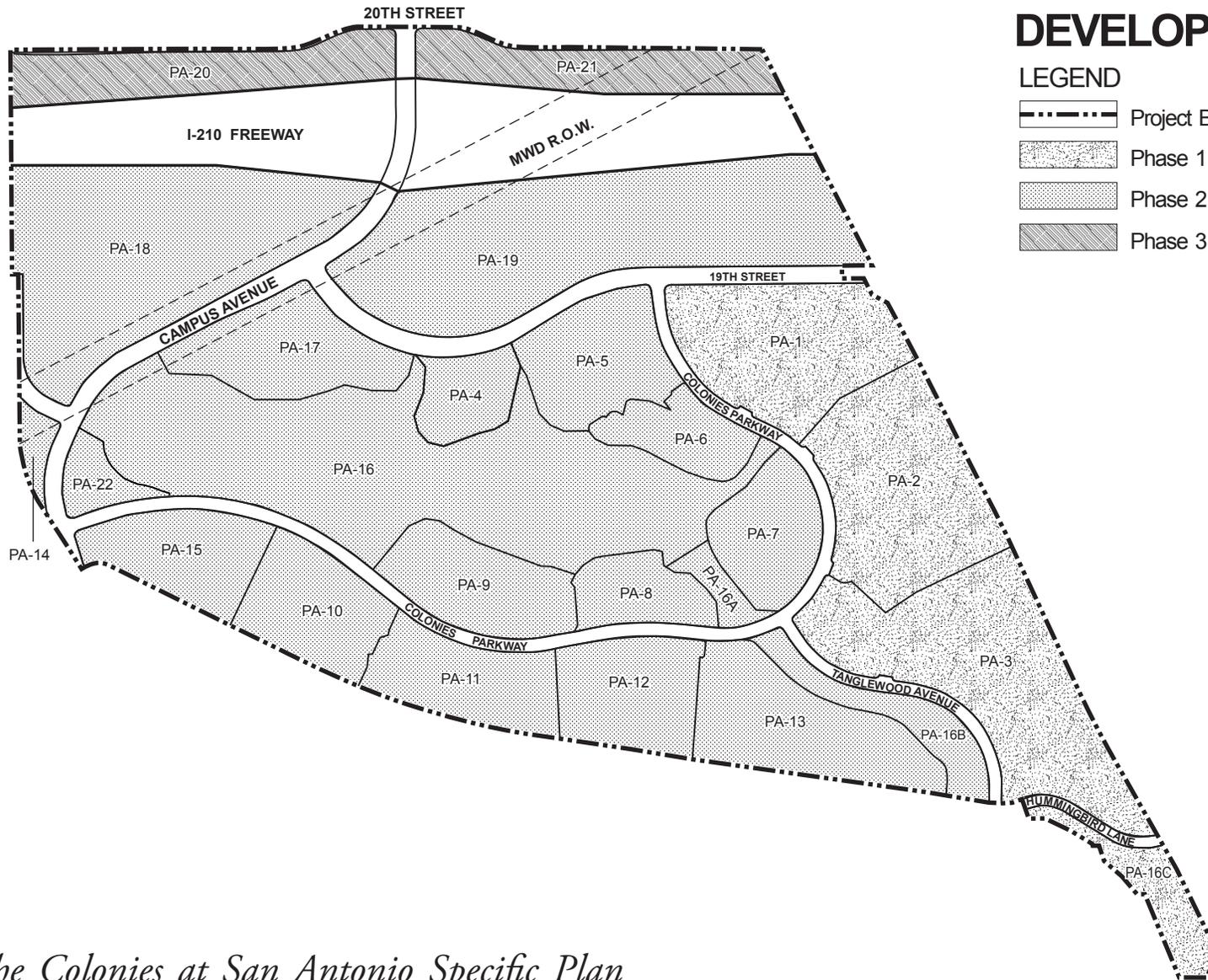
It is the intent of the Colonies at San Antonio Specific Plan to provide great flexibility in the precise order that individual planning areas may be developed. Subject to the provisions of this Chapter, individual Planning Areas within the Colonies at San Antonio Specific Plan may be constructed in any order, and individual Planning Areas may be developed in two or more increments. Thus, while the Specific Plan identifies three general development phases, it does *not* set forth any requirement that all or any portion of the first development phase be completed prior to initiating or completing development of Phase 2 or 3.

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# CONCEPTUAL DEVELOPMENT PHASING

## LEGEND

- Project Boundary
- Phase 1
- Phase 2
- Phase 3



*The Colonies at San Antonio Specific Plan*

↑  
N Not to Scale  
Source: LSA Associates, Inc.

Figure 10.1

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### **10.3.2 Sequencing of Water, Sewer, Drainage, and Street Improvements**

The planned development of the Colonies at San Antonio is based on the specific anticipation that each “phase” of project development will occur as a series of individual development increments, consisting of all or portions of one or more individual planning areas, and that the infrastructure associated with each phase of development will be constructed in multiple increments prior to completion of the phase.

To provide the requisite flexibility for successful project development, while ensuring ongoing adequacy of project infrastructure, the Specific Plan sets forth a precise delineation of the infrastructure requirements for each Planning Area in Table 10.A. Table 10.A identifies each of the backbone infrastructure facilities (roadway, water, sewer, drainage, etc.) that will serve the project site<sup>1</sup>, and defines which specific components of the project’s roadway, water, sewer, and drainage facility systems are needed to support development of each Planning Area. Table 10.A also defines the required timing for improving the Specific Plan’s central open space area. Within Table 10.A, each backbone infrastructure component needed to support development of each Planning Area is identified by a “T” in the Table. Thus, as part of the development of any individual planning area, every backbone infrastructure component identified by a “T” in the Table for that Planning Area must be constructed.

As standard conditions of approval for development of any planning area within the Colonies at San Antonio, the following requirements will apply.

- Each of the prior backbone infrastructure components identified in Table 10.A with a “T” shall be constructed and operational prior to the first occupancy of structure within the affected Planning Area.
- Unless a specific infrastructure and development phasing plan is approved by the City to permit incremental development of an individual Planning Area, all of the internal roadway, water, sewer, and drainage facilities serving an individual Planning Area shall be constructed and completed prior to the first occupancy of that Planning Area.
- Prior to delivery of combustible materials to the site, base roadways and community water systems shall be available.

### **10.3.3 Incremental Development of Individual Planning Areas**

It is intended that each residential, commercial, or public facility Planning Area be planned and designed as a cohesive neighborhood unit, commercial center, or community facility, respectively. Thus, tentative subdivision maps may not be submitted, and will not be approved covering only a portion of a planning area. Similarly, site plans for Planning Areas 17, 18, and 19 may not be submitted, and will not be approved covering only a portion of a planning area. Site plans within Planning Areas 20 and 21 may be submitted for a portion of the Planning Area, provided that a conceptual development plan for the entire Planning Area is submitted concurrent with the site plan application.

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<sup>1</sup> “Backbone facilities” are defined as infrastructure components that serve more than one Planning Area.

**Table 10.A - Backbone Infrastructure Phasing Matrix**

Backbone Infrastructure Components	Planning Area																					
	Phase 1			Phase 2																Phase 3		
	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22
<b>Roadway Improvements</b>																						
<i>Signalize:</i>																						
I-210 Interchange																			•	•	•	•
Campus Avenue/19 <sup>th</sup> Street (two locations)				•															•	•	•	
19 <sup>th</sup> Street/Colonies Parkway				•	•	•	•	•	•	•	•	•	•	•					•	•	•	
Colonies Parkway/Campus Avenue								•	•	•	•	•	•			•						•
Commercial Center Entry (full turning movements)																			•	•	•	
<i>Construct Colonies Parkway From:</i>																						
19 <sup>th</sup> Street to Tanglewood Avenue	•	•	•	•	•	•	•	•	•	•	•	•	•	•					•	•	•	•
Tanglewood Avenue to Campus Avenue								•	•	•	•	•	•			•						•
<i>Construct Campus Avenue From:</i>																						
I-210 Interchange to existing Campus Avenue				•															•	•	•	
<i>Construct 19<sup>th</sup> Street From:</i>																						
New 19 <sup>th</sup> St. to old 19 <sup>th</sup> St. along westerly Specific Plan Boundary				•															•	•	•	
Colonies Pkwy. to eastern project boundary	•	•	•	•	•	•	•	•	•	•	•	•	•	•					•	•	•	•
Colonies Pkwy. to realigned Campus Avenue (realignment)				•															•	•	•	
<i>Construct 20<sup>th</sup> Street From:</i>																						
Campus Avenue to west SP boundary																					•	
Campus Avenue to east SP boundary																					•	
<i>Construct Tanglewood Avenue</i>	•	•	•	•	•	•	•	•	•	•	•	•	•						•	•	•	•
<b>Drainage Improvements</b>																						
<i>Central Open Space Area</i>																						
Center open space area, basins, appurtenances, and connection to Cucamonga Creek. <i>Initial improvements including grading.</i>				•	•	•	•	•	•	•	•	•	•	•	•							•

Backbone Infrastructure Components	Planning Area																					
	Phase 1			Phase 2															Phase 3			
	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22
<i>hydroseeding, construction of passive spillway, and connections to Cucamonga Creek (Planning Areas 16, 16A, 16B)</i>																						
<i>Central open space area: increase height of outlet, construct gated valve outlet<sup>1</sup></i>																						
<i>Provision of wetland, riparian, and upland landscaping per the provisions of the Specific Plan and Department of Fish and Game 1603 permit<sup>2</sup></i>																						
<i>Colonies Parkway:</i>																						
19 <sup>th</sup> Street to Tanglewood Avenue	•	•	•		•	•	•													•		
Tanglewood Avenue to Campus Avenue								•	•													
Adjacent to PA 12 and PA 13, including line though PA 13								•	•				•									
Adjacent to PA 9 and 11									•		•											
<i>19<sup>th</sup> Street:</i>																						
West of Colonies Parkway					•															•		
<i>20<sup>th</sup> Street From:</i>																						
Campus Avenue to west SP boundary																				•		
Campus Avenue to east SP boundary																					•	
<i>Tanglewood Avenue, Colonies Parkway to Hummingbird Lane</i>	•	•	•				•															
<i>42" to 60" line along MWD right-of-way</i>																			•		•	•
<i>Connection from 19" Street storm drain into Basin A</i>				•	•	•		•	•	•	•	•	•	•	•	•						•
<i>Storm drain along southerly SP boundary, west of Tanglewood Ave.</i>								•	•	•	•	•	•		•							•
<i>36" line from PA 9 through PA 11</i>									•		•											

Backbone Infrastructure Components	Planning Area																					
	Phase 1			Phase 2															Phase 3			
	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22
<i>24" to 36" line through PA 5 and PA 6</i>					•	•																
<i>24" line from PA 10 through PA 11</i>										•	•											
<b>Water Facilities</b>																						
<i>Colonies Parkway From:</i>																						
19 <sup>th</sup> Street to Tanglewood Avenue	•	•	•	•	•	•	•												•	•	•	
Tanglewood Avenue to Campus Avenue									•	•	•	•	•	•		•						•
<i>Campus Avenue From:</i>																						
Existing Campus Avenue to southerly 19 <sup>th</sup> Street intersection									•	•	•	•	•	•		•						•
I-210 Interchange to existing southerly 19 <sup>th</sup> Street intersection				•	•	•	•	•	•	•	•	•	•		•		•	•	•	•	•	•
<i>19<sup>th</sup> Street From:</i>																						
Realigned Campus Avenue to existing 19 <sup>th</sup> Street									•	•	•	•	•	•		•			•			•
Colonies Parkway to eastern project boundary	•	•	•	•	•	•	•												•	•	•	
Colonies Parkway to realigned Campus Avenue				•	•	•	•												•	•	•	
<i>20<sup>th</sup> Street From:</i>																						
Campus Avenue to west SP boundary																					•	
Campus Avenue to east SP boundary																						•
<i>Tanglewood Avenue, Colonies Parkway to Hummingbird Lane</i>	•	•	•	•	•	•	•	•	•	•	•	•	•	•		•	•	•	•	•	•	•
<b>Sewer Facilities</b>																						
<i>Offsite Sewer Connections</i>																						
Grove Avenue between 15 <sup>th</sup> Street basins and 14 <sup>th</sup> Street				•	•	•	•	•	•	•	•	•	•		•		•		•			•
Memorial Park Road, San Bernardino Road, 13 <sup>th</sup> Avenue, Arrow Highway				•	•	•	•	•	•	•	•	•	•		•		•	•	•	•	•	•
Tanglewood Avenue from Baseline Road to 15 <sup>th</sup> Street	•	•	•	•	•	•	•	•	•	•	•	•	•		•		•	•	•	•	•	•
<i>Colonies Parkway From:</i>																						
19 <sup>th</sup> Street to Tanglewood Avenue	•	•	•	•	•	•	•												•		•	

Backbone Infrastructure Components	Planning Area																					
	Phase 1			Phase 2															Phase 3			
	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22
Tanglewood Avenue to Campus Avenue								•	•													
<i>Campus Avenue From:</i>																						
I-210 Interchange to existing Campus Avenue																			•		•	•
<i>19<sup>th</sup> Street From:</i>																						
Colonies Parkway, west				•															•		•	
<i>20<sup>th</sup> Street From:</i>																						
Campus Avenue to west SP boundary																						
Campus Avenue to east SP boundary																						
<i>Tanglewood Avenue, Colonies Parkway to Hummingbird Lane</i>	•	•	•	•	•	•	•												•		•	
<i>Line along southerly Specific Plan boundary</i>								•	•	•	•	•	•				•					•
<i>Common line within PA 6, serving PA's 5, 6</i>					•	•																
<i>Common line within PA 11, serving PA 9</i>									•		•											
<i>Common line within PA 11, serving PA's 10 and 11</i>										•	•											
<i>Common line within PA 12, serving PA's 9 and 12</i>									•			•										
<i>Common line within PA 13, serving PA's 8 and 13</i>								•					•									
Trails																						
<i>Trails, overlooks, and service roads within Planning Areas 16, 16A, 16B, 16C</i>																			•			
<i>Trail between Planning Areas 1 and 2</i>	•	•																				
<i>Trail between Planning Areas 2 and 3</i>		•	•																			
<i>Trail between Planning Areas 5 and 6</i>					•	•																
<i>Trail between Planning Areas 8 and 9</i>								•	•													
<i>Trail along the south side of</i>																						
Planning Area 10										•												

Backbone Infrastructure Components	Planning Area																					
	Phase 1			Phase 2																Phase 3		
	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22
Planning Area 11											•											
Planning Area 12												•										
Planning Area 13													•									

Notes:

1. The timing of construction of gated valve outlet to the central basin area is contingent upon County Flood Control District and State approvals. Applications for such approval shall be filed within three years of initiating grading of the central basin area.
2. Wetland riparian and upland native landscaping shall be installed in accordance with the provisions of the Specific Plan and California Department of Fish and Game 1603 Stream Alteration Agreement. Waste Discharge Requirements of the Regional Water Quality Control Board, and the Colonies at San Antonio Development Agreement, as amended. Except as may be extended by events of force majeure, landscaping shall be commenced and completed no later than specified in the Colonies at San Antonio Development Agreement, as amended on December 22, 2003.

While individual Planning Areas must be designed and planned as a cohesive unit, actual development within individual residential commercial, and community facilities Planning Areas may subject to approval of a phasing plan by the City. Such phasing plans must provide for the phasing of land development and internal infrastructure within the planning area in a manner that ensures adequate infrastructure from initial occupancy to final build out and that avoids routing of construction traffic along streets fronted by occupied dwelling units. In order for an individual Planning Area to be constructed in two or more increments, a development phasing plan is to be provided to the City for review and approval in accordance with the provisions of Table 10.B.

**Table 10.B - Requirements for Phasing Plans**

<b>Planning Area Land Use</b>	<b>City Review and Approval Shall Occur Prior to:</b>	<b>Approval Authority</b>
Single Family Residential	Recordation of any tract map for the first increment of development within the residential Planning Area.	Community Development Director
Luxury Attached Housing	Recordation of any tract map or submittal of building permit applications for the first increment of development within the LAH Planning Area.	Community Development Director
Commercial	Submittal of building permit applications for the first increment of development within the commercial Planning Area.	Community Development Director
Public Facilities	Not required.	NA

### 10.3.4 General Development and Infrastructure Sequencing Requirements

**a. “Roadway Improvements” Defined.** Roadway improvements (other than signalization) outlined in Table 10.A include construction of roadway base and pavement sections; medians (where required); curbs, gutters, sidewalks, street lighting, and landscaping within the roadway right-of-way along both sides of the street. Landscape improvements within “lettered lots” adjacent to the public right-of-way shall be constructed concurrent with development of the adjacent Planning Area.

**b. Screening of Undeveloped Planning Areas.** Following construction of backbone roadways, undeveloped Planning Areas shall be screened from view from the public roadway by a landscaped berm or opaque fencing.

**c. Screening of Undeveloped Portions of Residential Planning Areas.** Where a residential Planning Area (other than PA 4) is to be developed in multiple increments, a perimeter decorative block wall shall be constructed around the boundary of the entire Planning Area prior to the first occupancy within that Planning Area.

**d. Screening of Undeveloped Portions of Commercial and Luxury Attached Housing Planning Areas.** If a phasing plan is requested for Planning Areas 4, or 17-21, such request shall include a

plan for screening undeveloped portions of the Planning Area from view from public roadways until development of that Planning Area has been completed.

**e. Sequencing of Intersection Monumentation.** Monumentation for intersections identified in Figure 9.1 shall be constructed as follows.

**(1) Primary Intersections.** Intersection monumentation for the 19<sup>th</sup> Street/Colonies Parkway intersection shall be constructed as part of the proposed realignment and roadway improvements for 19<sup>th</sup> Street. Intersection monumentation for the Campus Avenue/Colonies Parkway intersection shall be constructed as part of the proposed realignment and roadway improvements for Campus Avenue.

**(2) Secondary Intersections.** Intersection monumentation for the westerly intersection of 19<sup>th</sup> Street and Campus Avenue shall be constructed as part of the roadway improvements for the realignment of 19<sup>th</sup> Street. Intersection monumentation for the easterly quadrants of the Colonies Parkway/Tanglewood Avenue intersection shall be constructed as part of the roadway improvements for Colonies Parkway and Tanglewood Avenue.

**(3) Commercial Intersections.** Monumentation for commercial intersections shall be constructed concurrent with development of the adjacent commercial Planning Area.

**(4) Neighborhood Intersections.** Monumentation for neighborhood intersections shall be constructed concurrent with development of the adjacent Planning Area.

## **11.0 IMPLEMENTATION**

### **11.1 GENERAL IMPLEMENTATION PROVISIONS**

The following general requirements shall apply to all developments within the Colonies at San Antonio Specific Plan area.

#### **11.1.1 General Plan Consistency**

Implementation of the Colonies at San Antonio Specific Plan is intended to carry out the goals and policies contained in the City of Upland General Plan, as amended, in an orderly and attractive fashion. Development within the Colonies at San Antonio Specific Plan area shall, therefore, be consistent with the provisions of the Upland General Plan.

#### **11.1.2 Relationship Between Specific Plan Provisions and the Upland Municipal Code**

- a. Development regulations and requirements contained in this document will supplement or replace those of the Upland Municipal Code as they might otherwise apply to lands within the Colonies at San Antonio Specific Plan area. Any regulations or requirements not specifically covered herein shall be subject to the regulations and requirements of the Upland Municipal Code.
- b. Unless otherwise specifically approved as part of this Specific Plan, all off-site improvements under the control of the City shall be subject to the City of Upland regulations and requirements in effect at the time improvement plans are submitted. Other improvements not under the control of the City (e.g., freeway interchange improvements subject to Caltrans authority, electrical lines under the control of SCE, etc.), shall be subject to the regulations and requirements of the responsible agency.
- c. If any provision of this document conflicts with the regulations or requirements of the Upland Municipal Code, the provisions of this document shall take precedence.

#### **11.1.3 Conformance with Uniform Building and Fire Codes**

All construction within the Colonies at San Antonio Specific Plan shall be in compliance with the Uniform Building Code, Uniform Fire Code, and all other ordinances adopted by the City pertaining to construction and safety features.

#### **11.1.4 Implementation of EIR Mitigation Measures**

- a. The mitigation measures contained in the certified Final EIR for the Colonies at San Antonio Specific Plan have been incorporated into this document, and shall, as appropriate, be conditions of approval on all development within the Specific Plan area as determined by the Upland Administrative

Committee pursuant to the provisions of CEQA and the City's rules to implement CEQA. Appendix of this document includes a listing of the mitigation measures contained in the EIR.

- b. Environmental assessment requirements for individual projects within the Specific Plan area shall focus on determining the extent to which the impacts of such individual development projects were previously addressed in the Final EIR for the Colonies at San Antonio Specific Plan. To the extent that the impacts of individual developments within the Colonies at San Antonio Specific Plan are adequately addressed in the Final EIR, subsequent environmental analysis will not be required.

### **11.1.5 Master Plans for Traffic, Water, Sewer, and Drainage**

Specific requirements for infrastructure improvements are determined by the Master Plans prepared for the Colonies at San Antonio Specific Plan area. The conclusions of the Master Plans have been included as part of this Specific Plan. The full texts of the Master Plans are available under separate cover. Master Plans may be amended over time to ensure the availability of adequate infrastructure and services to the project site, subject to approval of the City Engineer. Modifications to project infrastructure undertaken pursuant to Master Plan amendments approved by the City Engineer are not considered to constitute amendments to the Colonies at San Antonio Specific Plan.

### **11.1.6 Severability**

If any term, provision, condition, requirement, or portion thereof of this Specific Plan is for any reason held invalid, unenforceable, or unconstitutional, the remainder of this Specific Plan or the application of such term, provision, condition, requirement, or portion thereof to circumstances other than those in which it is held to be invalid, unenforceable, or unconstitutional, shall not be affected thereby; and each other term, provision, condition, requirement, or portion thereof shall be held valid and enforceable to the fullest extent permitted by law.

### **11.1.7 Definitions**

Words, phrases, and terms not specifically defined within this Specific Plan shall have the same definition and meaning as provided in the City of Upland Municipal Code or, if not defined in the Municipal Code, in a dictionary in common use.

## **11.2 SPECIFIC PLAN ADMINISTRATION**

Preceding chapters of the Colonies at San Antonio Specific Plan have established permitted land uses, building intensities, circulation plans, master plans for services, and other development standards and design guidelines. Subsequent to approval of the Specific Plan, implementation will entail the review of various individual development projects. To ensure compliance with all applicable development standards contained in the Colonies at San Antonio Specific Plan, applicable provisions of the Upland Municipal Code, and such applicable development standards as may be adopted by the City Council from time to time, all proposed development within the Specific Plan area shall be subject to review and approval by the City of Upland pursuant to the review procedures discussed below.

### **11.2.1 Land Divisions**

Applications for, as well as processing and approval of proposed divisions of land within the Colonies at San Antonio Specific Plan shall comply with the provisions of the California Subdivision Map Act and the subdivision ordinance of the City of Upland.

### **11.2.2 Site Development Review**

The site development review processes set forth below recognize the level of review by the City of Upland and other agencies that went into the original approval of, and subsequent amendments to, the Colonies at San Antonio Specific Plan. The following review processes also recognize the City's obligation to ensure that proposed non-residential development meets the requirements of the Colonies at San Antonio Specific Plan, applicable provisions of Title 17 of the Upland Municipal Code, and such other applicable development standards as may be adopted by the Upland City Council from time to time.

#### **a. Conditional Use Permits**

- (1) Approval of a Conditional Use Permit by the Planning Commission shall be required for the establishment of all new uses identified in Table 7.B of this Specific Plan as requiring a Conditional Use Permit.
- (2) Once established, expansions of uses that are subject to a Conditional Use Permit may be accommodated as follows.
  - (a) Expansions of less than 25 percent of the approved gross floor area for the conditional use may be approved by the Community Development Director.
  - (b) Expansions of 25 percent or more of the approved gross floor area for the conditional use shall require approval of a modification of the approved Conditional Use Permit by the Administrative Committee.

#### **b. Site Plans for Permitted Commercial and Non-Residential Uses**

- (1) Site Plans for Planning Areas 17, 18, 19, 20, 21, and 22 shall require review and approval by the Administrative Committee, as stated in Chapter 17.16.090 et. seq. of the Upland Municipal Code. The Administrative Committee shall receive, review, and make determinations on all site plans.
- (2) As set forth below, all new construction of non-residential uses outside of Planning Areas 17 through 22 (that are identified in Table 7.B as permitted without the requirement of a Conditional Use Permit) shall require approval of a Site Plan by the Administrative Committee.
- (3) Expansion of existing commercial uses in Planning Areas 17 through 22 that results in a net increase in total building square footage of more than fifteen percent of the building envelope or changes to a site plan beyond the building envelope established pursuant to Section 11.2.2f of this Specific Plan, shall require approval by the Administrative Committee, unless otherwise specified herein.
- (4) Expansion of existing non-residential uses outside of Planning Areas 17 through 22 that results in a net increase in total building square footage of more than fifteen percent of the building envelope or changes to a site plan beyond the building envelope established pursuant to Section 11.2.2f of this Specific Plan, shall require approval by the Community Development Director, unless otherwise specified herein.
- (5) Site Plan review as specified above shall be subject to the same procedures (including, procedures for notice, findings, and appeal) as the Administrative Committee Review procedures set forth in Chapter 17.16.090 et. seq. of the Upland Municipal Code.
- (6) Graphics proposed for the enhanced pedestrian crossing between Planning Areas 4 and 19 are conceptual. Design Review Board approval of the final pedestrian crossing design will be required

prior to site plan approval.

**c. Multifamily and Mixed Use Site Plans for Planning Areas 4, 15, and 22**

- (1) Site Plans for Planning Areas 4, 15, and 22 (if 4 is developed as MU and 22 is developed as RF; if not, NC policies shall be followed) shall require review and approval by the Administrative Committee, as stated in Chapter 17.16.090 et. seq. of the Upland Municipal Code. The Administrative Committee shall receive, review, and make determinations on all site plans. After a determination concerning Site Plan approval has been reached, the Site Plan will be forwarded to the Planning Commission for approval.
- (2) Planning Areas 4, 15, and 22 shall each require approval by the Planning Commission of a Site Plan. In Planning Areas 4, 15, and 22, for any uses in which detailed building footprints and elevations are not available at the time of a Site Plan application to the Planning Commission, the buildings shall be shown conceptually and shall require further future review by the City in compliance with subparagraph 3, below.
- (3) If a use in Planning Areas 4, 15, or 22 is only conceptually reviewed by the Planning Commission according to subparagraph 1, above, it shall *also* require the approval of a Site Plan (including detailed building footprint and building elevations) by the Administrative Committee (assuming that the use is identified in Table 7.B as permitted without the requirement of a Conditional Use Permit).
- (4) Review of Site Plans by the Planning Commission as specified in subparagraph 1, above, shall be subject to the same procedures (including procedures for notice, findings, and appeal) as for other applications under the approval authority of the Planning Commission, as set forth in Chapter 17.16.010, Development Application and Review Procedures, of the Upland Municipal Code.
- (5) Site Plan review as specified in subparagraphs 3 and 4, above, shall be subject to the same procedures (including, procedures for notice, findings, and appeal) as the Administrative Committee Review procedures set forth in Section 17.16.090 et. seq. of the Upland Municipal Code.

**d. Filing Requirements**

- (1) Applications for Conditional Use Permits or Site Plans may be initiated by either one or more of the recorded property owners of the site for which the application is being filed, or by an authorized agent thereof.
- (2) Applications shall be submitted to the Community Development Department in the manner prescribed by the Community Development Director.
  - (a) Applications shall be submitted to the Community Development Department in conformance with the Community Development Department application schedules.
  - (b) The Community Development Director shall prescribe the form in which applications are made for review of Conditional Use Permits and Site Plans, and shall prepare and provide application forms, including identification of the information necessary for evaluation of the application and the criteria by which the application will be determined to be complete. No application will be accepted by the Community Development Department unless it complies with such requirements.
  - (c) Applications filed pursuant to this section shall be numbered along with other Conditional Use Permits and Site Plan applications in the City of Upland, and shall become part of the permanent official records of the Community Development Department.
  - (d) Filing fees in an amount specified by resolution of the Upland City Council shall be paid upon the filing of each application. A written receipt will be issued to the person making such payment, and records shall be kept by the City in such manner as required by law.

- d. Review.** The Planning Commission or Administrative Committee shall review and make findings and

determinations on applications within its approval authority (as listed in Paragraphs a and b, above), not more than sixty (60) calendar days after the Community Development Department accepts the application as complete for processing for applications requiring Planning Commission review, or twenty (20) working days for applications requiring Administrative Committee review.

- (1) The Planning Commission or Administrative Committee may approve, in whole or in part, conditionally approve, or deny any application within its approval authority as listed in Paragraphs a and b, above, within the Colonies at San Antonio Specific Plan area. The approval authority shall announce its decision, and, in a written opinion, shall list the reasons for its decision. The written opinion shall be forwarded to the applicant.
- (2) In cases where there is no review by the applicable review authority within the specified time limits, it shall constitute an automatic approval of the application.

**e. Findings.** The determination of the Planning Commission or Administrative Committee shall clearly state, in writing, any conditions of approval and the reasons for approving, conditionally approving, or denying the application based on the following requirements for approval.

- (1) The Conditional Use Permit (or Site Plan) is consistent with the provisions of the Colonies at San Antonio Specific Plan.
- (2) The Conditional Use Permit (or Site Plan) is consistent with the provisions of the Upland Municipal Code and other City ordinances as they apply to the Colonies at San Antonio Specific Plan area and the proposed site plan.
- (3) The Conditional Use Permit (or Site Plan) is consistent with the mitigation measures contained in the Final Subsequent EIR for the Colonies at San Antonio Specific Plan.
- (4) The Conditional Use Permit (or Site Plan) indicates the proper consideration for the functional relationship between the proposed site development and the site developments that exist or have been approved for lands adjacent to the proposed site plan boundaries.
- (5) The Conditional Use Permit (or Site Plan) and the location of the proposed buildings, parking areas, detached signs, landscaping, exterior lighting, and other site features indicates that proper consideration has been given to both the functional aspects of site development, as well as automobile, pedestrian, and bicycle access and circulation, and the visual effect that site development will have from the public's view.
- (6) Applicable requirements of the California Environmental Quality Act (CEQA) and the City's rules to implement CEQA have been complied with.

**f. Conditions of Approval.** In approval of a Conditional Use Permit or Site Plan, the approval authority may require any conditions of approval as it deems proper and reasonable to ensure that the purpose and intent of the Colonies at San Antonio Specific Plan and the Upland Municipal Code will be met. Conditions may include, but are not necessarily limited to requirements necessary for achieving consistency with the provisions of the Colonies at San Antonio Specific Plan:

- (1) Implementing mitigation measures contained in the Final Subsequent EIR for the Colonies at San Antonio Specific Plan;
- (2) Meeting the required findings for approval of a Conditional Use Permit or Site Plan set forth in Paragraph e, above;
- (3) Providing certain features as an integral part of the development, such as special setbacks and buffer areas, fences and walls, lighting fixtures planned and installed so as to illuminate the property in accordance with the performance standards set forth in this Specific Plan, surfacing of parking areas in accordance with City specifications, and installation of landscaping;

- (4) Locating points of ingress and egress so as to conform to traffic patterns on adjacent roadways, and designing driveways to expedite traffic flow within the site;
- (5) Limiting operations or certain activities to certain hours in order to meet applicable standards for noise;
- (6) Keeping signs to a specified maximum size and type, located in accordance with the provisions of this Specific Plan and applicable City codes;
- (7) Maintaining all improvements and landscaping in accordance with reasonable standards as long as the use continues.

**g. Responsibility.** Responsibility for the posting of bonds and fulfilling of any city requirements for both on- and off-site improvements shall rest with the applicant.

**h. Commercial Site Plan Modifications.** It is recognized that commercial center building designs and site plans may change subsequent to Conditional Use Permit or Site Plan approval and prior to actual issuance of building permits in order to accommodate the specific needs of individual commercial users. Thus, in approved Conditional Use Permits and Site Plans for Planning Areas 17 through 21, the applicant shall include, in addition to building footprints, identify “building envelopes,” which define the area in which the building footprint, size, height, bulk, and orientation may be modified without further review by the applicable approval authority. Expansion of buildings or modifications to building locations within such approved building envelopes shall not require further review by the City except as may be required by design review procedures or as required for issuance of a building or grading permit. Requests for major changes in approved site plans for commercial development within Planning Areas 17 through 21 shall, however, require formal amendment to the approved Conditional Use Permit or Site Plan. Such amendments are to be reviewed in the same manner as for a new site plan application. Major changes include modifications to site plans for individual planning areas that would place buildings outside of the approved building envelope, or would significantly alter the type of commercial development contemplated for the Planning Area (e.g., addition or deletion of such key uses as a theater, supermarket, or department store).

**i. Appeals from Decisions.** Any decision on an application by the Planning Commission or Administrative Committee regarding the approval, conditional approval, or denial of such application may be appealed by any person, public or private, aggrieved by such decision pursuant to the provisions of Chapter 17.16.070 of the Upland Municipal Code in regards to Planning Commission decisions or Chapter 17.16.130 in regards to Administrative Committee decisions.

**j. Revocation of Approvals.** A Conditional Use Permit shall remain in full force and effect in accordance with the terms thereof, unless modified or revoked by the City Council, upon recommendation of the Planning Commission, after a public hearing in accordance with the procedures specified in Chapter 17.16 of the Upland Municipal Code, on one or more of the following grounds.

- (1) That the use for which such approval was granted is not in conformance with the conditions set forth in the permit approval;
- (2) That the use for which such approval was granted has become a use free of all conditions, pursuant to an amendment to this Specific Plan.

### 11.2.3 Design Review

To ensure implementation of the Colonies at San Antonio Specific Plan design guidelines and landscaping requirements, all new development, and/or substantial reconstruction of existing buildings, structures (including signs), and landscaping; excepting remodeling of single family homes, and routine

maintenance of all structures, shall be subject to design review by the City of Upland as set forth in Chapter 17.06 of the Upland Municipal Code.

#### **11.2.4 Other Review Procedures**

In addition to land divisions and site plan review, a number of other reviews will be necessary in order to implement the Colonies at San Antonio Specific Plan. These additional review processes are identified below.

- Conditional Use Permits may be granted by the Planning Commission subject to the provisions of Chapter 17.16.020 of the Upland Municipal Code.
- Permits for temporary signs may be granted by the Community Development Director subject to the provisions of Chapter 17.16.160 of the Upland Municipal Code.
- Variances may be granted pursuant to the provisions of Chapter 17.16.050 of the Upland Municipal Code.
- Lot line adjustments may be granted pursuant to the provisions of Chapter 17.16.100 of the Upland Municipal Code.
- Determinations for similarity of use to uses set forth as permitted or conditionally permitted, temporary signs, special events, deviations to site development standards, and reconstruction or remodeling of nonconforming structures may be granted pursuant to the provisions of Chapter 17.16.140 of the Upland Municipal Code.
- Landscape plans for streetscapes and project entries shall be prepared and will be reviewed by the City of Upland concurrent with street improvement plans.
- Residential Unit Density transfers will be allowed between Planning Areas within the Specific Plan. The number of residential units will not exceed 1,150 throughout the Specific Plan Area. Density transfer requests will be processed with the Site Plan through Planning Commission using the density transfer mechanism in Table 11.A on the following page.

#### **11.2.5 Appeals**

Any decision by the Community Development Director, Administrative Committee, or Planning Commission regarding an approval, conditional approval, or denial may be appealed by any person, public or private, who is aggrieved by the action. Such appeals shall be made in accordance with the provisions of Chapter 17.16 of the Upland Municipal Code.

#### **11.2.6 Specific Plan Amendments**

This Specific Plan, or any portion, may be amended or replaced using the same procedure as for the original Plan adoption.

#### **11.2.7 Penalties for Violations**

The provisions, covenants, and restrictions in Chapter 17.12 of the Upland Municipal Code shall apply.

**Table 11.A. The Colonies At San Antonio Specific Plan  
Unit Count Transfer Mechanism Worksheet\***

<b>Planning Area</b>	<b>Built Unit Count</b>	<b>Minimum Lot Size</b>	<b>Acres</b>	<b>Density</b>	<b>Possible Units</b>
1	92	4.5	19.08	4.8	
2	99	5.0	22.13	4.5	
3	114	6.0	29.04	3.9	
4		MU	5.64	62.0	350
5	52	5.0	11.39	4.6	
6	32	5.0	6.37	5.0	
7	46	5.0	9.12	5.0	
8	33	4.0	6.17	5.3	
9	47	6.0	11.07	4.2	
10	56	5.0	10.72	5.2	
11	48	5.0	10.78	4.5	
12	55	5.0	11.01	5.0	
13	68	5.0	15.6	4.4	
14	0	Fire	1.72	0	
15		RF	10.2	16.0	160
16	0	Basin	69.72	0	
17	0	NC	12.4	0	
18	0	CH	36.53	0	
19	0	CH	42.31	0	
20	0	CH	12.66	0	
21	0	CH	11.02	0	
22		RF/NC	3	24.0	72
<b>Total</b>	<b>742</b>				
<b>Allowable</b>	<b>1,150</b>				
<b>Available</b>	<b>408</b>				

\*Instructions: When a developer brings a site plan for Planning Areas 4, 15, or 22, enter the total number of units proposed into the "Built Unit Count" cell. Then calculate the "Density" cell and the "Total" and "Available" cells. If the "Total" exceeds the "Allowable", the proposed number of units exceeds the permitted number of dwelling units within the Specific Plan.

### 11.3 PROJECT MAINTENANCE

#### 11.3.1 Roadways

Each of the roadways shown in Figure 3.1, Circulation Plan will be dedicated to the City of Upland for maintenance. Traffic signals installed as part of the Colonies at San Antonio Specific Plan will also be dedicated to the City for maintenance.

It is intended that roadways within residential neighborhoods zoned SFR will also be dedicated to the City for maintenance; however, private streets may be approved as part of tract maps for individual SFR Planning Areas. Where private streets are approved, they will be maintained by a homeowners' association or other mechanism set up for that purpose. In addition, if private streets are approved, residential planning areas may be developed as "gated" neighborhoods, provided that maintenance of gated facilities are also maintained by a homeowners' association or other mechanism set up for that purpose.

On-site roadways and access within non-residential planning areas (commercial, fire station) will be the responsibility of the property owner.

### **11.3.2 Open Space and Drainage Facilities**

Maintenance of the central open space area and basins (Planning Areas 16, 16A, 16B) shall be provided by a landscape maintenance district to be formed and administered by the City of Upland. Trails, parkways, and lettered lots will be the maintenance responsibility of a master homeowners' association formed for the Colonies at San Antonio Specific Plan area.

Maintenance of storm drains within public streets, including outlets into the central open space area and on-site basins, will be the responsibility of the City of Upland. Where storm drains are located outside of a public street, such storm drains will be located within a public utilities easement.

Maintenance of the flood control facilities located in Planning Areas 16A and 16C shall be the responsibility of the San Bernardino County Flood Control District.

### **11.3.3 Water Facilities**

Maintenance of water facilities up to service meters for individual uses will be the responsibility of the City of Upland. Where such water lines are not located within a public street, appropriate easements to the City will be granted.

### **11.3.4 Sewer Facilities**

Maintenance of sewer facilities up to service laterals for individual uses will be the responsibility of the City of Upland. Where such sewer lines are not located within a public street, appropriate easements to the City will be granted.

### **11.3.5 Multi-family Residential Development**

Maintenance of the building exteriors and landscaping for all attached, multi-family residential developments will be the responsibility of a homeowners' association. Said homeowners' association will be a sub group of the existing Master Homeowners' Association, but may be subject to additional regulations and potentially higher assessments.

## **11.4 FINANCING OF PROJECT INFRASTRUCTURE**

The ultimate responsibility for construction of the infrastructure and facilities outlined in Chapter 10.0, with the exception of the final drainage improvements within the Central Open Space Area, shall lie with the developer of the Colonies at San Antonio Specific Plan<sup>1</sup>. The ultimate responsibility for the construction of the final drainage improvements within the Central Open Space Area, including Planning Areas 16, 16A, and 16B shall lie with the San Bernardino County Flood Control District. Assessment district financing will be utilized for on-site and off-site public facilities needed to support project development. Infrastructure and facilities costs, except for the costs of final drainage improvements to be constructed within the Central Open Space Area, above that which can be financed with a maximum 2.0 percent property tax rate will be privately financed by the project developer. In addition, the City and developer will cooperate to prepare and file applications and provide support as necessary for funding from any appropriate federal, state, and/or county source to defray the cost of regional impacts in connection with construction and installation of offsite public improvements required to support project build out. Where infrastructure is required to be provided or sized in excess of what is needed for

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<sup>1</sup> Offsite water facilities needed for the Colonies at San Antonio Specific Plan will be constructed by the developer, unless determined otherwise by mutual agreement of the City and the developer. The City will provide a site for the offsite water storage facility discussed in Chapter 5.0.

development of the Colonies at San Antonio Specific Plan, costs in excess of the project's fair share will be reimbursed to the project developer by the City or San Bernardino County through credits against project fees, reimbursement agreement(s), and/or cash payments.

To facilitate construction of a fire station within the Colonies at San Antonio, the developer of the Specific Plan will pay a fee to the City in addition to the dedication of land for the fire station. The City will use the fee and other City funds to construct and equip the fire station.

#### 11.5 TRAFFIC IMPACT ANALYSIS AND MITIGATION REQUIREMENTS

To be completed based on Kimley-Horn and Associates TIA and the EIR.

The modifications that were made in SPA-7 Mod #5 require a traffic impact analysis to be submitted at the time of site plan review for Planning Areas 4, 15, and 22.