

UPLAND CROSSING SPECIFIC PLAN AMENDMENT

PREPARED FOR:

THE CITY OF UPLAND
DEVELOPMENT SERVICES DEPARTMENT
460 NORTH EUCLID AVENUE
UPLAND, CA 91786

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UPLAND CROSSING SPECIFIC PLAN AMENDMENT

Proposed by:

SC BALDY VIEW DEVELOPMENT COMPANY, LLC.

1156 N. MOUNTAIN AVENUE
UPLAND, CA 91785
(909) 985-0971
CONTACT: BRYAN GOODMAN

Prepared by:

TEMPLETON PLANNING GROUP
20250 ACACIA STREET, SUITE 260
NEWPORT BEACH, CA 92660
(949) 724-0640
CONTACT: PETER TEMPLETON

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I. INTRODUCTION

A. EXECUTIVE SUMMARY

1. PURPOSE

This Specific Plan document has been prepared to facilitate processing and approval of the Upland Crossing Specific Plan development and its associated discretionary and administrative approvals. The Upland Crossing Specific Plan provides detailed text and exhibits which describe the residential and possible commercial-retail uses, and associated improvements that are envisioned to occur within the project. Design guidelines and development standards contained within this Specific Plan will guide site planning, architecture, and landscape design to create a cohesive project identity. This Specific Plan document will guide the build-out of the Upland Crossing Specific Plan area in a manner which is consistent with City and State policies and standards and assures that the project is developed in a coordinated manner.

2. SPECIFIC PLAN AUTHORITY AND CONTENT

The Upland Crossing Specific Plan has been prepared pursuant to the provisions of California Government Code Section 65450, which grants local government agencies the authority to prepare specific plans of development for any area covered by a General Plan, for the purpose of establishing systematic methods of implementation of the agency's General Plan. A General Plan does not show local streets, individual parcels, or site-specific land uses. A Specific Plan, on-the-other-hand, is specific in nature as compared to a General Plan. It is designed to address site specific issues such as building setbacks and visual appearance, as well as project-wide concerns such as circulation and provisions for utilities. While a General Plan examines an entire City or County, a Specific Plan concentrates on the individual development

issues of a region or particular property. The City of Upland General Plan contains objectives which mandate the preparation of Specific Plans to ensure that new development meets basic standards for environmental safety, infrastructure, and site planning while providing provisions for the maintenance of aesthetic quality and cultural identity.

California Government Code Sections 65450 through 65454 establish the authority to adopt a Specific Plan, identify the required contents of a Specific Plan, and mandate consistency with the General Plan. According to Section 65450, a Specific Plan shall include a text and a diagram or diagrams which specify all of the following detail:

1. The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan;
2. The proposed distribution, location, extent, and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan;
3. Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable;
4. A program of implementation measures including regulations, programs, public works projects, and financing measures necessary to carry out items (1), (2), and (3) above; and
5. A discussion of the relationship of the Specific Plan to the General Plan.

In response to government requirements, this Specific Plan has been

prepared to provide the essential link between the policies of the Upland General Plan and actual development in the project area. By functioning as a regulatory document, the Upland Crossing Specific Plan provides a means of implementing and detailing the City of Upland's General Plan. In this regard, all future development plans and entitlements within the Specific Plan boundaries shall be consistent with the standards set forth in this document. This Specific Plan provides a path to properly develop the Upland Crossing Specific Plan, taking into account local goals, objectives, and policies.

3. PROJECT SUMMARY

The proposed project consists of a Specific Plan to allow for the development of a residential and possible small commercial-retail community, offering options of flexibility within this future community in the City of Upland. The Upland Crossing Specific Plan is located at the western gateway of the City of Upland, generally bounded by Foothill Boulevard to the north, Monte Vista Avenue to the west, 11th Street to the south, and Dewey Way to the east. The Upland Crossing Specific Plan consists of up to 332 single family residential homes and condominiums and up to 27,500 square feet of new commercial-retail floor area or 23 additional single family residential homes on the approximately 31.6-acre property. Up to 27,500 square feet of commercial-retail floor area or a residential overlay of approximately 23 single family residential homes are proposed in Planning Area 1. Planning Area 2 will have up to 132 single family residential homes and Planning Area 3 will have up to 200 single family condominiums. The residential density for Planning Areas 1 through 3 will range from 12.0 to 20.0 dwelling units per acre. The proposed recreation and open space areas include an approximate 1.0-acre recreation area, including a swimming pool, common gathering area, and perimeter landscaping. In addition, Planning Area 4 will include the San Antonio Creek Channel which generally bisects the Upland Crossing property in a slight northeast to southwest direction. A pedestrian trail will be located on the west side and an access road will be located on the east side of this

channel, subject to regulatory approvals. Additionally, a vehicular roadway will cross the channel at the southern portion of the project site, providing convenient access by connecting Planning Areas 2 with 3. All interior streets within this community will be private. Five vehicular access points to the project are proposed along Foothill Boulevard, Monte Vista Avenue, 11th Street and Dewey Way.

Until recently, the Upland Crossing property was used as quarries and basins for the Chino Basin Water Conservation District. A General Plan Amendment changed the land use designation from Commercial/Industrial-Special Use Permit (CI-S) to Specific Plan (SP). The project site's zoning was changed from Highway Commercial (CH) and Light Industrial (ML) to Specific Plan (SP).

The Upland Crossing Specific Plan provides for flexible development under the Specific Plan designation contained in the City of Upland General Plan. The Specific Plan contains development controls to provide the City of Upland with assurances that the site area will be built out as planned. To ensure that the design criteria are adhered to, a set of design guidelines is included within the Upland Crossing Specific Plan. These design guidelines encompass site planning, landscaping, lighting, architectural design, architectural theme and community character.

4. PROJECT GOALS

The Upland Crossing Specific Plan is a development that is consistent with the City's desire to revitalize the area while creating a sense of place and a central activity focus. The project will provide for residential uses, a possible commercial-retail area, and a recreation area integrated into one master development plan. Each developed use will be within easy walking distance of one another and will be designed with common landscape and architectural themes. The goal of providing a development in a previously underutilized and unincorporated area will be met with the implementation of the Upland

Crossing Specific Plan.

More specifically, the goals and objectives of the Upland Crossing Specific Plan are to:

- Create a comprehensive plan that is consistent with the City’s desire to revitalize the area that was annexed into the City, while creating a sense of place and a central activity focus;
- Create a high quality infill development that provides a combination of homes and lot types;
- Provide development guidelines so that the builders can develop using complementary architectural themes that will allow a variety of elevations within their respective styles. The architectural theme will include Spanish Colonial and Collegiate styles.
- Provide a new development that complements and strengthens the residential neighborhoods east and southeast of the project site. Multiple-Family Residential including condominiums (12-20 DU/AC), Single Family Residential (4-6 DU/AC) and Multiple-Family: Mobile Homes (8-14 DU/AC) are located east of the project site. To the southeast are Multiple-Family Residential (12-20 DU/AC) and Single Family Residential (4-6 DU/AC);
- Provide housing that meets the market demand with similar densities to those of the residential communities previously described;
- Improve the appearance of the existing San Antonio Creek Channel which generally bisects the project in a slight northeast to southwest direction;
- Provide landscape and architecture elements that incorporate the City’s theme of preserving the past with the current and future

needs of the community. The project will be designed so the architecture, street and pedestrian corridors, and neighborhood identification elements reflect the City’s current vernacular; and

- Create a development that combines landscape and urban design elements to help create the urban gateway that the City desires.

B. SETTING

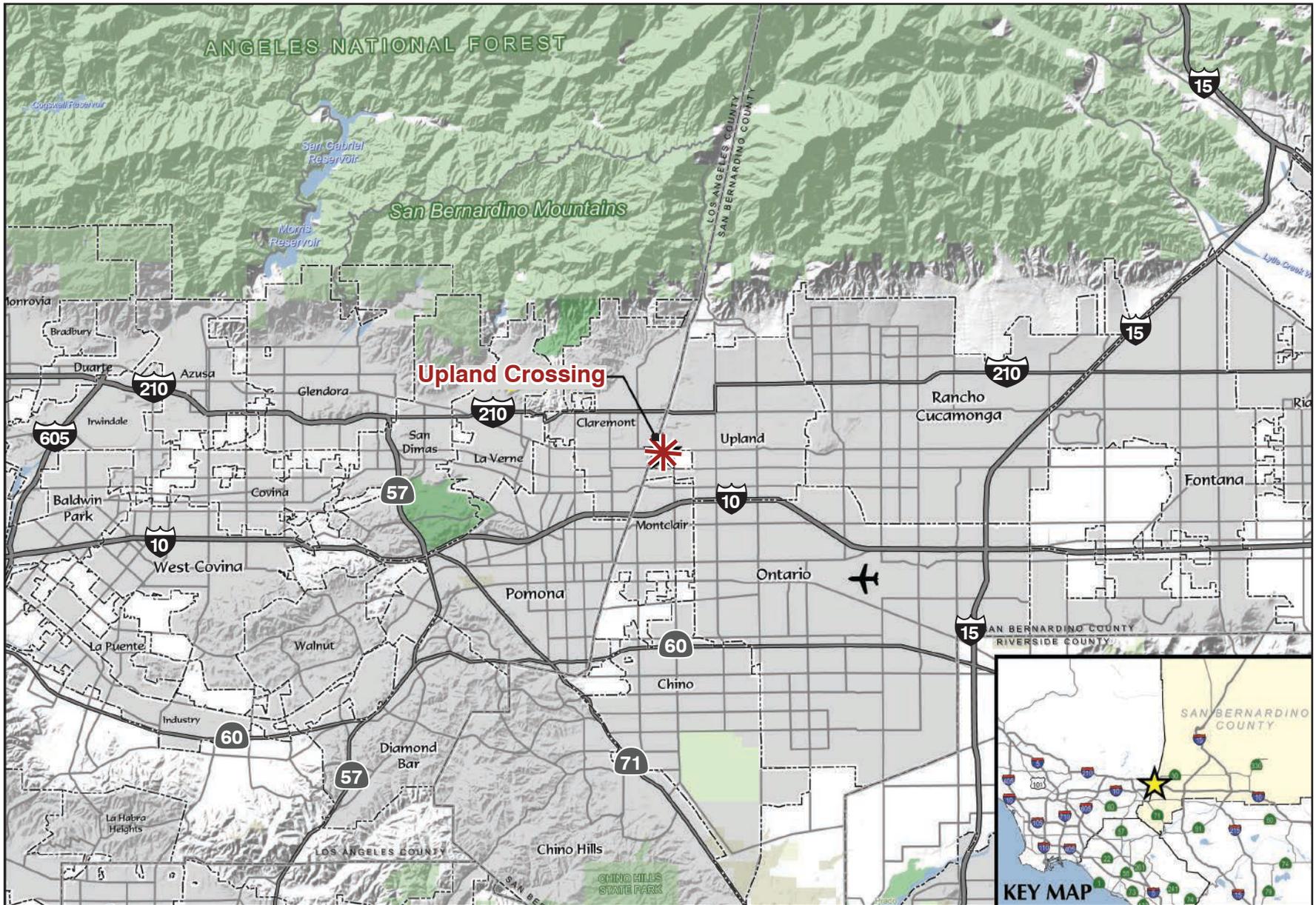
1. SITE LOCATION

The 31.6-acre Upland Crossing Specific Plan property is located in San Bernardino County, California in the western portion of the City of Upland. The City of Upland is situated west of the City of Rancho Cucamonga, north of the City of Ontario, and east of the City of Claremont (see Figure 1, Regional Map and Figure 2, Vicinity Map). Regional access is provided by Interstate 10 (I-10) to the south and State Route 210 (SR-210) to the north. The property is bounded to the west by Monte Vista Avenue, to the north by Foothill Boulevard, and to the south by the future extension of 11th Street (see Figure 3, Aerial Photograph).

2. SITE CONDITIONS

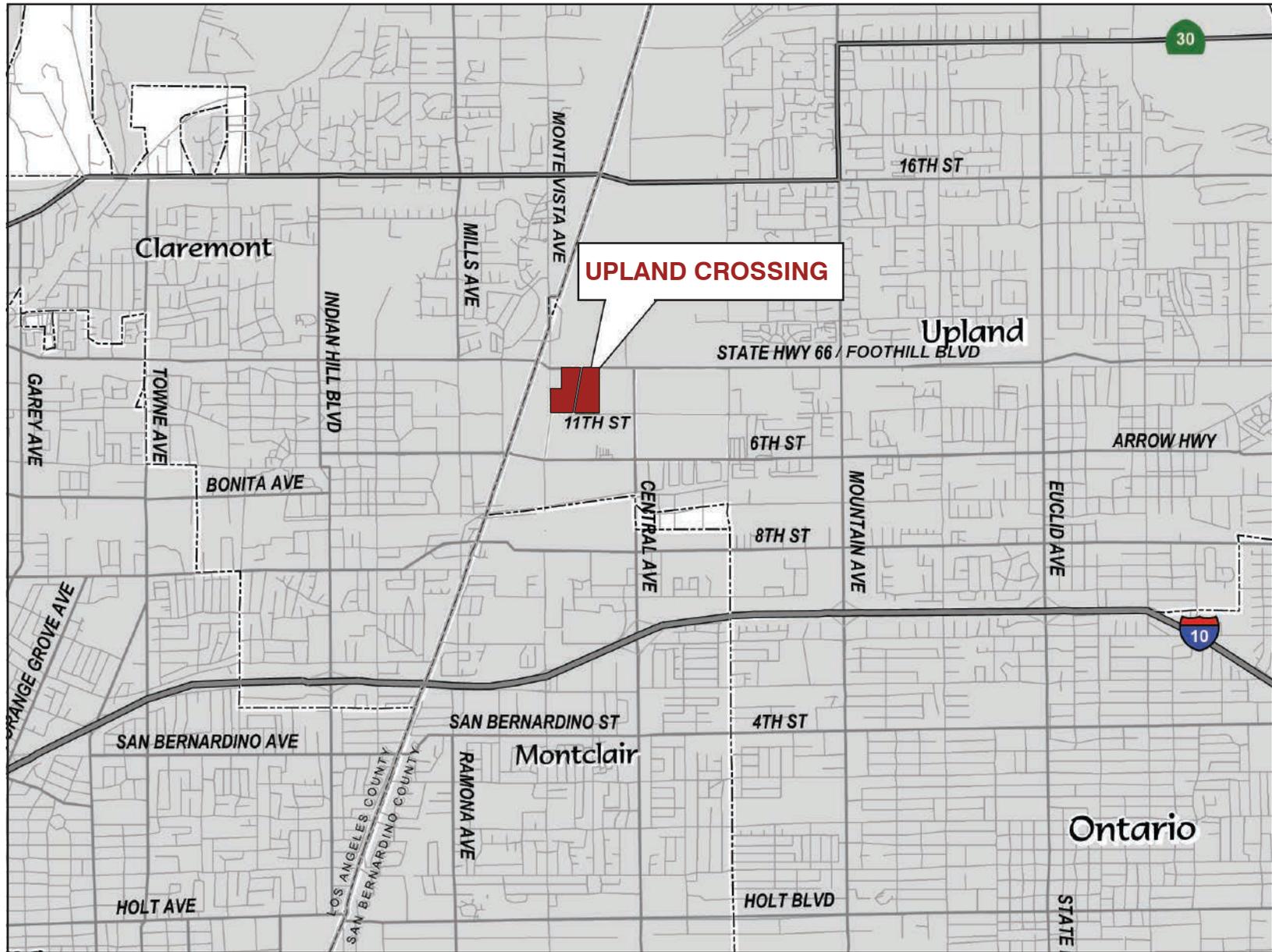
At present, the project site is generally undeveloped and previously consisted of two retention basins. Over the years, these detention basins have been filled and cleared and are currently surrounded by a light to moderate growth of weeds and shrubs along their boundaries. Diagonally separating the two former retention basins is the San Antonio Creek Channel and adjacent property owned by the Chino Basin Water Conservation District.

The project area slopes significantly from the northern boundary at Foothill Boulevard to the southern boundary at 11th Street. Elevations on-site range from 1,284 feet to 1,336 feet above mean sea level,



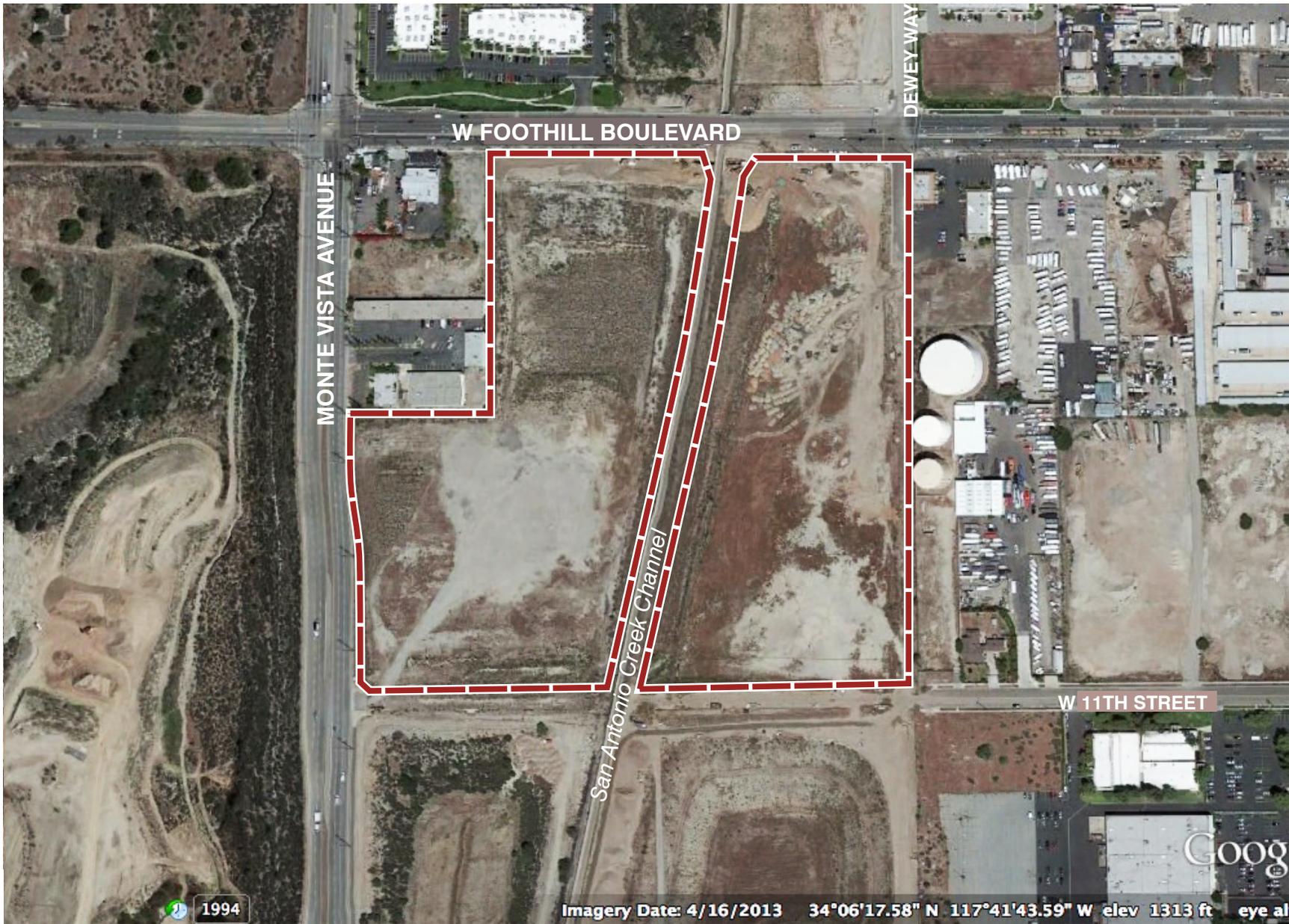
Upland Crossing Specific Plan Amendment
 Upland, CA

Figure 1
Regional Map



Upland Crossing Specific Plan Amendment
 Upland, CA

Figure 2
Vicinity Map



Source: Google Earth

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Upland, CA

Figure 3
Aerial Photograph

generally sloping in a southwest direction (see Figure 4, Topographic Map). Future grading will be limited to excavation for foundations and associated infrastructure. Currently there is no access provided to the project site.

3. SURROUNDING LAND USES AND DEVELOPMENT

The Upland Crossing Specific Plan is located in an area that was annexed into the City of Upland from Los Angeles County. The majority of the Foothill Boulevard corridor has been developed over the years with commercial uses. Specifically, land to the north of the Upland Crossing Specific Plan site is developed with several business parks. Located east of the project site are water tanks operated by the Monte Vista Water District and a small commercial center. The southern edge of the project site is bordered by the future extension of 11th Street connecting to Monte Vista Avenue and water recharge basins owned by the Chino Basin Water Conservation District. Unused land owned by Claremont College is located west of the project site. Overall, the Upland Crossing Specific Plan will develop the previously underutilized character of this area.

C. LAND USE ENTITLEMENT

1. GENERAL PLAN DESIGNATION

The City of Upland General Plan Land Use Map currently designates the 31.6-acre Upland Crossing project site as Specific Plan (SP), as shown on Figure 5, General Plan Land Use Designations. The SP designation permits all proposed uses, with multi-family residential allowed pursuant to a special use permit. The Specific Plan Overlay designation requires a Specific Plan and its design guidelines to be prepared and adopted to provide direction related to site planning, architectural design, and site specific development standards. Project compliance to the City of Upland General Plan is demonstrated in Section VI of this Specific Plan.

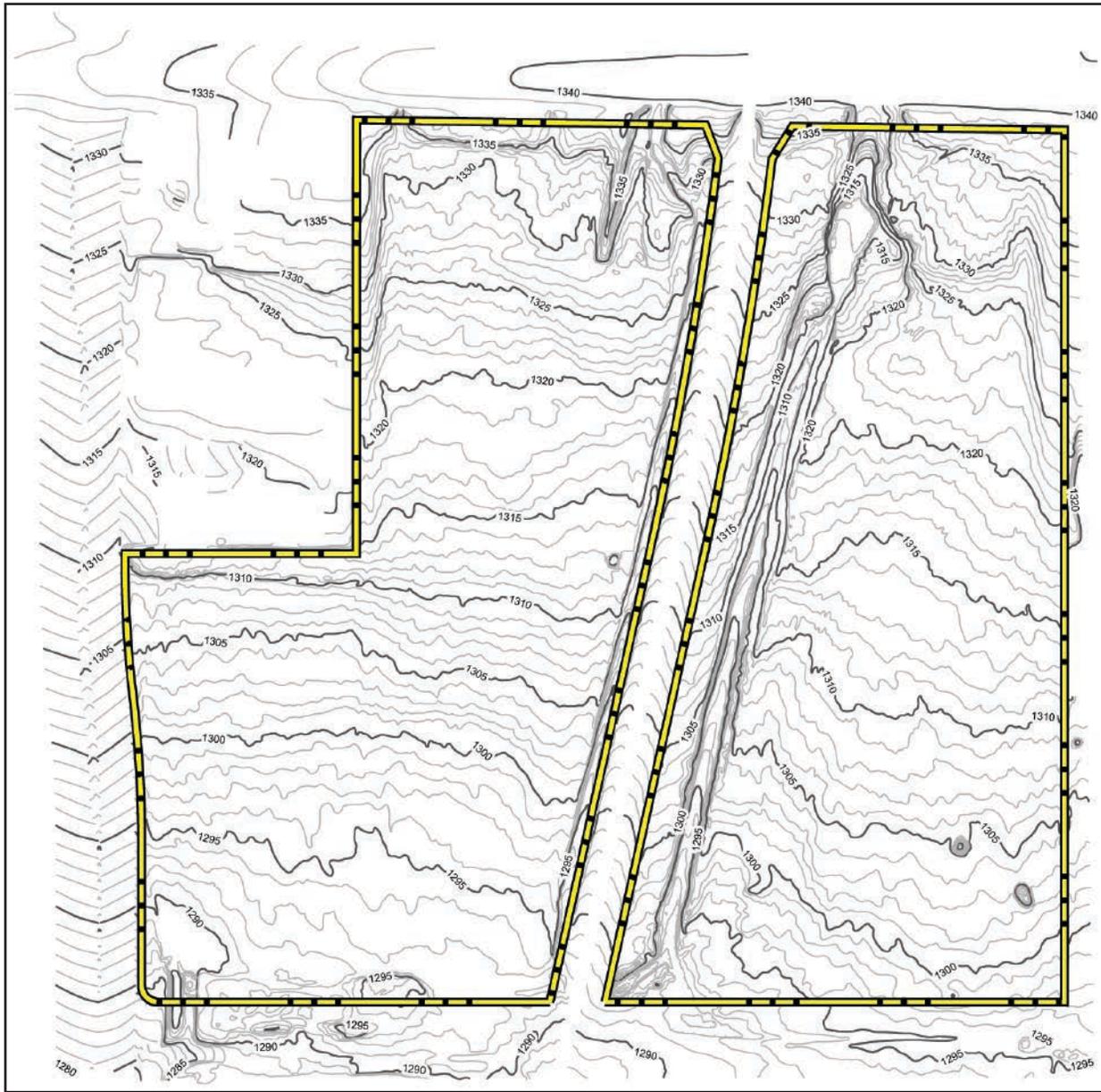
2. ZONING

The Upland Crossing Specific Plan site is currently zoned Specific Plan (SP) by the City of Upland, as depicted on Figure 6, Zoning Map. Development Standards are contained in Section III of this Specific Plan document, which provide detailed zoning standards specifically tailored for the Upland Crossing Specific Plan.

3. DISCRETIONARY ACTIONS AND APPROVALS

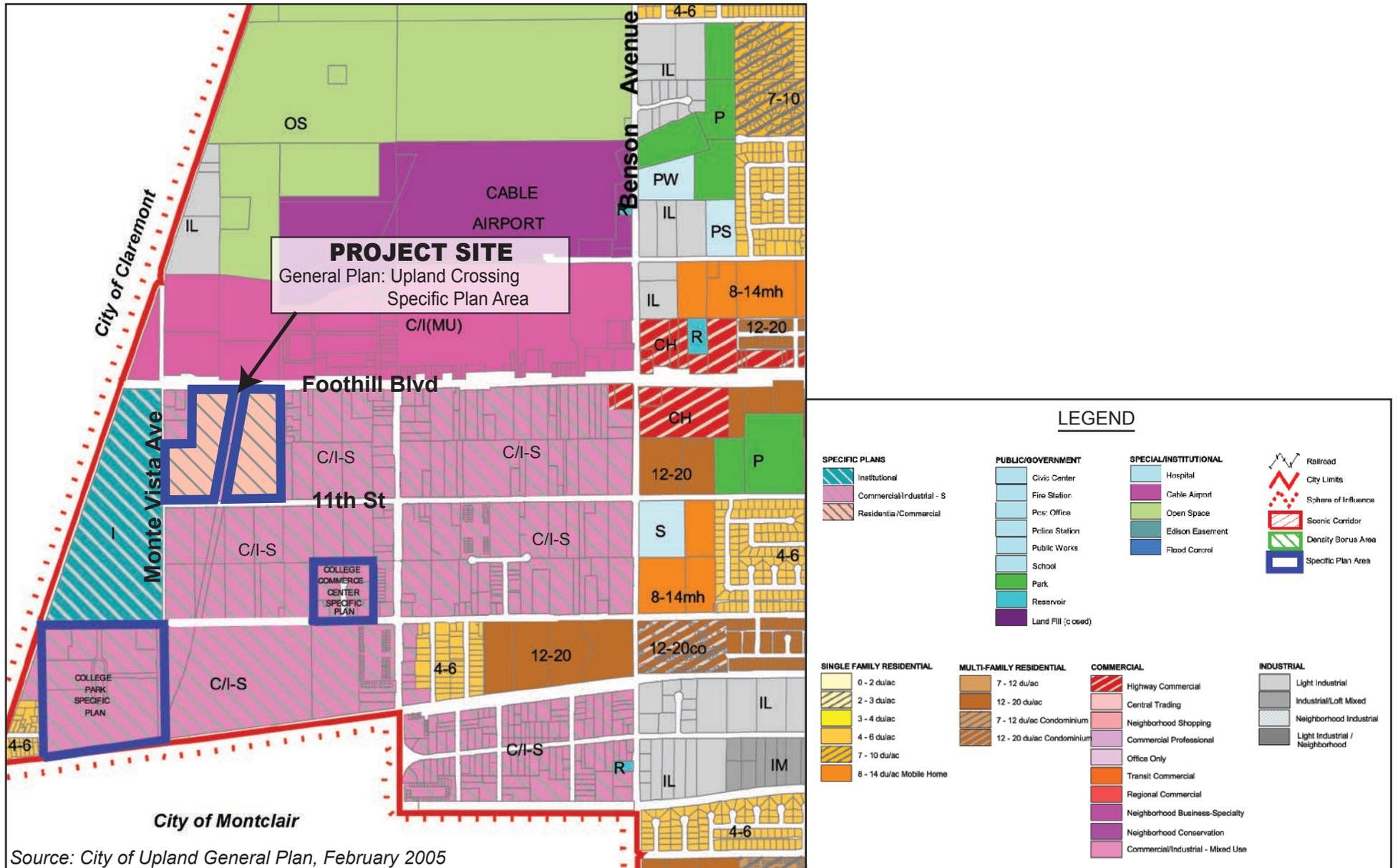
The project applicant seeks to amend the Upland Crossing Specific Plan to allow for the proposed uses. The Upland Crossing Specific Plan was prepared under the authority of the City of Upland. This document will be used in connection with the following related decisions:

- Adoption of the Upland Crossing Specific Plan Amendment;
- Approval/adoption of a Development Agreement for the Upland Crossing Specific Plan;
- Approval/adoption of Tentative Tract Maps implementing the Upland Crossing Specific Plan;
- Certification or approval of the required California Environmental Quality Act (CEQA) document; and
- Conditional Use Permits (CUPs), Variances, and Design Review as implementing actions of the Upland Crossing Specific Plan.



Source:

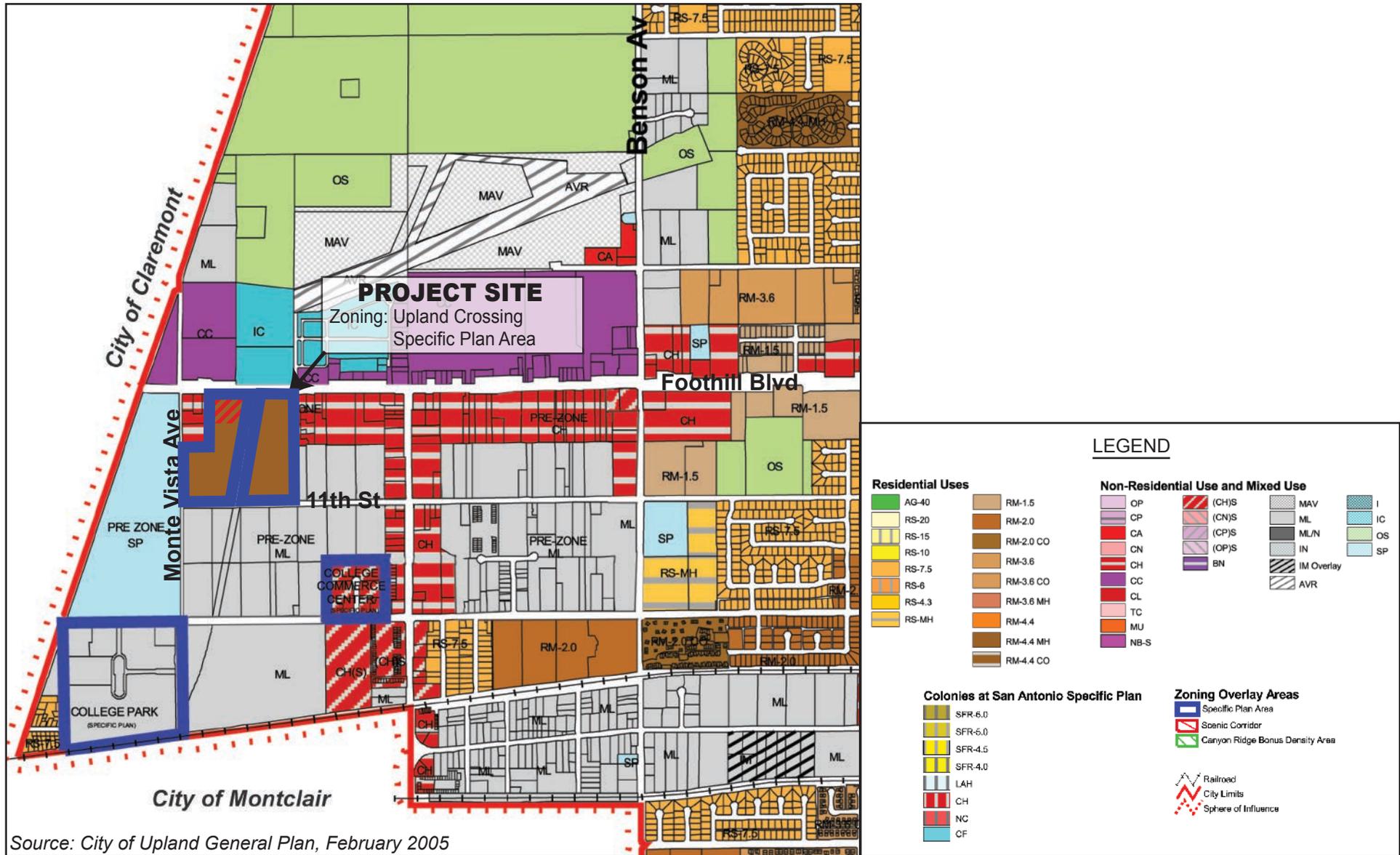
Figure 4
Topographic Map



Source: City of Upland General Plan, February 2005

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Figure 5
General Plan Land Use Designations



Source: City of Upland General Plan, February 2005

Upland Crossing Specific Plan Amendment
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Figure 6
Zoning Map



II. SPECIFIC PLAN PROGRAM ELEMENTS

A. OVERVIEW

Upon completion, the Upland Crossing Specific Plan will contain a high-quality development composed of up to 332 residential units in a variety of densities, a recreation area, and up to 27,500 square feet of new commercial-retail floor area or 23 additional residential units on a 31.6-acre property, as shown in Table 1, Land Use Summary. Figure 7, Specific Plan Land Use Plan, illustrates the distribution of land uses. Options for this site include residential and retail, thereby enhancing the potential for establishing a vibrant master planned development. The development of the Upland Crossing Specific Plan will incorporate sound site planning designed to connect the various land uses and facilitate vehicular and pedestrian movement.

It is the intent of this Specific Plan to ensure that site development is accomplished in a logical and timely manner. Specific design elements, including the siting of compatible land uses, selection of complementary architectural design, and implementation of a unified landscape plan, have been conceived and consistently applied in order to assure a varied and fully integrated development. A number of important issues were examined and considered during the preparation of this Specific Plan document, including market acceptance, economic viability, environmental sensitivity, and positive aesthetic contribution to the City of Upland and its residents.

Review and approval of development plans will occur as implementing actions of this Specific Plan. Section V, Implementation, details the submittal requirements and process for implementing the Upland Crossing Specific Plan.

**TABLE 1
LAND USE SUMMARY**

PLANNING AREA	LAND USE	ACRES	DENSITY RANGE (DU/AC)	MAXIMUM DWELLING UNITS	MAXIMUM RETAIL FLOOR AREA (S.F.)
1	Commercial-Retail	2.6	--	--	27,500
1	Residential Overlay	2.6	12.0 – 20.0	52	--
2	Residential	11.8	12.0 – 20.0	236	--
3	Residential	12.3	12.0 – 20.0	246	--
4	Channel Trail	1.5	--	--	--
5	Private Recreation Center	1.0	--	--	--
--	Project Roads	1.8	--	--	--
TOTAL		31.6		546*	27,500

*Densities may vary; maximum number of du's will not exceed 355 units.



Note: Planning areas do not include dedicated perimeter public street right-of-ways



B. PLANNING AREAS

Four distinct development areas, identified as “Planning Areas,” implement the planning objectives set forth for the Upland Crossing Specific Plan. These four planning areas arise from a logical organization of uses and planned units of development that will create the environment envisioned for the project. Collectively, the various planning areas will generate a unique identity for the Upland Crossing Specific Plan through complementary architectural and landscape themes and a strong circulation network. A general description of each planning area is set forth below, while specific development standards and guidelines for design implementation are provided in Section III, Development Standards, and Section IV, Design Guidelines.

1. COMMERCIAL-RETAIL WITH RESIDENTIAL OVERLAY—PLANNING AREA 1

Located in the northwestern portion of the site adjacent to Foothill Boulevard, Planning Area 1 could serve as the visual and retail focal point of the Upland Crossing Specific Plan and could provide up to 27,500 square feet of new commercial-retail floor area on 2.6 acres. Planning Area 1 could provide for smaller shops that would offer various types of goods and services. Primary access and identification signage for commercial-retail tenants would be located in Planning Area 1 and the area would be integrated with adjacent planning areas through secondary pedestrian access points.

A residential overlay on Planning Area 1 is suggested for the Upland Crossing Specific Plan. This overlay would allow for a maximum of 23 residential units in place of the 27,500 square feet of commercial-retail floor area maximum allowed on the 2.6-acre Planning Area 1. Residential densities within the Upland Crossing Specific Plan Area shall be transferable. Planning Areas 1 and 2 would be site planned as if it were one residential planning area, ensuring a cohesive design. Additionally, this residential overlay would omit the project entrance in Planning Area 1 at Foothill Boulevard. Residents living in Planning

Area 1 or 2 would enter and exit the project site via 11th Street and/or Dewey Way, or exit only onto Monte Vista Avenue. An emergency vehicle access would be provided at the northeast corner of Planning Area 1 adjacent to the San Antonio Creek Channel at Foothill Boulevard.

2. RESIDENTIAL – PLANNING AREA 2

Situated in the southwest portion of the site, Planning Area 2 will provide for the development of up to 236 residential dwelling units on 11.8 acres. The density for Planning Area 2 shall range from 12.0 to 20.0 dwelling units per acre.

3. RESIDENTIAL – PLANNING AREA 3

Residential uses will be developed on the east half of the Upland Crossing Specific Plan site. Specifically, Planning Area 3 will be developed with up to 246 dwelling units on 12.3 acres. The density for Planning Area 3 shall range from 12.0 to 20.0 dwelling units per acre.

4. CHANNEL TRAIL – PLANNING AREA 4

The San Antonio Creek Channel will be located in Planning Area 4, which generally bisects the Upland Crossing Specific Plan area in a slight northeast to southwest direction. This channel will include approximately 1.5 acres of improvements within the right of way, including a pedestrian trail on the west side, and an access road on the east side. Informal plantings of canopy trees and medium scale vertical screen trees planted on both sides of the channel will provide residents living along the channel privacy as well as shade for trail users during the warm summer months. Additionally, the project will include a road located at the south end of the channel which will connect the two residential neighborhoods together. All improvements within the San Antonio Creek Channel right of way are subject to U.S. Army Corps of Engineers and San Bernardino County Flood Control District approval.

5. PRIVATE RECREATION CENTER – PLANNING AREA 5

Located immediately west of the San Antonio Creek Channel, Planning Area 5 will include an approximate 1.0-acre private recreation area which is intended to serve the active and passive recreational needs of the community. This area will include a swimming pool and common gathering area, and may also include walking paths, open turf areas for play, tot lots, covered picnic shelters, and picnic/barbeque areas, as shown on Figure 35, Private Recreation Area Concept.

C. CIRCULATION PLAN

1. VEHICULAR CIRCULATION AND ACCESS

The Upland Crossing Specific Plan improvements on the internal circulation network will ensure safe, direct, and convenient vehicular access to and through the project’s various land uses. The Vehicular Circulation Plan, Figure 8, illustrates the existing and proposed roadway network configuration with the residential overlay in place, which is consistent with the City of Upland’s General Plan Circulation Element. Direct access to the Upland Crossing Specific Plan is provided via the existing Foothill Boulevard and Monte Vista Avenue, and will have access from the future extension of 11th Street and Dewey Way.

This Specific Plan also calls for landscaping improvements to existing exterior project roadways. To the extent possible, existing lane configurations and right-of-way improvements on existing exterior project roadways will be retained. If the required project CEQA document and associated Traffic Analysis determine that improvements to exterior project roadways are required, they shall be completed as a part of implementing the Upland Crossing Specific Plan. The project will be required to pay applicable fair-share costs for improvements as required by the MMRP of the certified EIR. A description of the exterior project roadways adjacent to the Upland Crossing Specific Plan is described below.

- **Foothill Boulevard**

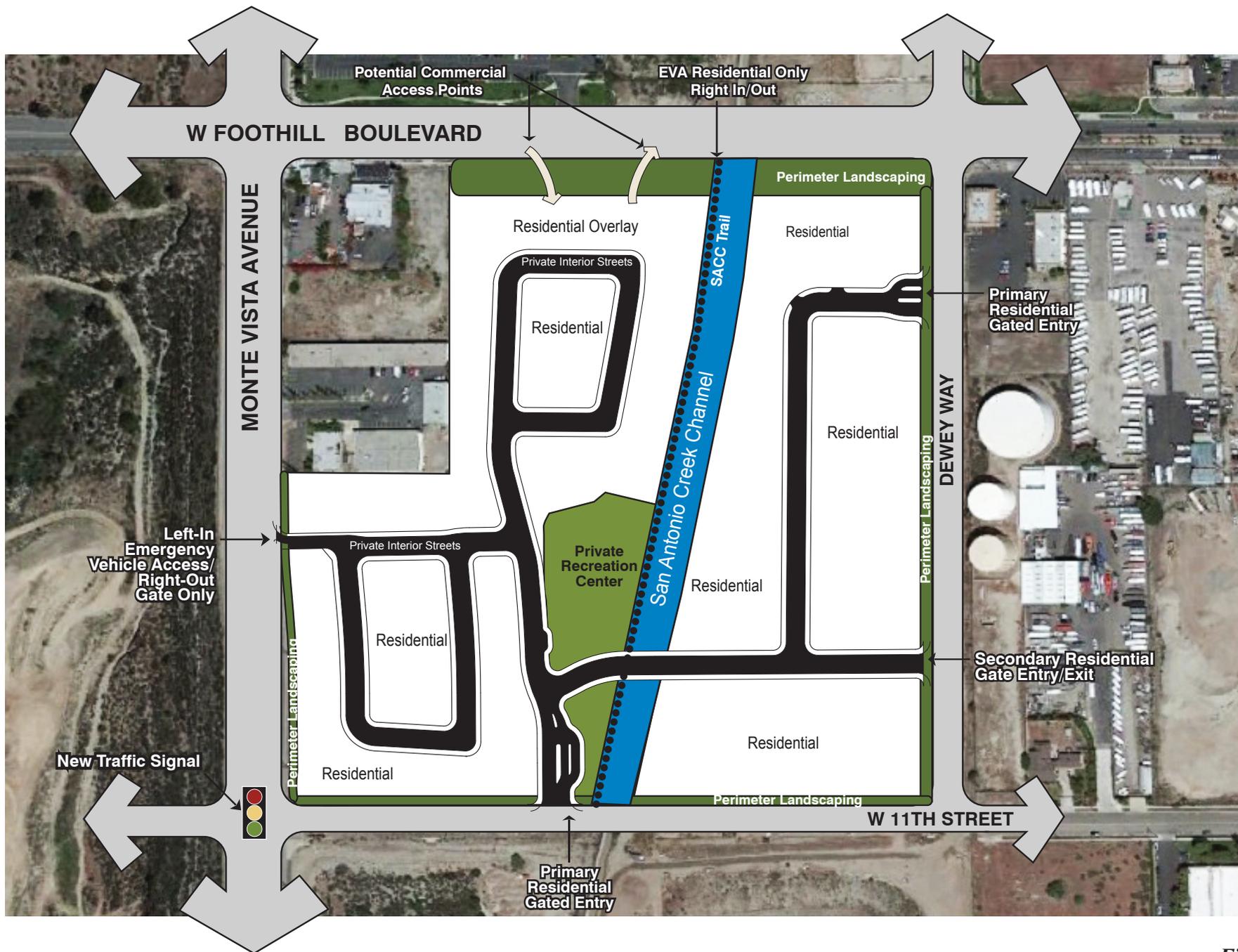
Foothill Boulevard, also called historic Route 66, is classified as a Major Arterial and serves as a backbone of Upland’s transportation system by offering adjacent and nearby commercial, industrial, and residential development a major access route to Interstate 10 (I-10) and State Route 210 (SR-210) via Monte Vista Avenue. It also acts as the primary east-west distributor to other segments of the City’s circulation system. Foothill Boulevard forms the northerly boundary of the project and has a base right-of-way width of 107 feet south of centerline. The San Antonio Creek Channel covering at the Foothill bridge was extended approximately 42 feet. Right-turn in and right-turn out access into Planning Area 1 on Foothill Boulevard will be provided. If the residential overlay is exercised, only an emergency vehicle access to Foothill Boulevard will be provided.

- **Monte Vista Avenue**

Monte Vista Avenue, which forms the western boundary of the Upland Crossing Specific Plan, is designated as a Major Arterial with a base right-of-way width of 136 feet. Monte Vista Avenue runs in a north-south alignment and provides direct access from Interstate 10 (I-10) and State Route 210 (SR-210) to the Upland Crossing Specific Plan site. A gated right-turn out exit only will be provided along Monte Vista Avenue at the northwestern corner of Planning Area 2. A traffic signal at the intersection of Monte Vista Avenue and 11th Street will be constructed.

- **Future 11th Street**

The future extension of 11th Street, which will form the



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Figure 8
Vehicular Circulation Plan

southern boundary of the Upland Crossing Specific Plan, is designated as a Collector Road with a base right-of-way width of 66 feet. Future 11th Street will run in an east-west alignment and will provide direct access to Central Avenue and Monte Vista Avenue. A gated full access in and out of the project site will be provided along 11th Street near the southeast corner of Planning Area 2. A three-way stop sign will be constructed at the intersection of 11th Street and Dewey Way.

- **Future Dewey Way**

Future Dewey Way, which will form the eastern boundary of the Upland Crossing Specific Plan, will be designated as a local collector street with a base right-of-way width of 61 feet. Future Dewey Way will run in a north-south alignment and will connect with Foothill Boulevard and the future extension of 11th Street. A gated primary full access in and out of the project site will be provided along Dewey Way south of Foothill Boulevard, and a gated secondary entry and exit will be provided along Dewey Way north of 11th Street.

2. PEDESTRIAN CIRCULATION AND ACCESS

The Upland Crossing Specific Plan is designed to be a pedestrian friendly community. Internal pedestrian circulation is designed around sidewalks on both sides of every street which connect to the recreation area and pedestrian trail that will be constructed along the west side of the San Antonio Creek Channel. See Figure 9, Pedestrian Circulation Plan for more detail. Pedestrians will also be able to safely and conveniently walk along the streets which border the project site via pedestrian access ways. These gated pedestrian access ways will be provided by the four vehicular access locations previously described, as well as by the north and south gated entrances to the pedestrian trail.

D. GRADING PLAN

The grading plan for the Upland Crossing Specific Plan will be tailored to the existing topography of the project site. Due to the extensive topographic variation occurring on-site, a significant grading effort will be required for future development proposed by this Specific Plan. The existing site elevation ranges from 1,284 feet to 1,336 feet above mean sea level and has an approximate fall of 52 feet between these two elevations. The project area slopes significantly from the northern boundary at Foothill Boulevard to the southern boundary at 11th Street. Soils and geologic conditions present no significant constraints to grading. The site will be graded to provide for the locations of the proposed buildings and layout of the proposed circulation and parking facilities, as shown on Figure 10, Conceptual Grading Plan. The primary objectives of the grading plan are to: a) establish road grades that are consistent with those of the existing City streets around the site; and b) provide stable development pads. All grading plans and associated activities will comply with City, State, and federal requirements. Grading plans will be approved by the City Building, Planning, and Public Works Departments prior to issuance of the grading permit.

E. PUBLIC FACILITIES AND SERVICES

1. WATER PLAN

The City of Upland will provide water service within the Upland Crossing Specific Plan. Metropolitan Water District shall satisfy water service requirements above local capabilities. The water master plan is shown on Figure 11 which shows the proposed pipes. There is an existing loop system with water mains located in Foothill Boulevard, Monte Vista Avenue, 11th Street, and Dewey Way. The project site will be served via the existing water mains from the loop system. In addition, the City has required that a 12-inch parallel line be installed in 11th Street between Dewey Way and Monte Vista Avenue. A portion of this line has been constructed and will be completed with buildout of the project.





Upland Crossing Specific Plan Amendment
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Figure 10
Conceptual Grading Plan





Upland Crossing Specific Plan Amendment
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Figure 11
Water Master Plan



The water system shall be integrated into the design of the bridge at 11th Street so that the facilities meet the requirements of both the Public Works Director and the Development Services Director. All perimeter water facilities shall be publicly maintained and the appropriate easements will be provided as needed.

2. SEWER PLAN

The City of Upland will provide wastewater service within the Upland Crossing Specific Plan. The proposed sewer master plan is shown on Figure 12. All proposed sewer lines within the project site will be a minimum size of 8 inches in diameter, pending confirmation by a sewer analysis, and will run in the private interior street system. The sewer system will be split according to the East and West portions of the project site. All sewer lines located on the west side of the project site will be directed to an 8-inch sewer line located in Monte Vista Avenue and will continue south to facilities being constructed at Arrow Highway. If said facilities at Arrow Highway are not complete at time of construction, the line would continue south in Monte Vista Avenue, cross under the San Antonio Creek Channel and connect to the existing west end interceptor at Huntington Drive (vacated).

Similarly, all sewer lines located on the east side of the project site will be directed to an existing 8-inch sewer line located in Dewey Way. This sewer line will then be connected to an 8-inch sewer line in 11th Street that will flow in an 8-inch sewer line south of Dewey Way, into an 8-inch sewer line in Arrow Highway. It will then be connected to an 8-inch sewer line that leads to an existing 18-inch sewer main at Huntington Drive (vacated). Ultimate sewer line sizing will be determined and approved by the City of Upland Department of Public Works.

3. DRAINAGE PLAN

Development of the project site will increase runoff as a result of creating impervious surfaces such as paved streets, driveways, surface

parking, and foundations for structures. The proposed storm drain system will consist of pipes within the streets and a variety of concrete inlet and outlet structures. Preexisting flows from the north will be collected at the existing culverts at Foothill Boulevard and carried through the project site. Drainage from the west side of the project site will be collected and carried through the site to 11th Street. At 11th Street the drainage shall be conveyed beneath the San Antonio Creek Channel in a 36-inch RCP pipe and outfall into the Chino Basin Water Conservation District recharge basin located on the east side of the Channel, as shown on Figure 13, Drainage Master Plan. A proposed 48-inch stub is proposed on the southeast side of the project site and will drain all storm water from that side of the project site to the existing 72-inch RCP pipe located in Dewey Way. This pipe will then connect to an existing 102-inch RCP pipe south of Dewey Way to Arrow Highway. All backbone storm drain systems shall be publicly owned, operated and maintained. Backbone systems include systems that collect and convey offsite drainage. In addition, all storm drainage facilities shall be consistent with the City’s Master Plan of Drainage. NPDES and WQMP implementation shall be incorporated as required. The Developer and its successors shall be responsible for the operation and maintenance of the storm drain facilities required by the development to meet storm water quality regulations and maintenance of the Chino Basin Water Conservation District (CBWCD) basin directly or indirectly related to storm water discharge from the west side development (TTM No. 18249 and TTM No. 18274). The developer and CBWCD shall not claim any rights to, or associated with, storm water discharge and/or recharge, and all rights to said storm water shall remain with the City of Upland, the development water purveyor. Should the developer be unable to secure an executed third party agreement (CBWCD/Upland/Developer) that is acceptable to the City, the storm drain system to convey storm water from the west side of the development shall be designed to convey storm water flows to the City’s storm drain line in Dewey Way, which is connected to the City of Upland Flood Control and Groundwater Recharge Basin.

4. PUBLIC SERVICES

Required public services and anticipated providers for the Upland Crossing Specific Plan area are listed below in Table 2, Public Service Providers. Southern California Gas Company and Southern California Edison Company shall satisfy the gas and electrical requirements of the project site, respectively. Solid Waste shall be managed by the City of Upland and Burrtec Waste. Each unit shall provide individual storage of solid waste. Verizon shall provide telephone service, and cable service shall be provided by Time Warner Cable. The Upland Police Department and Upland Fire Department shall provide the project site with police and fire protection, respectively.

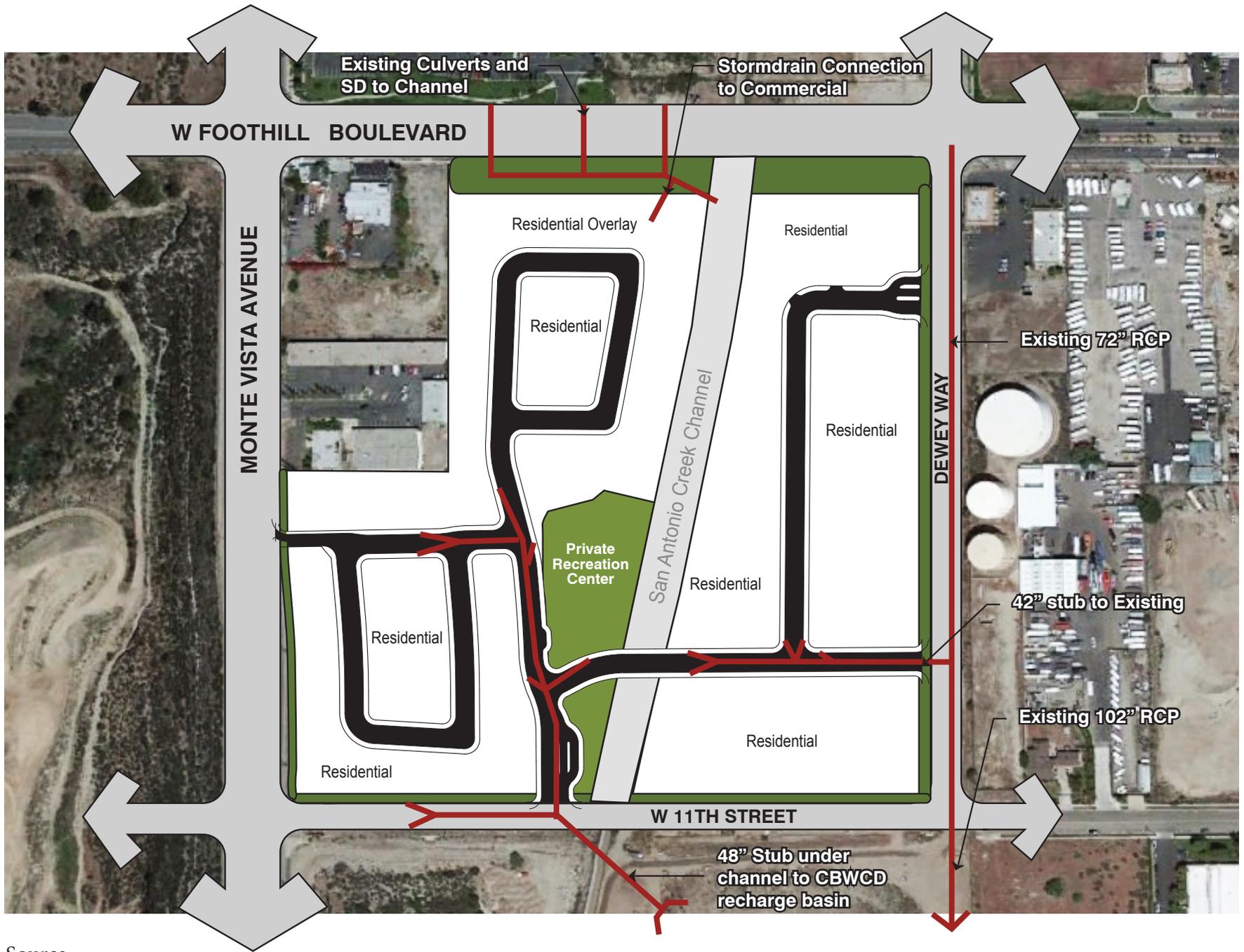
**TABLE 2
PUBLIC SERVICE PROVIDERS**

SERVICE	PROVIDER
Water	City of Upland
Sewer	City of Upland/IEUA
Gas	Southern California Gas Company
Electricity	Southern California Edison Company
Solid Waste	City of Upland / Burrtec Waste
Telephone	Verizon
Cable	Time Warner Cable
Police Protection	Upland Police Department
Fire Protection	Upland Fire Department



Source:
Upland Crossing Specific Plan Amendment
 Upland, CA

Figure 12
Sewer Master Plan



Source:
Upland Crossing Specific Plan Amendment
 Upland, CA

Figure 13
Drainage Master Plan



III. DEVELOPMENT STANDARDS

A. PURPOSE AND INTENT

The development standards contained in this Section shall be enforced to implement the Upland Crossing Specific Plan. These standards and regulations are consistent with the land use goals and objectives of the City of Upland General Plan and provide the basic criteria which will govern all development within the boundaries of the Upland Crossing Specific Plan.

In accordance with California Government Code Section 65860, each City within the state of California must adopt a zoning code which is consistent with the City’s general plan. The City of Upland has adopted a zoning ordinance that is consistent with and serves to implement the City’s adopted General Plan. This Specific Plan is complementary to the zoning code of the City of Upland and is specific to the Upland Crossing Specific Plan property. The standards identified in this Section shall apply only to the Upland Crossing Specific Plan area. These site-specific standards are tailored for this Specific Plan and do not apply outside of the Specific Plan area boundaries.

Development within the Specific Plan area shall be subject to the review processes discussed in Section V of this document. In addition to the Development Standards and criteria identified herein, development within the Upland Crossing Specific Plan shall comply with Title 17 of the City of Upland Municipal Code. Should a conflict arise between a Title 17 provision(s) and any standard(s) or regulation(s) contained herein, the standards and regulations of this Specific Plan shall take precedence.

B. GENERAL PROVISIONS

The Upland Crossing Specific Plan will provide for a mixture of residential, recreational and possible commercial-retail land uses. The following general provisions apply to all development within the Specific Plan boundaries:

- Definitions for terminology used in this Section comply with the definitions contained in the City of Upland Municipal Code, Title 17, Chapter 17.14;
- Rules for interpretation of this Section shall be as provided in the City of Upland Municipal Code, Title 17; and
- All setbacks and heights established by this Section, unless otherwise noted, shall be measured in accordance with the City of Upland Municipal Code, Title 17, Chapter 17.18.

The following subsections set forth the development standards for the land uses of the Upland Crossing Specific Plan. This subsection has been prepared in accordance with California Government Code 65450 and all applicable development and design standards of the City of Upland Municipal Code, with some modification to provide flexibility and encourage innovative use of land within the Upland Crossing Specific Plan area.

C. COMMERCIAL-RETAIL

- (a) Table 3, Commercial Development Standards, sets forth minimum site development standards for commercial uses;
- (b) Permitted and Conditionally Permitted uses in areas designated for development with commercial-retail shall be consistent with the Highway Commercial (CH) zone of the City of Upland Municipal Code, Title 17, Chapter 17.74;



**TABLE 3
COMMERCIAL DEVELOPMENT STANDARDS**

DEVELOPMENT STANDARD	COMMERCIAL-RETAIL
LOT DIMENSIONS	
Minimum Lot Size	0.5 acre
Minimum Lot Width	100 feet
Minimum Lot Depth	100 feet
SETBACKS	
Minimum Front Yard	20 feet
Minimum Side Yard	15 feet
Minimum Rear Yard	20 feet
Minimum Side and Rear Yard Abutting Public Street	20 feet
Minimum Side and Rear Yard Abutting Residential	20 feet
OTHER	
Floor Area Ratio (FAR)	0.25
Maximum Height	40 feet
Minimum Landscape Area	15 percent
Vehicle Parking and Loading Requirements	5 spaces for every 1,000 square feet of gross floor area
Restaurant Parking Requirements	10 spaces for every 1,000 square feet of gross floor area



- (c) The commercial-retail development is intended to serve the neighborhood needs for convenience goods only. The stores are intended to fit into the residential pattern of development and not create either architectural or traffic conflicts;
- (d) Suggested permitted uses in the commercial-retail development include: pharmacy, grocery store, coffee shop, offices, bank, bakery, quick service restaurants and commercial businesses; and
- (e) Prohibited and undesirable uses in the commercial-retail development area include: gas station, laundry mat, dry cleaner, adult-oriented businesses, industrial businesses, employment or training centers. Alcohol sales are allowed subject to City regulations.

D. RESIDENTIAL

1. CLUSTER RESIDENTIAL (12.0 - 20.0 DU/AC)

Product types may include attached and detached units that are oriented in clusters surrounding a private motor court. Access to these units may be from the motor court, or located in common paseos between clusters.

- (a) Table 4, Residential Development Standards, sets forth minimum site development standards for development consisting of Cluster Residential. Graphical views of these development standards are shown on Figure 14, Cluster Residential Typical Plotting;
- (b) Permitted and Conditionally Permitted uses in areas designated for development with Cluster Residential shall be consistent with Table 5, Permitted and Conditionally Permitted Uses (Residential); and

- (c) Model home complexes and/or sales trailers, and associated parking facilities, landscaping, monumentation, and flags shall be permitted within residential planning areas, subject to administrative site plan approval by the City of Upland Planning Department.

2. DETACHED RESIDENTIAL (12.0 - 20.0 DU/AC)

Product types may include small lot single family detached units with access from individual front porches, or through common entry courtyards.

- (a) Table 4, Residential Development Standards, sets forth minimum site development standards for development consisting of Detached Residential. Graphical views of these development standards are shown on the following Figure 15, Detached Residential Typical Plotting. Typical street sections for these residential product types are shown on Figure 16, Typical Street Sections;
- (b) Permitted and Conditionally Permitted uses in areas designated for development with Detached Residential shall be consistent with Table 5, Permitted and Conditionally Permitted Uses (Residential);
- (c) Model home complexes and/or sales trailers, and associated parking facilities, landscaping, monumentation, and flags shall be permitted within residential planning areas, subject to administrative site plan approval by the City of Upland Planning Department; and
- (d) All other uses not identified above shall require approval of a Conditional Use Permit by the City of Upland.



E. RECREATION FACILITIES

Multi-family residential developments shall be provided with the minimum number of major and minor recreation facilities, based on the number of units in the project, as follows:

- 0-25 units: 0
- 26-50 units: 1 major facility, 1 minor facility
- 151+ units: 1 major facility, 2 minor facilities

The selection of recreation facilities in any project shall permit a variety of activities for different age groups. The overall variation of recreation facilities is subject to approval through the development review process, as defined in Section V, Implementation Plan.

1. MAJOR RECREATION FACILITY SPECIFICATIONS

- (a) Tot Lot or Play Area: A required tot lot or play area shall be a minimum of 500 square feet in area, be fully enclosed by low-level fencing at a height of 3.5 feet, and shall have a shaded seating area, drinking fountain and trash container. Play equipment such as climbers, slides, swing sets, play walls, play houses, and/or play sculptures shall be provided. The tot lot shall be placed in a convenient location and provide joint public/private use where feasible.
- (b) Recreation Building: A required recreation building shall be a minimum of 1,200 square feet of usable floor area. An assembly area, restrooms and an activity room shall be provided. The architecture of the recreation building shall be coordinated with the design of the residential project by incorporating similar design features, exterior materials, and colors.
- (c) Pool and Spa: A required pool and spa shall be a minimum of 800 square feet and 80 square feet, respectively. A deck area, fencing, restrooms and enclosed mechanical room for

pool and spa equipment shall be provided. Use of a solar water heating system is encouraged to be provided as the primary water heating source.

2. MINOR RECREATION FACILITY SPECIFICATIONS

The following amenities qualify as a minor recreation facility:

- Community garden
- Concrete basketball court
- Sand volleyball court
- Sports court
- Bocce court
- Paddle tennis court
- Tennis court
- Handball court
- Barbecue facility with seating and shade structures
- Water feature with seating area; should be usable
- Large shade area structure, such as gazebo with seating
- Spa
- Other facilities as may be approved through the development review process

A major recreation facility shall qualify as two minor recreation facilities, subject to approval through the development review process.



**TABLE 4
RESIDENTIAL DEVELOPMENT STANDARDS**

DEVELOPMENT STANDARD	CLUSTER RESIDENTIAL (12.0 - 20.0 DU/AC)	DETACHED RESIDENTIAL (12.0 – 20.0 DU/AC)
MINIMUM BUILDING SETBACKS		
Front to Front (Living Area to Living Area)	10 feet	N/A
Front to Street (Building to R.O.W.)	8 feet	8 feet
Side to Street (Building to R.O.W.)	8 feet	8 feet
Side to Side (Building to Building)	6.5 feet	6.5 feet
Rear Yard (Building to Property Line)	N/A	5 feet
Garage Door to Drive Court/Street	1.5 feet	8 feet
Garage Door to Garage Door	28 feet	N/A
PERIMETER SETBACKS		
Foothill Boulevard (Property Line to Building)	15 feet	8 feet
Monte Vista Avenue (Property Line to Building)	8 feet	8 feet
11 th Street (Property Line to Building)	8 feet	5 feet
Dewey Way (Property Line to Building)	8 feet	5 feet
San Antonio Creek Channel (Building to R.O.W.)	8 feet	10 feet
OTHER		
Maximum Building Height	45 feet	45 feet
Minimum Unit Size	1,000 sq. ft.	1,650 sq. ft.
Private Parking Requirement ¹	2 spaces per unit (plus 0.5 guest space per unit)	2 spaces per unit (plus 0.5 guest space per unit)
Private Recreation Center Parking	On-street parking within project	On-street parking within project
Common Open Space	100 sq. ft. per DU	100 sq. ft. per DU
Private Open Space ²	100 sq. ft. per DU	175 sq. ft. per DU
Mechanical Equipment ³		
Architectural Projections ⁴	3 feet	2 feet

**TABLE 4
RESIDENTIAL DEVELOPMENT STANDARDS
(CONTINUED)**

DEVELOPMENT STANDARD	CLUSTER RESIDENTIAL (12.0 - 20.0 DU/AC)	DETACHED RESIDENTIAL (12.0 – 20.0 DU/AC)
WALLS / FENCES		
Site Perimeter Retaining Walls ⁵	10 feet from top of wall to footing	10 feet from top of wall to footing
Freestanding Property Line Walls/Fences	6 feet	6 feet
Combination Property Line Walls/Fences	9 feet	9 feet
<p>¹ Private parking spaces are in the form of a garage. Tandem garages are allowed.</p> <p>² Porches and balconies may be included in this calculation. There shall be a one-sided dimension of 8 feet minimum.</p> <p>³ Mechanical Equipment includes air conditioning units, water tank closets, roof-mounted satellite equipment, etc. Screening will be integrated into architecture.</p> <p>⁴ Any architectural feature that projects from the building and seeks to enhance the structure may encroach into the setback area provided that there is always 3 feet of clearance around the projection. Projections shall not encroach into the required clear space for Fire Department Access (26 feet garage door to garage door). Examples of architectural projections include, but are not limited to, pilasters, chimneys, pop-outs, belly bands, roofs, covered porches patios, etc.</p> <p>⁵ Landscaping consisting of hedges, vines, and tall shrubs shall be utilized to reduce the massing of tall perimeter retaining walls.</p>		
<p>Note: Same development standards apply to the residential overlay.</p>		

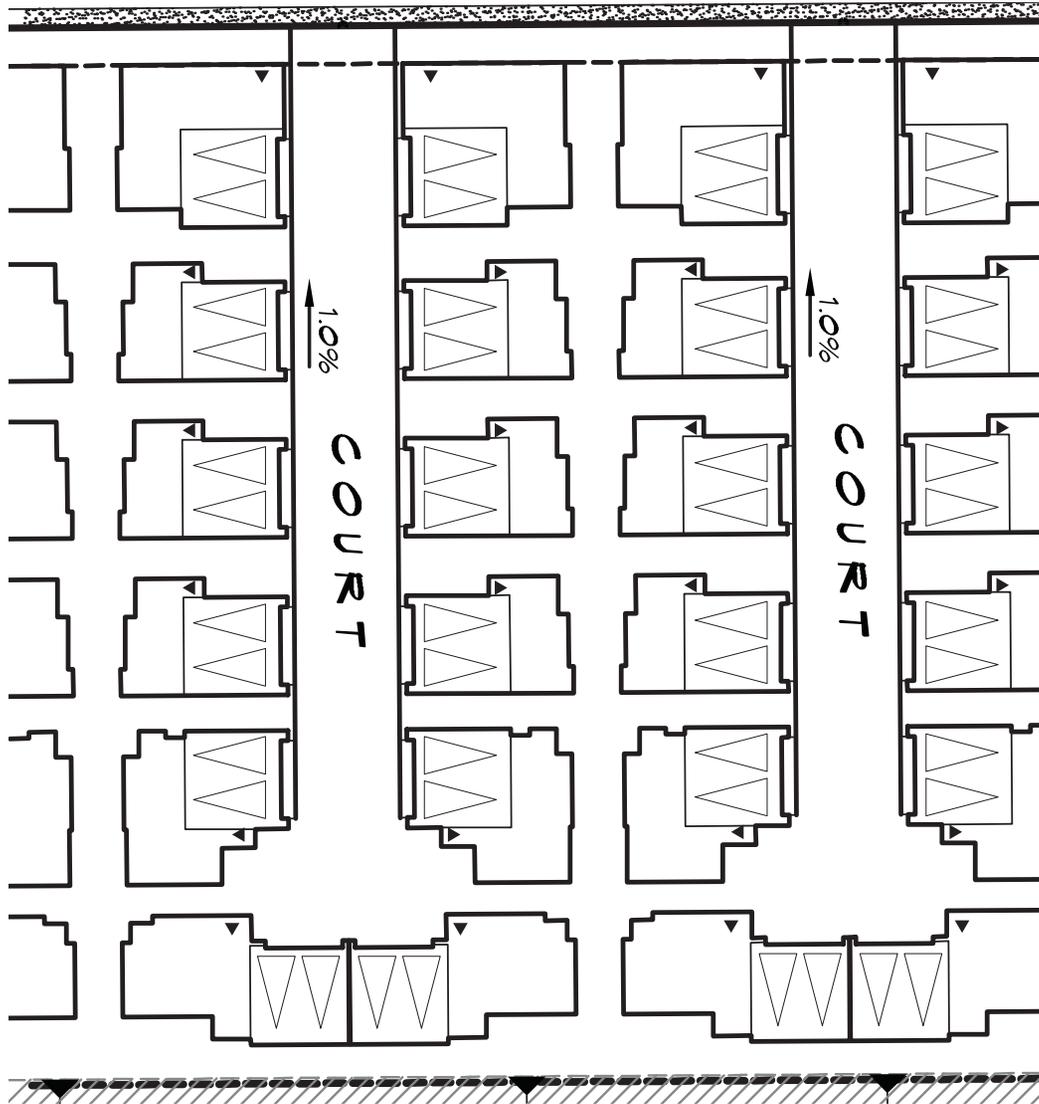


**TABLE 5
PERMITTED AND CONDITIONALLY PERMITTED USES
(RESIDENTIAL)**

LAND USE	CLUSTER RESIDENTIAL ⁶ (12.0 - 20.0 DU/AC)	DETACHED RESIDENTIAL (12.0 - 20.0 DU/AC)
RESIDENTIAL		
Child Day Care ¹	P	P
Home Occupations ²	P	P
Multi-Family Housing ³	P	P
Residential Accessory Uses and Structures ⁴	P	P
Single Family Homes	P	P
Duplex	P	P
Temporary Dwellings Construction Trailer Construction Guard Facility	P	P
EDUCATION, PUBLIC ASSEMBLY, AND RECREATION		
Residential Recreation Facilities	P	P
COMMUNICATION FACILITIES		
Wireless Facilities ⁵	C	C
Satellite Dishes/Antennas	P	P
¹ Subject to applicable State and Local requirements, permits and approvals. ² Subject to Chapter 17.124 of the City of Upland Municipal Code. ³ Multi-Family Housing is defined as 3 or more attached products. ⁴ Subject to Title 17 of the City of Upland Municipal Code. ⁵ Wireless Facilities require approval pursuant to Upland Municipal Code requirements. ⁶ Cluster units are typically organized around a common street or drive aisle, with pedestrian access from the street or by paseos between the clusters.		

SYMBOL	APPLICABLE PROCESS
P	Permitted Use by the City of Upland
C	Conditional Use – Approval required by the City of Upland
“Blank”	Use not allowed

NOTE: Same uses apply to the residential overlay.

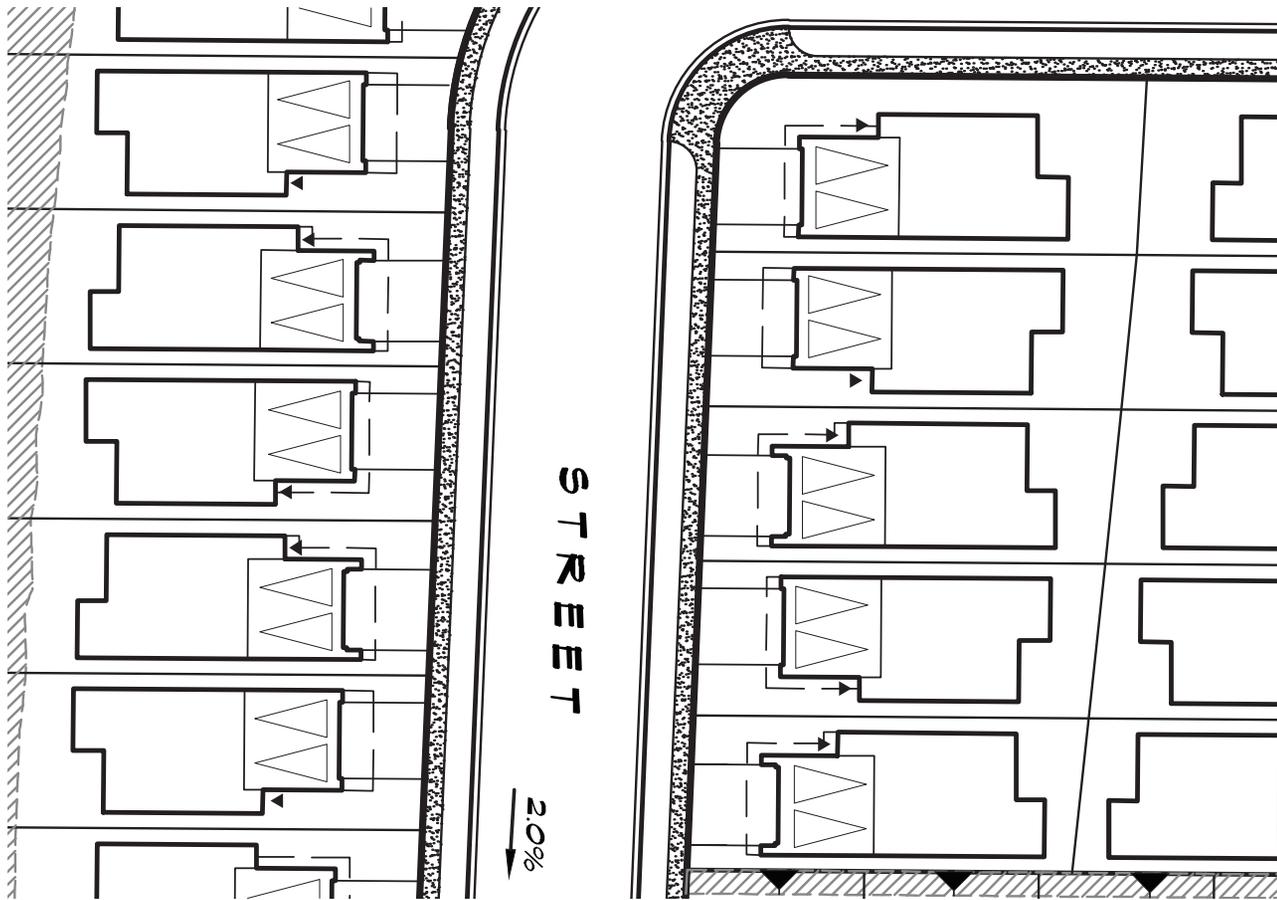


Summary

Cluster homes are two or three story single family homes with two car garages. They may be plotted in groups of up to 12 units per cluster. Cluster homes may have attached garages. Front entries may be on the motor court or a common greenbelt.

Source: TMAD Taylor & Gains

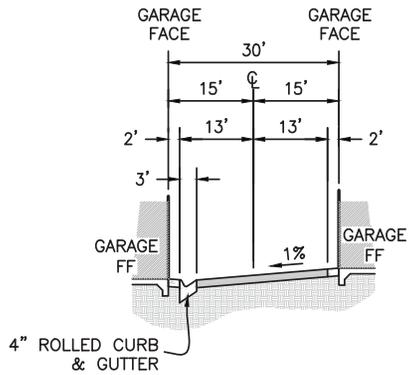




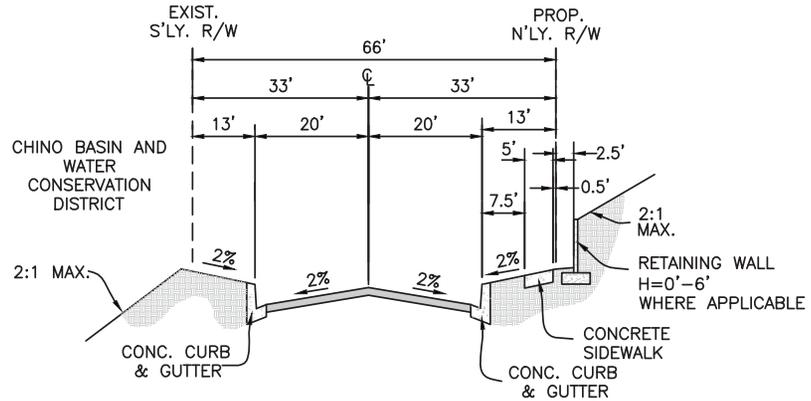
Summary

Detached single family homes are one to two story homes with private backyards.

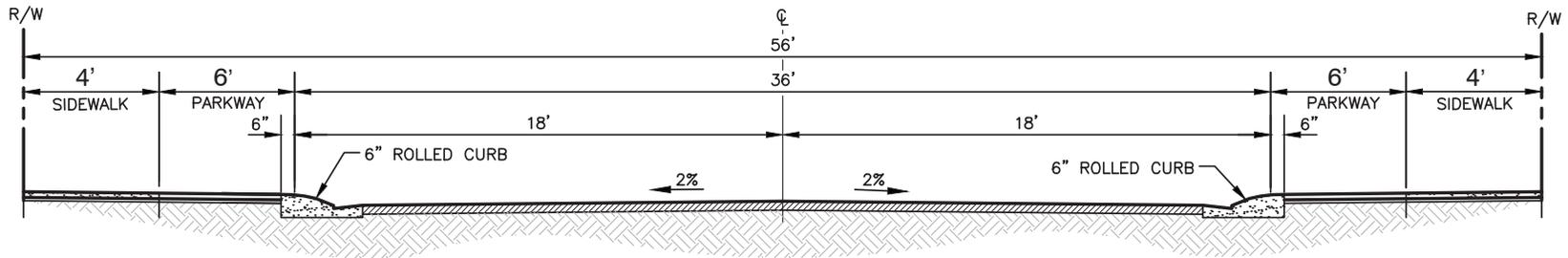
Source: TMAD Taylor & Gains



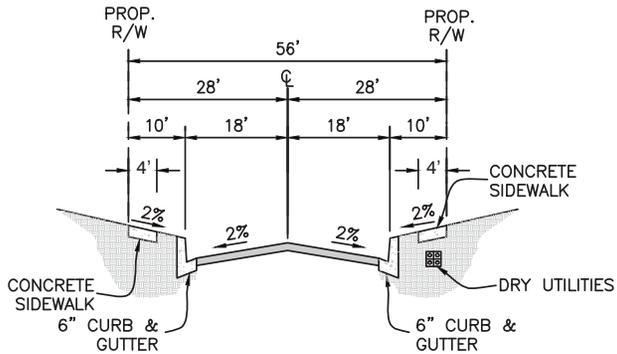
TYPICAL MOTORCOURT STREET SECTION



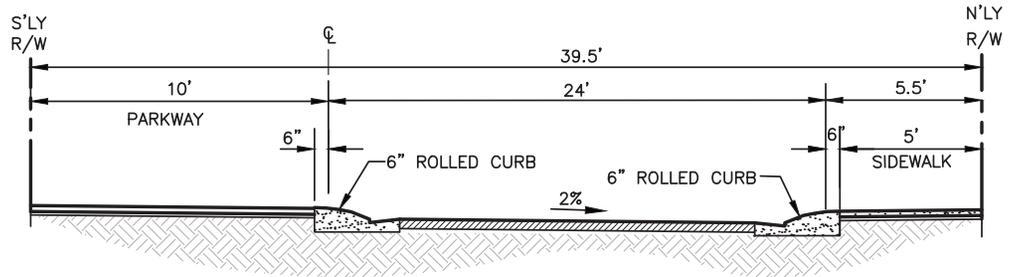
TYPICAL 11TH STREET SECTION



TYPICAL RESIDENTIAL STREET SECTION (ROLLED CURB)



TYPICAL PRIVATE STREET SECTION (VERTICLE CURB)



**TYPICAL STREET SECTION
(GATED EXIT ONLY TO MONTE VISTA AVE. & DEWEY WAY)**

Source: TMAD Taylor & Gains



IV. DESIGN GUIDELINES

A. PURPOSE AND INTENT

The Upland Crossing Design Guidelines provide a general framework for architecture, landscaping, entry monumentation, and signage in order to ensure a high-quality development. The overall goal of the Upland Crossing Design Guidelines is to create a development that will build a strong character for the area and provides an aesthetically pleasing gateway for the City of Upland. These guidelines are intended to provide criteria for design, while allowing flexibility for architects, landscape architects, developers, builders, and others involved in the development of the Upland Crossing Specific Plan. Through collaboration and cooperation between the master developer, the builder(s), and the City of Upland, the development of the Upland Crossing Specific Plan is destined to be a successful and desirable addition to the Upland community. These Design Guidelines are in addition to the existing City Design Guidelines, to the extent reasonably possible to accomplish the goals of this Specific Plan and City of Upland.

The specific objectives of the Design Guidelines are to:

- Provide guidance to builders, engineers, architects, and landscape architects, and other professionals in order to obtain the desired design quality envisioned for the Upland Crossing Specific Plan by the master developer and the City of Upland;
- Provide the City of Upland with the necessary assurances that the Upland Crossing Specific Plan will be developed in accordance with a certain design quality and character as set forth in this document;

- Provide guidance to City staff, the Planning Commission, and the City Council in the review of future development projects in the Specific Plan area;
- Reinforce a design theme through the creation of an internal circulation system, an interconnected network of pedestrian focal nodes, plazas, and public spaces unified by an alluring hardscape and landscape theme;
- Emphasize internal continuity among the five planning areas in the Upland Crossing Specific Plan area;
- Maintain sensitivity to adjacent uses through adherence to these design guidelines by establishing setback, building orientation, signage, and lighting guidelines; and
- Provide design guidelines which permit the Upland Crossing Specific Plan area to develop its own theme and character while allowing it to interface with and respond to the character and design fabric of adjacent areas.

The Design Guidelines are intended to be flexible and work in concert with the Development Standards contained in Section III of this document. However, it is critical that these guidelines are followed in a manner consistent with the established design theme in order to create a unified concept while providing the opportunities for diversity and visual interest, which are so apparent in the most successful developments in southern California.

This Specific Plan document recognizes that the development of the Upland Crossing Specific Plan area will play a vital role and serve as a visual gateway into the City of Upland. This document and the design guidelines contained herein support the City’s planning concept and expands upon the goals outlined in the Upland General Plan for this site. Taking into account the unique project location, the remaining existing

uses, and the site's topographic and elevational differences, an overarching design concept has been consistently applied throughout the project to establish a unified development.

The photographs, sketches, and other graphic representations in these Design Guidelines are offered as general visual aids in understanding the basic intent of the design theme and its key implementing elements. The design components presented in this document are provided for informational purposes and are to be used in identifying the desired design composition within the Upland Crossing Specific Plan. Design components are provided as a palette of character, materials, and colors defining elements that should be reflected in the residential and possible commercial-retail design proposals.

B. DESIGN THEME

The design fabric envisioned for the Upland Crossing Specific Plan will strive to support a wide array of architectural styles that reflect a blend of the traditional and contemporary built environment. The Upland Crossing Specific Plan is a project that will incorporate conditions appropriate for this growing community. The projected conditions of the area must be taken into consideration when implementing these Design Guidelines. The Design Guidelines provided by the Upland Crossing Specific Plan are encouraged for application to new structures, while providing general architectural, landscape, and hardscape treatments that will complement the future facilities and entire development area into a cohesive community.

C. ARCHITECTURAL DESIGN GUIDELINES

The purpose of establishing Architectural Design Guidelines is to establish the development criteria at the individual house level and possible commercial-retail level to assure a unified environment. The intent is not to require rigid adherence to a particular architectural style

on each and every structure or to limit the range of materials or colors used throughout. More correctly, these guidelines should be used to achieve continuity and establish a larger visual context.

The selection of an architectural style is meant to fulfill two specific goals. The first goal is to establish a sense of place within the City of Upland. This can be accomplished by utilizing a set of architectural styles within which all structures, signage, and lighting will be complementary. Each set element of the project will thus have an identifiable relationship with the total designed environment. The second goal is to respond to the orientation and the warm semi-arid nature of the area. Attractive, durable, high quality materials with sensible attention to maintenance will be used. The following is a description of the architecture proposed for the project site.

1. COMMERCIAL-RETAIL WITH RESIDENTIAL OVERLAY

a. Architectural Style

A style highlighting the historical significance of the area is anticipated for the overall architectural theme in the possible commercial-retail portion of the Upland Crossing Specific Plan area. All commercial-retail buildings, whether inline or freestanding, shall utilize elements complementary to the selected style, while encouraging creative and unique designs in order to provide variation and aesthetic interest in the Upland Crossing Specific Plan. Conceptual elevations and images of the proposed architectural styles are provided on Figure 17.

b. Architectural Details

The architectural details listed below provide general design components that should be reflected in the design character of



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Figure 17
Conceptual Imagery - Commercial Retail



the commercial-retail area of the Upland Crossing Specific Plan. If an alternate architectural style is selected, then design elements appropriate to that style shall be used.

- Articulation of building elements both vertically and horizontally;
- Accentuated entries;
- 6:12 to 12:12 roof pitch with flat concrete tile;
- Dormer accents where appropriate;
- Warm, light to dark earth tone color palette; and
- Tower elements at entries and focal points.

c. Building Orientation

The orientation of buildings within a project of this scale will play an important role in the successful design of the Upland Crossing Specific Plan. From a site planning perspective, Foothill Boulevard has been identified as a high visibility area. This location is prominent, but reflects a strong statement about the character of the Upland Crossing Specific Plan community. Therefore, the design of commercial-retail uses should provide defined entrances and entry access points that are easily located from a distance by pedestrians and/or vehicular traffic.

d. Building Design, Form and Mass

1. Architecture should be comprised of simple and distinct masses. Masses should be expressed with clean, sharp edges and corners. A simple hierarchy should be established among the various masses;
2. Architectural elements and accessories should be provided on the building mass, including but not limited to, arcades, balconies, towers, colorful banners, and decorative lighting features. Building facades should include a distinct base,

middle and top with variation in materials and colors, as well as articulation;

3. Building materials should be durable and convey a sense of permanence. The textural and material quality of a building's facade is important. Traditional materials including stone, brick, concrete, precast concrete, metal, block, and stucco should be used creatively to provide a sense of permanence;
4. Buildings should project a positive public image, enhancing the function and aesthetics of the project by integrating buildings with landscape and/or structural elements;
5. Provide variation in plan, form, and function of buildings, which will result in adjacent spaces both inside and out through the uses of recesses, pop-outs, positioning, and relationships of buildings;
6. Avoid monotony and repetition in building elevations and the street scene by varying building heights, massing, and rooflines, design elements, color, texture and materials, building placement, and landscape;
7. Avoid long, uninterrupted building planes by designing in smaller components, varying massing and/or facade treatments as a series of individual units, providing overhangs or shadow elements for shade and interest; and
8. Articulate all building elevations that are visible from public or private streets, providing visual interest with window patterns, size, and placement, and integrate overhangs and other external elements into the overall building design.

e. Parking/Vehicular Circulation Guidelines

1. Adequate areas for maneuvering, loading, and emergency vehicle access should be accommodated on site; and
2. Parking areas should be arranged to minimize conflicts with commercial loading activities. Commercial loading should generally be located to the side and rear of buildings. Commercial buildings along Foothill should have parking areas that are located behind or to the side of buildings and landscaping and should be screened from street view.

f. Service, Loading, and Equipment Areas

1. Storage for supplies, merchandise, equipment, and similar materials are not permitted outside the buildings;
2. Service, loading, and storage areas should be separated from pedestrian and private automobile circulation;
3. Service areas, loading docks, and equipment areas should be screened from views either by locating these uses within a building or by screening them with landscaping, walls, fences, or other architectural treatments; and
4. All utility equipment, mechanical equipment, tanks, ducts, transformers, downspouts, and other similar equipment should be screened from view from all pedestrian, vehicular (where appropriate), and open space vantage points and should be graffiti- and vandal-resistant.

g. Walls and Fencing

1. Walls, except for walls used to enclose loading docks and trash enclosures, should not be more than six (6) feet in height. They should be constructed of light colored stucco, concrete, or masonry. Fences should be used only as part of gated areas or as an extension of walls;
2. Chain-link fencing with barbed wire, large blank and flat wall surfaces, and exposed untreated concrete block walls shall be prohibited; and
3. Walls, fences, and gates should complement the adjacent architecture and be accented with pilasters. All block walls should be constructed with decorative block. Walls over 3.5 feet in height should not be located within the front or street-side setback area.

h. Outdoor Lighting

Lighting must be considered with the architectural theme as well as for safety purposes. Lighting fixtures and standards can define the quality appearance of developments and help identify focal areas. As a basic guideline, lighting should comply with the City of Upland Municipal Code, as well as the following:

1. All outdoor lighting, including spotlights, floodlights, electrical reflectors, and other means of illumination for signs, structures, landscaping, parking, loading, unloading, and similar areas should be focused, directed, and arranged to prevent glare and illumination on streets or adjoining property;
2. Low intensity, energy conserving night-lighting is preferred;

3. No freestanding lighting fixtures should exceed 36 feet in height except in parking lots;
4. Service area lighting should be contained within the service yard boundaries and enclosure walls. No light spillover shall occur outside the service area. The light source shall not be visible from the street;
5. No specific design is prescribed and variations in treatments should be allowed; however, light fixtures and standards should be coordinated throughout the Upland Crossing Specific Plan community;
6. Parking lot lighting should provide adequate illumination for the safety of visitors while minimizing glare into adjacent property and uses;
7. Energy conservation, safety, and security should be emphasized during the designing and siting of project lighting;
8. Light standards should blend architecturally with buildings, pedestrian areas, other hardscape elements, and street furniture;
9. Parking areas should use lighting standards and fixtures that are consistent with and a continuation of the character of the project site's structures; and
10. Exposed neon tubes in signage should not be permitted and should be hidden from view.

i. Refuse Containers and Storage

1. Refuse enclosures and equipment should be easily accessed by service vehicles with enclosures and equipment located within a building's facade or within a screened enclosure. The enclosure's design should reflect the architectural style of adjacent buildings and use similar, high-quality materials. Landscaping or trellis work should screen enclosures visible from a street or connecting walkway and should be permanently maintained;
2. Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties; and
3. Cart storage areas should be integrated within the initial building and site design.

j. Property Owners Association

A master property owners association (POA) shall be created for the commercial-retail area in order to preserve and enforce the goals and objectives of this commercial development and the City of Upland. The POA shall be responsible for the maintenance of all buildings, onsite utilities, common areas and perimeter landscaping within the commercial-retail development. The POA shall process a full set of CC&R's, which are not part of this Specific Plan document, through Development Services and the City Attorney's Office in conjunction with the processing of the project's final map.

*Note: If the residential overlay is exercised, the following residential design guidelines will be in effect.

k. Master Sign Program

Commercial applicants shall prepare a master sign program as required by the City Zoning Code.

2. CLUSTER RESIDENTIAL

Residential dwelling units of the Upland Crossing Specific Plan may consist of attached or detached cluster product. One of the goals of the Upland Crossing Specific Plan is to create a high quality infill residential development that provides a variety of homes and lot types. The Specific Plan will meet this goal by incorporating several home and lot styles, a diversity of architectural style, smooth transitions between structures, and enhanced neighborhood hardscape and landscape features. Upon completion, the residential area should encourage neighborhood interaction and integration within the overall development of the Upland Crossing Specific Plan area.

It is the intent for all residential architecture to achieve a high level of quality in building function and visual appearance, assure variety and compatibility in architectural character, and to enhance the community’s overall value. The goal is to promote these qualities in conjunction with the landscape and site planning by using heritage styles combined with modern technology and architectural innovation to provide a pleasant, livable community.

a. Architectural Style

The residential architectural styles will be characterized by specific style elements that are addressed in detail in this section. A sampling of elements is required in order to appropriately represent the designated architectural style; however, not all of the styles are required. Additional styles are allowed, if approved by the City of Upland Planning Department. If another style is proposed, the architectural style should be clearly defined as part of the entitlement application

process, including common architectural features within the style. Proposed building types should be appropriate to the architectural style and meet all other applicable standards and design guidelines provided in the Specific Plan.

Another important goal of the Upland Crossing Specific Plan community is to develop varied and interesting street scenes. In order to achieve this goal, an architectural palette has been selected that represents the following selected architectural styles:

- Spanish Colonial**
- Collegiate**

1. Spanish Colonial

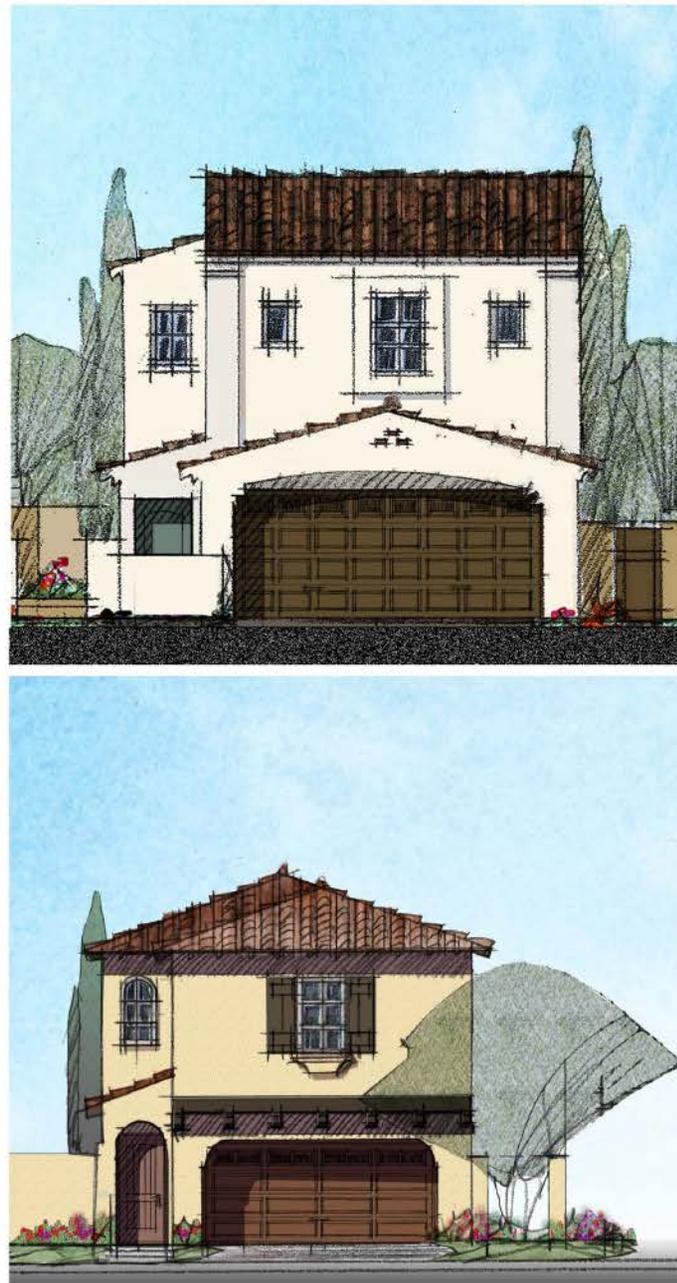
The Spanish Colonial architectural style originated from the Spanish colonization period, when Hispanic-influence vernacular in America encompassed not only visions of distant Spain and its rich history represented by villages and villas enhanced by Moorish and Renaissance influences, but also inspiration derived from adobe churches, missions and ranches built by early settlers.

The style is of modest detailing with formal ornamentation and interests focused at entries and prominent wall planes. The characteristic of this style is the use of simple forms, porches, courtyards, arched colonnades, thickened walls, plain plaster wall surfaces, recessed windows and low pitched roofs with barrel tiles.

Table 6 summarizes the Spanish Colonial design elements, Figure 18 shows the architectural facades, and Figure 19 includes examples of photographed elements.

**TABLE 6
SPANISH COLONIAL DESIGN**

ELEMENT	MINIMUM REQUIREMENTS	
FORM	<ul style="list-style-type: none"> • Predominant gable or shed roof forms with some simple hip roof forms • One story and two story masses 	
ROOF	<ul style="list-style-type: none"> • Low pitched roofs, 3:12 – 4.5:12 • Concrete “S” tile (various blends) • Stucco molded eave or exposed shaped rafter tails 	<ul style="list-style-type: none"> • Flush roof rake • Tight to 9” overhangs
WALLS	<ul style="list-style-type: none"> • Stucco with sand finish 	
WINDOWS	<ul style="list-style-type: none"> • Vertical windows at front elevations • Divided lights at front elevations 	<ul style="list-style-type: none"> • All windows recessed • Some arch top windows
DETAILS	<ul style="list-style-type: none"> • Stucco trim at entries • Stucco trim at window heads, jambs or sills • Stucco corbels at openings • Raised panel shutters 	<ul style="list-style-type: none"> • Wrought iron and wood potshelves • Stucco gable end vents • Themed exterior light fixtures at entries, pedestrian courts, and auto courts
COLORS	<ul style="list-style-type: none"> • Light to mid-toned wall colors with contrasting rich trim and accents 	
TYPICAL ELEVATION	Refer to Figure 18.	



Source: Angeleno Associates, Inc.

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Figure 18
Spanish Colonial Architectural Style



Source: Angeleno Associates, Inc.

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Figure 19
Conceptual Imagery - Spanish Colonial Architectural Style



2. Collegiate

Collegiate architecture was popularized by numerous prominent architects trained in the Beaux Art tradition punctuated by the use of axis and formal architectural detailing. This vernacular is found in many century old respected California college campuses and public facilities. These early structures strived for residential scale and proportion resulting in restrained roof lines and form that stood the test of time. The facade conveys a prestigious character without being too ostentatious. Expressions from shadows cast by the strong Southern California sun allows subtle surface relief and plane offsets to accentuate simple plan shapes and forms.

Although derived from Mediterranean and Italian origins, many complicated ornamentations were filtered and refined, which resulted in clean, understated and elegant structures. Its success relies on restraints and simplification in design.

Emphasis on a few materials and balance composition of windows, walls and doors are the hallmark of this style. Tone-on-tone plaster, modeled barrel tile roofs, classical surrounds and entry embellishments respond well to the local climate and are forgiving enough to patina with time. These architectural elements are summarized in the following Table 7, architectural facades are shown on Figure 20, and photographed examples are shown on Figure 21.

**TABLE 7
COLLEGIATE DESIGN**

ELEMENT	MINIMUM REQUIREMENTS	
FORM	<ul style="list-style-type: none"> • Simple hip and gable or shed roof forms • One story, two story masses and occasional tower • Symmetrical and formal arrangement of masses 	
ROOF	<ul style="list-style-type: none"> • Low pitched roofs, 3:12 – 4.5:12 • Concrete “S” tile (various blends) • Stucco molded eave 	<ul style="list-style-type: none"> • Flush roof rake • Scalloped details at rake as accent at prominent gable • Tight to 9” overhangs
WALLS	<ul style="list-style-type: none"> • Stucco with sand finish 	
WINDOWS	<ul style="list-style-type: none"> • Symmetrical composition of windows • Vertical windows at front elevations • Divided lights at front elevations 	<ul style="list-style-type: none"> • All windows recessed • Some arch top windows
DETAILS	<ul style="list-style-type: none"> • Stucco trim at entries • Stucco surround trim at prominent windows • Stucco corbels at openings • Raised panel shutters 	<ul style="list-style-type: none"> • Wrought iron and wood potshelves • Stucco gable end vents • Themed exterior light fixtures at entries, pedestrian courts, and auto courts
COLORS	<ul style="list-style-type: none"> • Light to mid-toned wall colors with contrasting rich trim and accents 	
TYPICAL ELEVATION	Refer to Figure 20.	



Source: Angeleno Associates, Inc.

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Figure 20
Collegiate Architectural Style



Source: Angeleno Associates, Inc.

Upland Crossing Specific Plan Amendment
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Figure 21
Conceptual Imagery - Collegiate Architectural Style



b. Plan and Style Requirements

1. Provide at least three (3) building plans per neighborhood, with the ability to reverse plot plans and vary placement or add elements to corner units;
2. Provide a minimum of two (2) individual unit plans per building. Individual unit plans may be repeated between building plans; and
3. Provide at least three (3) different, yet compatible color schemes for each neighborhood. Individual building colors should be selected from the color palettes selected for that style.
4. Provide variation in the placement of front elevations so as to vary the building setbacks along a block.

c. Building Orientation

Creating street scenes that function well and have visual interest are primary community objectives. The following basic elements and criteria are intended to develop variations in appearance and a sense of individuality for each home, and are conceptually shown on Figure 22, Typical Street Elevations: Cluster Residential. Neighborhoods that have nearly identical homes and streets without variation in product placement and form will not be approved in the design review. This section lists architectural plotting techniques that work together to create a quality environment.

1. Buildings should relate appropriately to surrounding development and streets and create a cohesive visual identity for the neighborhood and street scene. There

should be cohesiveness with building scale, massing, and architectural elements with surrounding development;

2. Public spaces should be clearly defined from private spaces through the use of building materials, grade separations, and physical separations such as fences, walls, or building enclosures;
3. Provide variation in floor plans, massing forms and elements on facades;
4. Design simple floor plans and building masses using the correct proportion and scale for the selected styles in mind;
5. Adjacent and facing residences having the same front elevations are prohibited. However, reversed footprints with variation in elevation design and color/material application are permitted;
6. The upper story portion of elevations of all homes that back or side onto streets, recreation areas or other public space must include a variety of window treatments, roof projections, etc. to provide variety on these elevations; and
7. Provide variation in floor plans and setbacks to reduce the massing effect on the paseo and create visual interest for pedestrians.

d. Building Mass and Scale, Form, and Height

The roof forms and pitches and heights of various architectural elements should conform to the individual styles selected for the Upland Crossing Specific Plan. The following guidelines should be followed:



Source: Angeleno Associates, Inc.

1. No building shall exceed a height of three stories, with a loft or bonus. An architectural projection such as a chimney or tower may exceed the maximum building height, but shall not exceed a height of 45 feet;
2. Long, unbroken facades and box-like forms should be avoided. Building facades should be broken up to give the appearance of a collection of smaller structures;
3. To the extent possible, each of the units should be individually recognizable. This can be accomplished with the use of balconies, setbacks, and projections which help articulate individual dwelling units or collections of units, and by the pattern and rhythm of windows and doors;
4. Elevations visible from public or private streets should be carefully articulated and large unbroken surfaces should be discouraged;
5. Emphasis should be placed on horizontal architectural lines including trim and fascia lines to make tall buildings appear wider;
6. Buildings should be designed to define outdoor spaces, with floor plans that have a logical and functional relationship between indoor spaces and outdoor areas; and
7. Single-story elements are encouraged on all buildings to establish pedestrian scale and add variety to the street scene.

e. Shade and Shadow – Building Relief

1. Project a front door image and access to the street front or paseo. Added articulation of end unit elevations is

encouraged to achieve four-sided articulation;

2. Place and access garages from drive courts to separate pedestrians from vehicular areas;
3. Provide front porches where style appropriate and possible to accomplish stepped massing and transition to public spaces;
4. Vary setbacks on building elements/facades; and
5. Roof pitch should be authentic to the architectural style.

f. Roofs and Chimneys

1. A variety of roof styles should be permitted, including hip, gable, and shed. The roof design should follow the wall planes and roof pitch should be consistent with the architectural style. Flat roofs shall be prohibited;
2. Roofs should be constructed of non-combustible materials. Acceptable materials include clay or concrete tile. Unacceptable materials include pressed wood, corrugated fiberglass, asphalt roll roofing, and corrugated metal;
3. Roofing trim materials should be of similar materials and complementary colors. Roof vents and appurtenances should be painted to match either the body or trim of the building;
4. Roof overhangs should be typical of the architectural style and should be designed as an integral part of the roof form. Awnings shall be permitted if true to the architectural style; and

5. Fireplaces are typically gas appliances, not wood-burning, and therefore, chimneys are not required in a functional sense. Chimneys may be included as an architectural element. If included, they shall be constructed of veneer or stucco.

g. Entries and Windows

1. Window pediments, small roof elements, overhangs, and projections over windows, doors, and garage doors should be incorporated into the architectural detailing; and
2. A variety of window and door treatments should occur. French doors, dormer windows, and decorative shutters should be incorporated into the building design.

h. Balconies and Porches

1. Balconies and porches should be used to articulate and reduce mass, as well as to provide shadow relief. Where architectural style and density are appropriate, stoops may be used instead of porches;
2. Porches, balconies, and trellis structures should be compatible with the overall architectural theme, style, and design of the structure; and
3. Materials used to construct balconies and porches should be appropriate to the designated architectural style. Where balconies are proposed, facades should vary in size and plane.

i. Details

1. Potshelves, pilasters, and brick and tile accents are

encouraged around doors, windows, and entries, particularly near front and side entries that are easily visible from the street;

2. Low and high walls should be used to define courtyards, patios, and entries;
3. Mechanical equipment such as gas meters and air conditioning and heating units should be screened from public view, either by landscaping, fences, and walls, or combination thereof; and
4. Rain gutters, downspouts and other architectural elements constructed of sheet metal should be painted with complementary colors to the building, either matching the color of the trim, fascia or stucco, depending on its location.

j. Materials and Colors

The colors and materials used in the Upland Crossing Specific Plan should reflect a general theme of warm, sunwashed colors that are in harmony with the surrounding region’s environment and support the community’s Spanish character. The architectural style palette selected for this community contributes to this goal as they all share this warm, earthy use of materials and colors that are indigenous to the region. Stone and other veneers that are indigenous to the architectural style should be used.

A variety of natural looking materials and colors will provide the diversity required for visual interest while supporting the community character and creating a timeless appeal. The primary purpose of the architectural color palette selection is to avoid monotony, provide a variety of colorful schemes and

promote visual diversity. To further this goal of diversity, the following criteria shall be met:

1. Individual color schemes must be appropriate to the architectural styles with a harmonious selection of accent materials, roof profiles and colors;
2. A minimum of three color schemes shall be provided for each building;
3. Each building shall have a minimum of three colors: body, trim, and accent;
4. Each defined color scheme shall include a different roof color per building; and
5. No three adjacent buildings in a row shall have the same repeating color scheme.

k. Garage Doors and Driveways

1. Garage doors should be of complementary colors, textures, and materials used on the front building elevation; and
2. Automatic, sectional, roll-up garage doors shall be required.

l. Garage Placement and Treatment – Drive Court

The home and front yard should be the primary focus of the front elevation. All garages will be accessed at the drive court in order to create an attractive street scene.

m. Garage Types

1. Drive Court Garage: This garage type allows the resident to enter their garage through the drive court, thereby eliminating the front-facing garage and driveway.

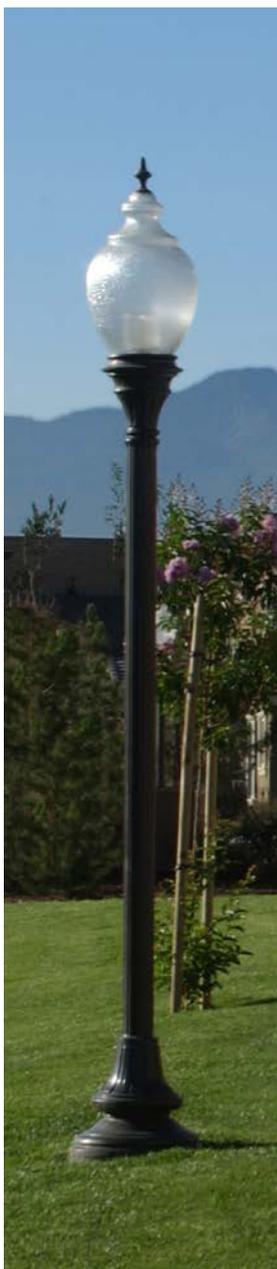
n. Lighting

1. All street lighting shall be nostalgic luminaire post top light fixtures, also referred to as decorative acorn-shaped SCE street lights, as shown on Figure 23. All street lighting shall conform to CalGreen and City standards or an approved theme lighting program, and shall be approved by the City Engineer.

o. Homeowners Association

A master homeowners association (HOA) shall be created for the Upland Crossing Specific Plan area in order to preserve and enforce the goals and objectives of this residential development and the City of Upland. The HOA shall be responsible for the maintenance of all perimeter landscape, walls, lights, interior streets, onsite utilities and common areas within the Specific Plan. The HOA shall process a full set of CC&R’s, which are not part of this Specific Plan document, through Development Services and the City Attorney’s Office in conjunction with the processing of the project’s final map.

In addition, a separate HOA shall be created for the cluster residential development which shall be responsible for building maintenance only. A POA shall be created for commercial-retail Planning Area 1.



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Figure 23
Acorn Light



3. DETACHED RESIDENTIAL

Residential dwelling units of the Upland Crossing Specific Plan will consist of detached residential product. Detached residential, like cluster residential previously described, will incorporate several home and lot styles, a diversity of architectural styles, smooth transitions between structures, and enhanced neighborhood hardscape and landscape features. Upon completion, the detached residential area should encourage neighborhood interaction and integration within the overall development of the Upland Crossing Specific Plan area.

It is the intent for all residential architecture to achieve a high level of quality in building function and visual appearance, assure variety and compatibility in architectural character, and to enhance the community’s overall value. The goal is to promote these qualities in conjunction with the landscape and site planning by using heritage styles combined with modern technology and architectural innovation to provide a pleasant, livable community.

a. Architectural Style

In order to encourage cohesiveness between the cluster and detached residential products, the same architectural styles previously mentioned for the cluster residential should be used. See Section 2.a. for more detail.

b. Plan and Style Requirements

1. Provide at least three (3) building plans per neighborhood, with the ability to vary placement or add elements to corner units; and
2. Provide at least three (3) different, yet compatible color schemes for each neighborhood. Individual building colors

should be selected from the color palettes selected for that style.

3. Provide variation in the placement of front elevations so as to vary the building setbacks along a block.

c. Building Orientation

Creating street scenes that function well and have visual interest are primary community objectives. The following basic elements and criteria are intended to develop variations in appearance and a sense of individuality for each home, and are conceptually shown on Figure 24, Typical Street Elevations: Detached Residential. Neighborhoods that have nearly identical homes and streets without variation in product placement and form will not be approved in the design review. This section lists architectural plotting techniques that work together to create a quality environment.

1. Buildings should relate appropriately to surrounding development and streets and create a cohesive visual identity for the neighborhood and street scene. There should be cohesiveness with building scale, massing, and architectural elements with surrounding development;
2. Public spaces should be clearly defined from private spaces through the use of building materials, grade separations, and physical separations such as fences, walls, or building enclosures;
3. Provide variation in floor plans, massing forms and elements on facades;



Source: Angeleno Associates, Inc.

4. De-emphasize garages through building orientation and projection of roof forms and living space over the garage towards the street.
5. Design simple floor plans and building masses using the correct proportion and scale for the selected styles in mind; and
6. Adjacent and facing residences having the same front elevations are prohibited. However, reversed footprints with variation in elevation design and color/material application are permitted.

d. Building Mass and Scale, Form, and Height

The roof forms, pitches and heights of various architectural elements should conform to the individual styles selected for detached residential. The following guidelines should be followed:

1. No building shall exceed a height of three stories, with a loft or bonus. An architectural projection such as a chimney or tower may exceed the maximum building height, but shall not exceed a height of 45 feet;
2. To the extent possible, each of the units should be individually recognizable. This can be accomplished with the use of balconies, setbacks, and projections which help articulate individual dwelling units or collections of units, and by the pattern and rhythm of windows and doors;
3. Elevations visible from public or private streets should be carefully articulated;

4. Emphasis should be placed on horizontal architectural lines including trim and fascia lines to make tall buildings appear wider;
5. Buildings should be designed to define outdoor spaces, with floor plans that have a logical and functional relationship between indoor spaces and outdoor areas; and
6. Single-story elements are encouraged on all buildings to establish pedestrian scale and add variety to the street scene.

e. Shade and Shadow – Building Relief

1. Project a front door image and access to the street front. Added articulation of end unit elevations is encouraged to achieve four-sided articulation;
2. Provide walking paths or courtyards to direct pedestrians from the sidewalk to the front door;
3. Provide front porches where style appropriate and possible to accomplish stepped massing and transition to public spaces;
4. Vary setbacks on building elements/facades; and
5. Vary roof pitches and directions.

f. Roofs and Chimneys

1. A variety of roof styles should be permitted, including hip, gable, and shed. The roof design should follow the wall planes and roof pitch should be consistent with the architectural style. Flat roofs shall be prohibited;

2. Roofs should be constructed of non-combustible materials. Acceptable materials include clay or concrete tile. Unacceptable materials include pressed wood, corrugated fiberglass, asphalt roll roofing, and corrugated metal;
3. Roofing trim materials should be of similar materials and complementary colors. Roof vents and appurtenances should be painted to match either the body or trim of the building;
4. Roof overhangs should be typical of the architectural style and should be designed as an integral part of the roof form. Awnings shall be permitted if true to the architectural style; and
5. Fireplaces are typically gas appliances, not wood-burning, and therefore, chimneys are not required in a functional sense. Chimneys may be included as an architectural element. If included, they shall be constructed of veneer or stucco.

g. Entries and Windows

1. Window pediments, small roof elements, overhangs, and projections over windows, doors, and garage doors should be incorporated into the architectural detailing; and
2. A variety of window and door treatments should occur to match the architectural style of the house. Awnings, and decorative shutters should be incorporated into the building design as appropriate.

h. Balconies and Porches

1. Balconies and porches should be used to articulate and reduce mass, as well as to provide shadow relief. Where architectural style and density are appropriate, stoops may be used instead of porches;
2. Porches, balconies, and trellis structures should be compatible with the overall architectural theme, style, and design of the structure; and
3. Materials used to construct balconies and porches should be appropriate to the designated architectural style. Where balconies are proposed, facades should vary in size and plane.

i. Details

1. Potshelves, pilasters, and brick and tile accents are encouraged around doors, windows, and entries, particularly near front and side entries that are easily visible from the street;
2. Low and high walls should be used to define courtyards, patios, and entries;
3. Mechanical equipment such as gas meters and air conditioning and heating units should be screened from public view, either by landscaping, fences, and walls, or combination thereof; and
4. Rain gutters, downspouts and other architectural elements constructed of sheet metal should be painted with complementary colors to the building, either matching the color of the trim, fascia or stucco, depending on its

location.

j. Materials and Colors

A variety of natural looking materials and colors will provide the diversity required for visual interest while supporting the community character and creating a timeless appeal. The primary purpose of the architectural color palette selection is to avoid monotony, provide a variety of colorful schemes and promote visual diversity. To further this goal of diversity, the following criteria shall be met:

1. Individual color schemes must be appropriate to the architectural styles with a harmonious selection of accent materials, roof profiles and colors; and
2. No adjacent home shall have the same color scheme.

k. Garage Doors and Driveways

1. Garage doors should be of complementary color, texture, and material.

l. Lighting

1. All street lighting shall be nostalgic luminaire post top light fixtures, also referred to as decorative acorn-shaped SCE street lights, as previously shown on Figure 22. All street lighting shall conform to City standards or an approved theme lighting program, and shall be approved by the City Engineer.

m. Association

A master homeowners association (HOA) shall be created for the Upland Crossing Specific Plan area in order to preserve and

enforce the goals and objectives of this residential development and the City of Upland. The HOA shall be responsible for the maintenance of all perimeter landscape, walls, lights, interior streets, onsite utilities and common areas within the Specific Plan. The HOA shall process a full set of CC&R's, which are not part of this Specific Plan document, through Development Services and the City Attorney's Office in conjunction with the processing of the project's final map.

In addition, a separate POA shall be created for commercial-retail Planning Area 1.

D. LANDSCAPE DESIGN GUIDELINES

1. INTRODUCTION

The Upland Crossing Landscape Design Guidelines establish a landscape theme for the planning areas and set forth general criteria for landscaping to enhance the urban "Village" environment. These guidelines consist of a written summary and establish criteria for entries, streetscapes, special treatment/buffer areas, plant palette, walls and fences, and lighting. When implemented, they will serve to create a unifying fabric for the Upland Crossing Specific Plan.

The Upland Crossing Specific Plan landscaping theme is driven by the desire to create an identifiable sense of place within the site and will assist tremendously in providing a successful addition to the Upland community. Landscape and streetscape elements will be used to create visual continuity throughout the Upland Crossing Specific Plan. Design elements such as entries, streetscapes, and walls and fences will establish a design theme for the project by reinforcing the design hierarchy and by providing visual gateways. These features are created through a thematic blend of landscape, hardscape, and planting elements.

The landscape theme for the Upland Crossing Specific Plan should incorporate a compatible plant palette of trees, shrubs, and ground covers. Careful thought has been given to integrate the structural and aesthetic elements of a balanced development. Once a particular plant or plant combination is used for a particular application, it shall be repeated in similar areas of the project to reinforce a sense of cohesion within the development. Landscape design shall not, however, result in monotony or lack of variety or biological diversity. The following is a description of landscape elements for the overall project area. These elements shall be detailed at the submittal of tentative map, site development plan, or other discretionary permit for each area of development. The landscape design is based on the vision and goals of the Upland Crossing Specific Plan (see Figure 25, Conceptual Landscape Plan).

2. STREETSCAPES

Roadway streetscapes adjacent to the Upland Crossing Specific Plan are critical in maintaining the perception, unification, and quality of the development. Landscaping of perimeter and internal streets will help provide continuity throughout the project and create an appropriate connection with adjacent areas. Street trees and groundcover should reflect the project’s design theme and be selected from the project’s plant palette (see Table 8). Landscaping on Foothill Boulevard, Monte Vista Avenue, 11th Street and Dewey Way will establish the main planting theme for the Upland Crossing Specific Plan.

The Design Guidelines for the Upland Crossing Specific Plan strongly encourage landscaping and hardscape within and adjacent to the site. The proposed landscape and hardscape must be sensitive to the needs of the community. More specifically, the area in between Foothill Boulevard and Planning Areas 1 and 3 will include an approximate 12-foot wide planting strip with large canopy street trees, a 6-foot wide pedestrian walkway, and up to a 47-foot wide HOA landscaped area with a variety of low groundcover, groupings of accent trees and

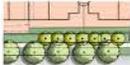
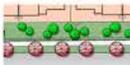
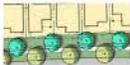
random groupings of vertical background trees, as shown on Figure 26, Foothill Boulevard Streetscape. This landscaped area will have a 3:1 south sloping grade beginning approximately 2 feet south of the pedestrian walkway. The City of Upland’s right-of-way includes all of the paved area of Foothill Boulevard, a portion of the planting strip approximately 2 feet north of the pedestrian walkway, and the approximate 55-foot wide area immediately south. Additionally, Foothill Boulevard has a 20-foot wide raised landscaped median at the centerline with medium scale canopy trees. The width of Foothill Boulevard and Monte Vista Avenue both accommodate bike lanes.

As shown on Figure 27, Monte Vista Avenue Streetscape, this street will have a 7.5-foot wide planting strip with large canopy street trees, a 5-foot wide sidewalk, and an 8-foot wide, 2 to 1-foot slope maximum landscaped setback with a formal row of vertical background trees. 11th Street will be similarly landscaped, however, medium scale canopy street trees, low mounding and cascading shrubs and informal groupings of vertical background/accent trees will be planted on this narrower street, as shown on Figure 28, 11th Street Streetscape. There will be a 2 to 1-foot slope maximum along the project’s boundary on 11th Street, and a variable height retaining wall along 11th Street East. Dewey Way will be landscaped with similar trees and shrubs to complete the project’s identification. As shown on Figure 29, Dewey Way Streetscape, the west side of Dewey Way will have an 8-foot wide landscape setback with informal groupings of vertical background trees which could have a 2 to 1-foot slope maximum, a 5-foot wide sidewalk, and an 8-foot wide planting strip with large canopy street trees. An 8-foot wide 3:1 slope will be constructed on the east side of Dewey Way. A sidewalk on the east side of Dewey Way will not be constructed.

As shown on Figure 30, Interior Road Streetscape with Parking, all interior streets with on-street parking will have 56-foot right-of-ways. Each side of the street includes a 10-foot wide lane, 8-foot wide on-street parking, a 6-foot wide planting strip with medium or large canopy street trees, and a 4-foot wide sidewalk. Vehicle considerations have



LEGEND

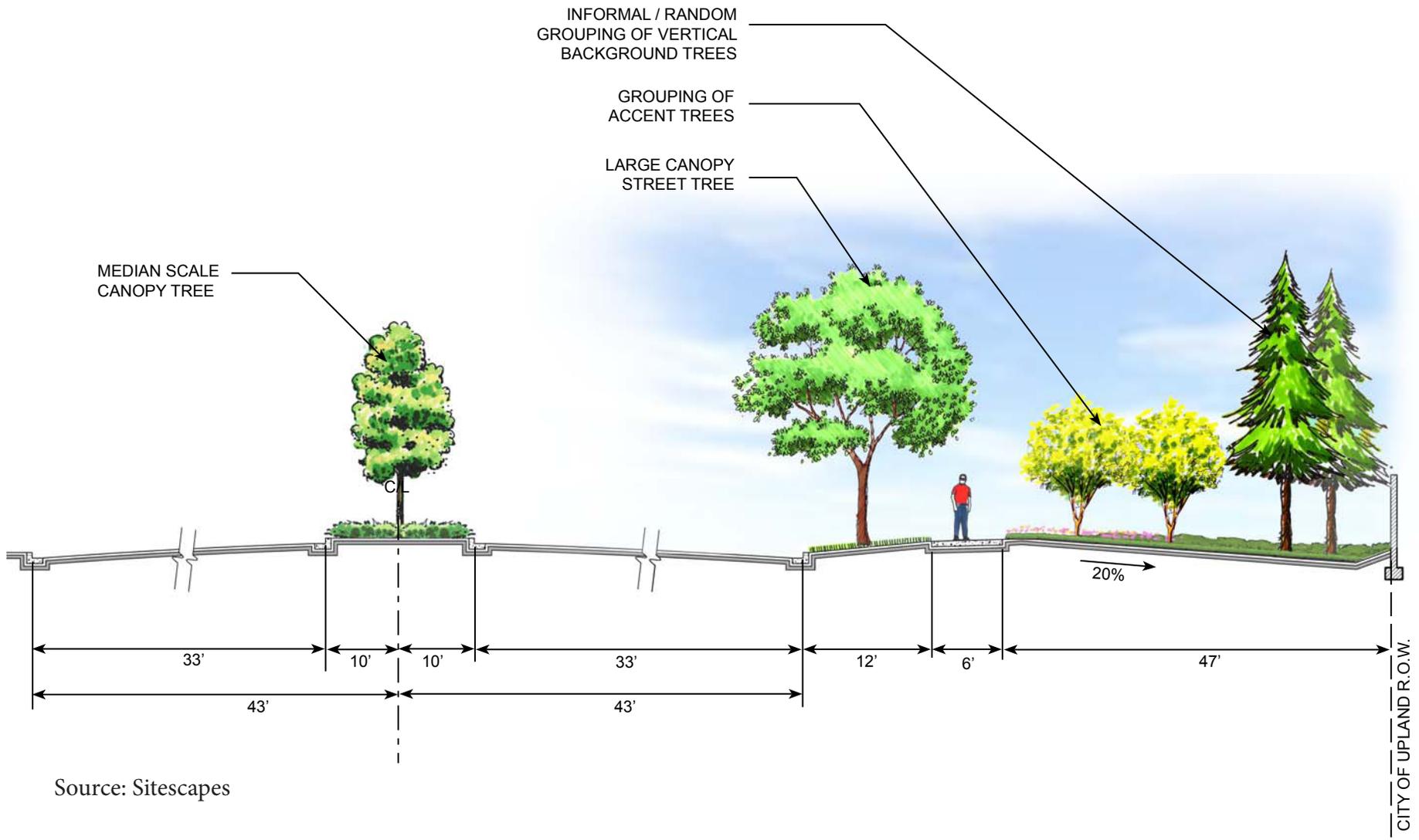
-  FOOTHILL BLVD. LANDSCAPE STREETScape
-  DEWEY WAY LANDSCAPE STREETScape
-  11th STREET LANDSCAPE STREETScape
-  MONTE VISTA AVE. LANDSCAPE STREETScape
-  CHANNEL TRAIL LANDSCAPE PASEO
-  RESIDENTIAL ENTRIES
-  CORNER TREATMENT
-  TREE, SHRUB & GROUNDcover AREA (TYPICAL)

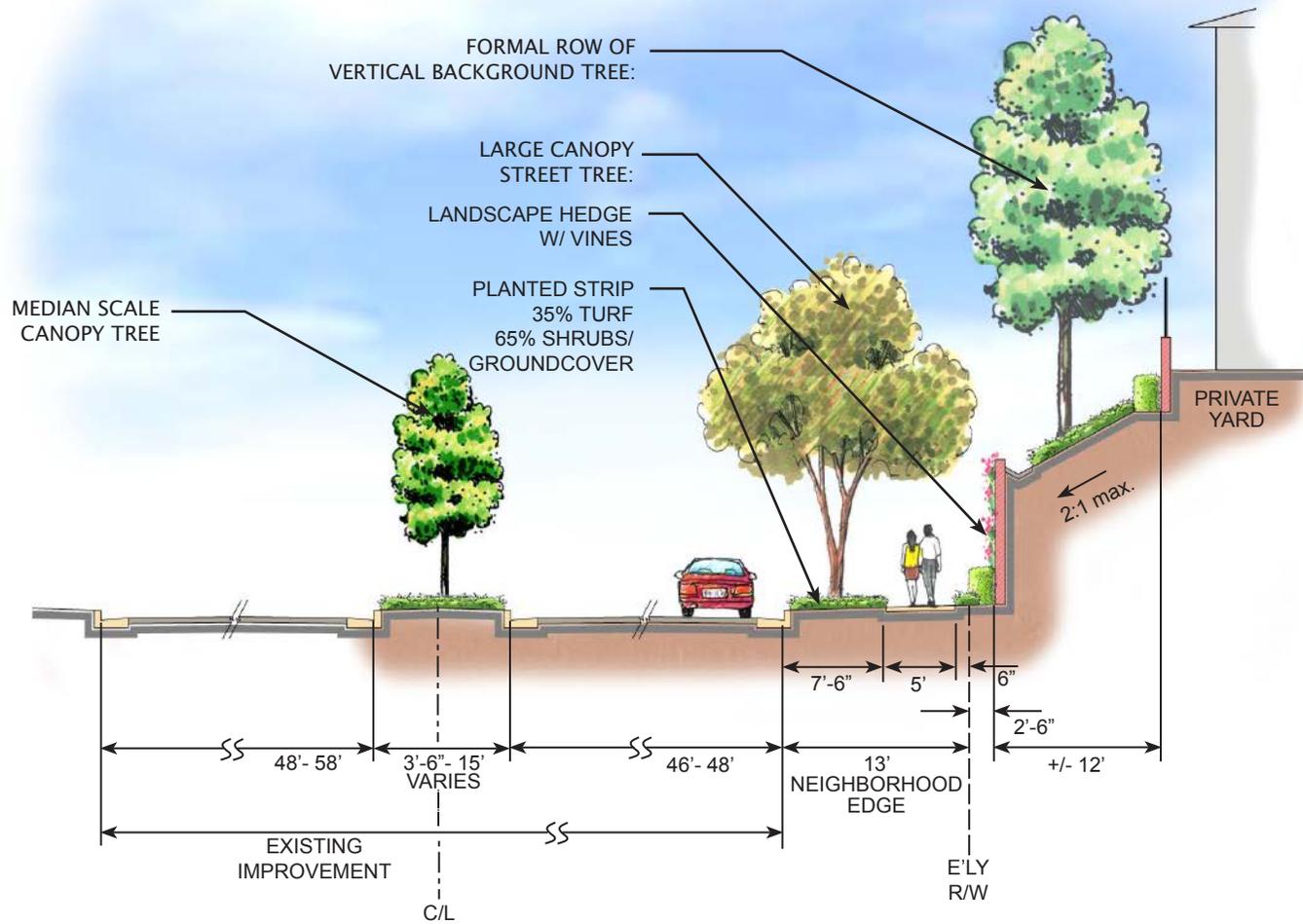
Source: SITESCAPES

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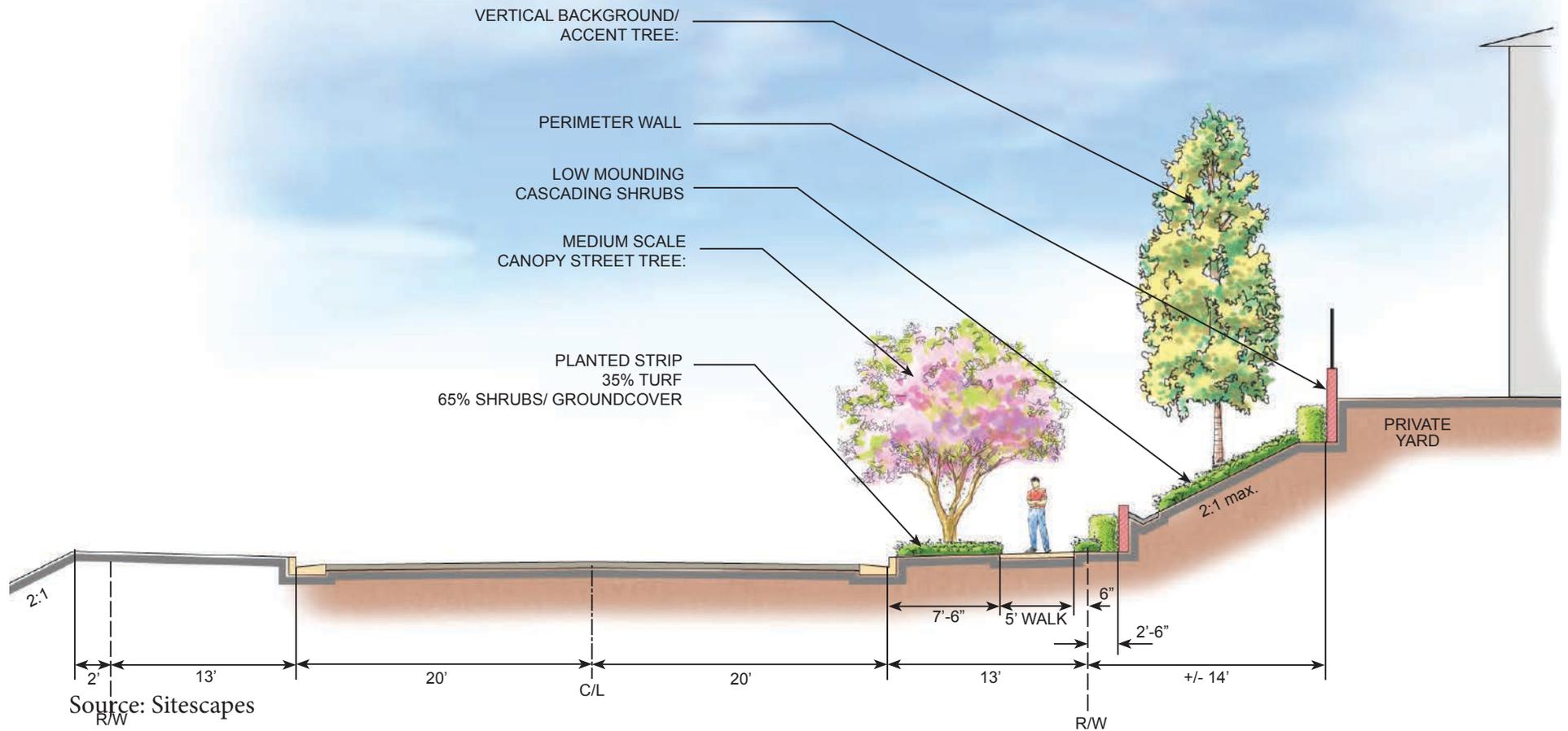
Figure 25
Conceptual Landscape Plan





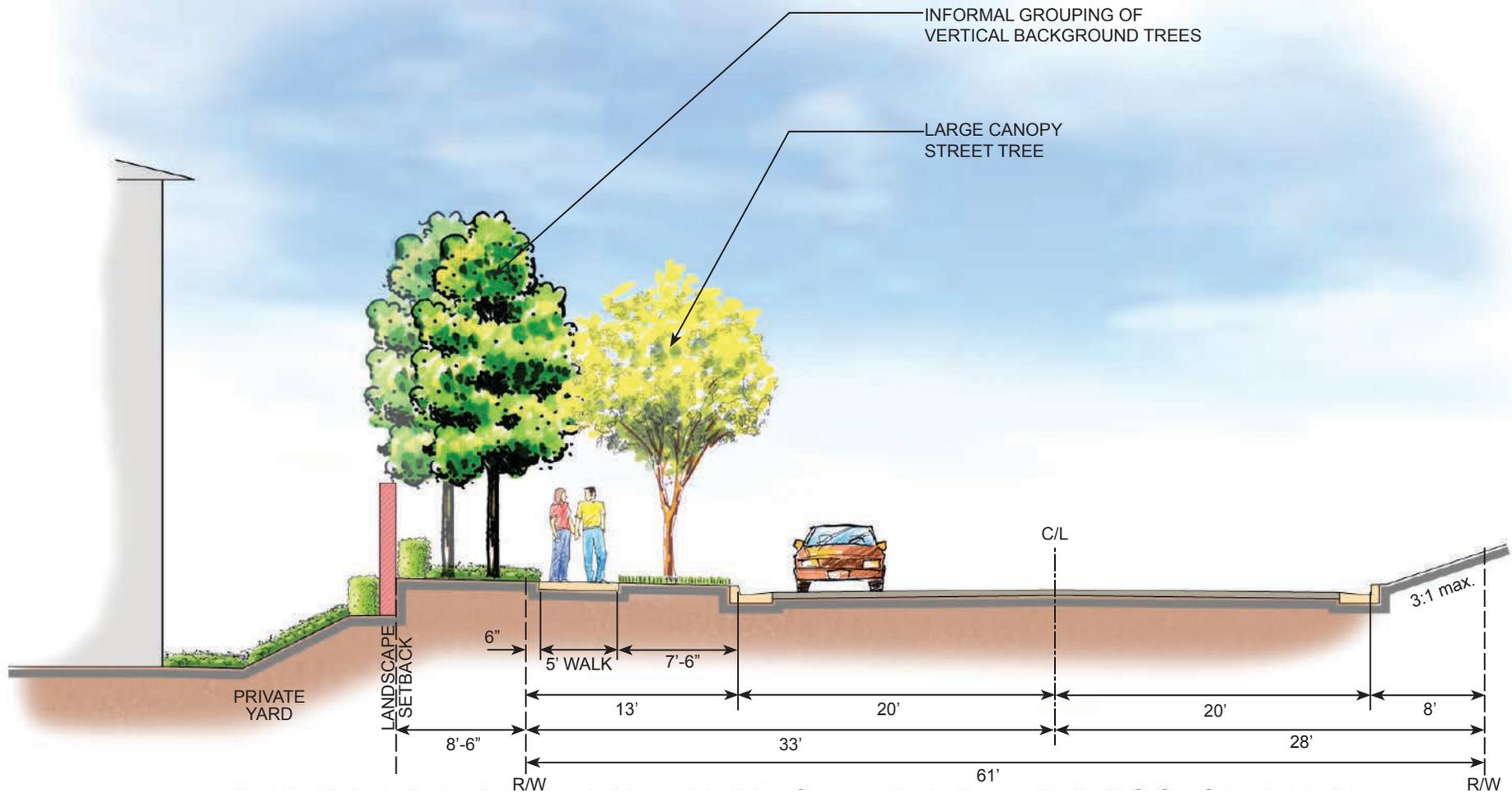


Source: SITESCAPES



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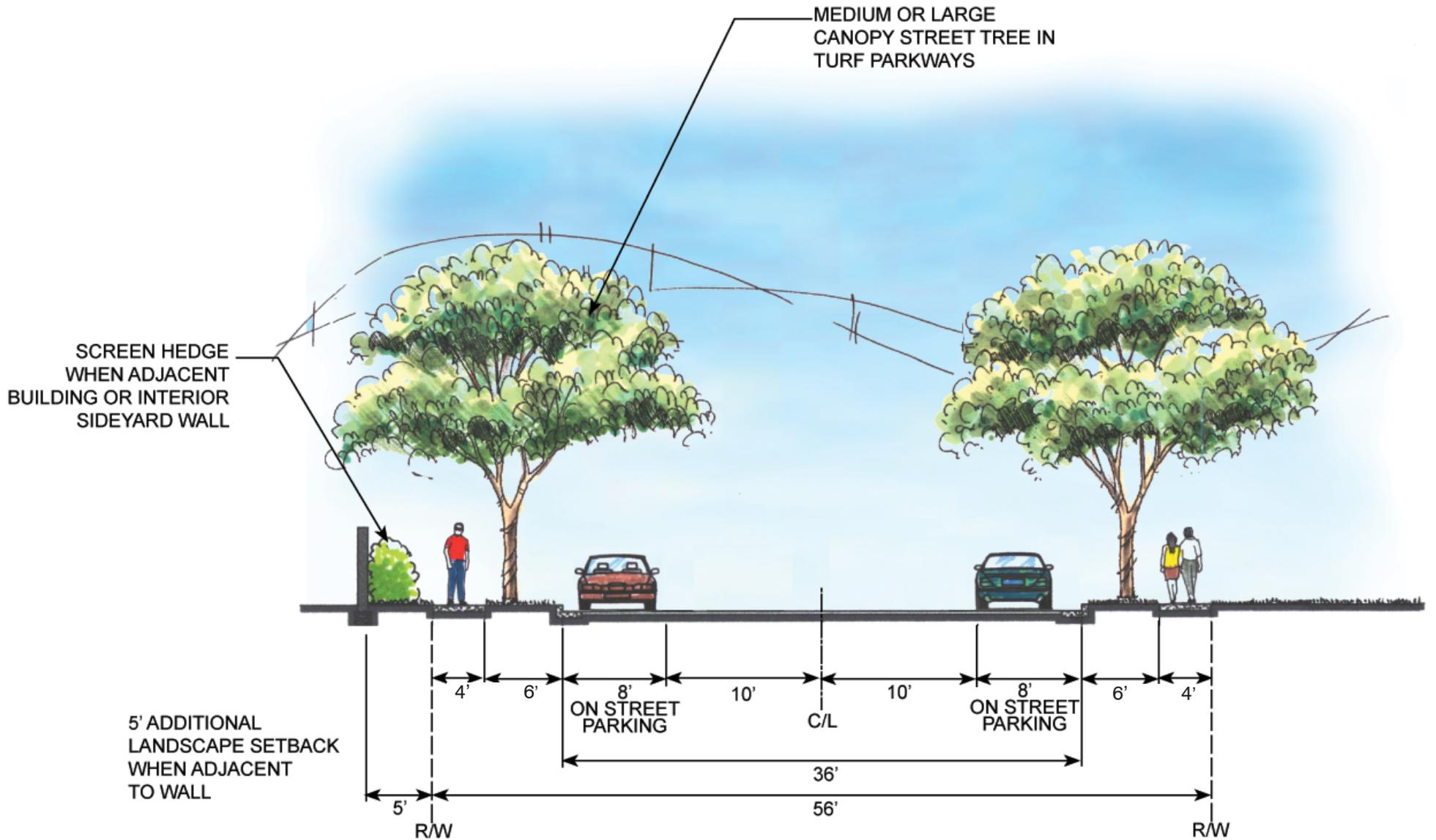
Figure 28
11th Street Streetscape



Source: SITESCAPES

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Figure 29
Dewey Way Streetscape



Source: Sitescapes



been recognized, striking a balance between the needs of both the pedestrian and the automobile.

3. ENTRY MONUMENTATION

Entry monuments serve several important purposes and careful consideration should be given to the design of the project entries. The intent of project monumentation design is to create visual gateways into the Upland Crossing Specific Plan and to provide aesthetically pleasing entry statements that highlight the distinctiveness of the development. Entry monumentation within the Upland Crossing Specific Plan will establish the design theme by creating a thematic blend of hardscape and planting elements. Each entry monument setting is site specific and should be comprised of a blending of construction features, signage, and landscape and hardscape which enhances the overall outlook of the Upland Crossing Specific Plan. Neighborhood monuments will be located at the project entries along 11th Street and Dewey Way. Corner monumentation shall occur at Foothill Boulevard and Dewey Way as well as 11th Street and Dewey Way and 11th Street and Monte Vista Avenue which will complement the entry monuments and provide project identities.

Figure 31, Primary Residential Entry at 11th Street, will be built at the southern entrance to Planning Area 2. More specifically, a 10-foot tall by 3-foot wide stucco pilaster with a pre-cast finial cap will flank the entry gates. For project identity, a residential sign will be inlayed into the adjacent 9-foot stucco wall with peaked stucco cap. A five-foot wide arched pedestrian gated opening is provided as part of the entry stucco wall. Stamped AC pavement at each entry will provide a sense of “arrival.” A similarly designed residential exit from Planning Area 2 will be built on Monte Vista Avenue, which will have full project wall and landscape treatment at the southern corner, but only a project monument on the northern corner next to the existing commercial site.

Figure 32, Primary Residential Entry at Dewey Way, will be located

near Foothill Boulevard, will be a similar scale monument and will serve as the primary residential entry to Planning Area 3. A 10-foot tall stucco pilaster with adjacent 9-foot tall stucco wing wall will grace both sides of the gated entry. A 5-foot wide, gated arch opening within the wall provides pedestrian access. This entry will have stamped AC paving similar to the previous entryways described.

4. RECREATION AND OPEN SPACE

An important element of the Upland Crossing Specific Plan is the provision of recreation and open space areas to enhance the quality of living for residents of the Upland Crossing community. The Conceptual Landscape Plan shows a recreation system that consists of a recreation area with several amenities, a pedestrian trail along the San Antonio Creek Channel, and pedestrian friendly walkways along the perimeter of the project area. More specifically, the approximate 1.0-acre recreation area will be located in Planning Area 2 along the west side of the San Antonio Creek Channel. Several amenities shall be provided, including a pool, spa, outdoor pool building with restrooms, drinking fountains, and pool equipment storage, an entertainment plaza, barbeque area, tot lot, gazebo, and several open turf areas, as shown on Figure 33. Elevations of this recreation center are shown on the following figures: Figure 34, Private Recreation Center Elevation, Figure 35, Private Recreation Center and Pool Elevation, and Figure 36, Private Pool Building Elevation. The Upland Crossing Specific Plan is a pedestrian-oriented community and dedicated parking at the recreation area would be unnecessary.

The San Antonio Creek Channel will provide approximately 1.5 acres of improvements within the right of way, including a pedestrian trail and access road on the west side, and an access road on the east side. The 12-foot wide access road and pedestrian trail will be located along the west side of the channel abutting the channel fence, with a variable width landscape planter containing trees, shrubs and ground cover to buffer the adjacent residences from the trail. The east side will contain a

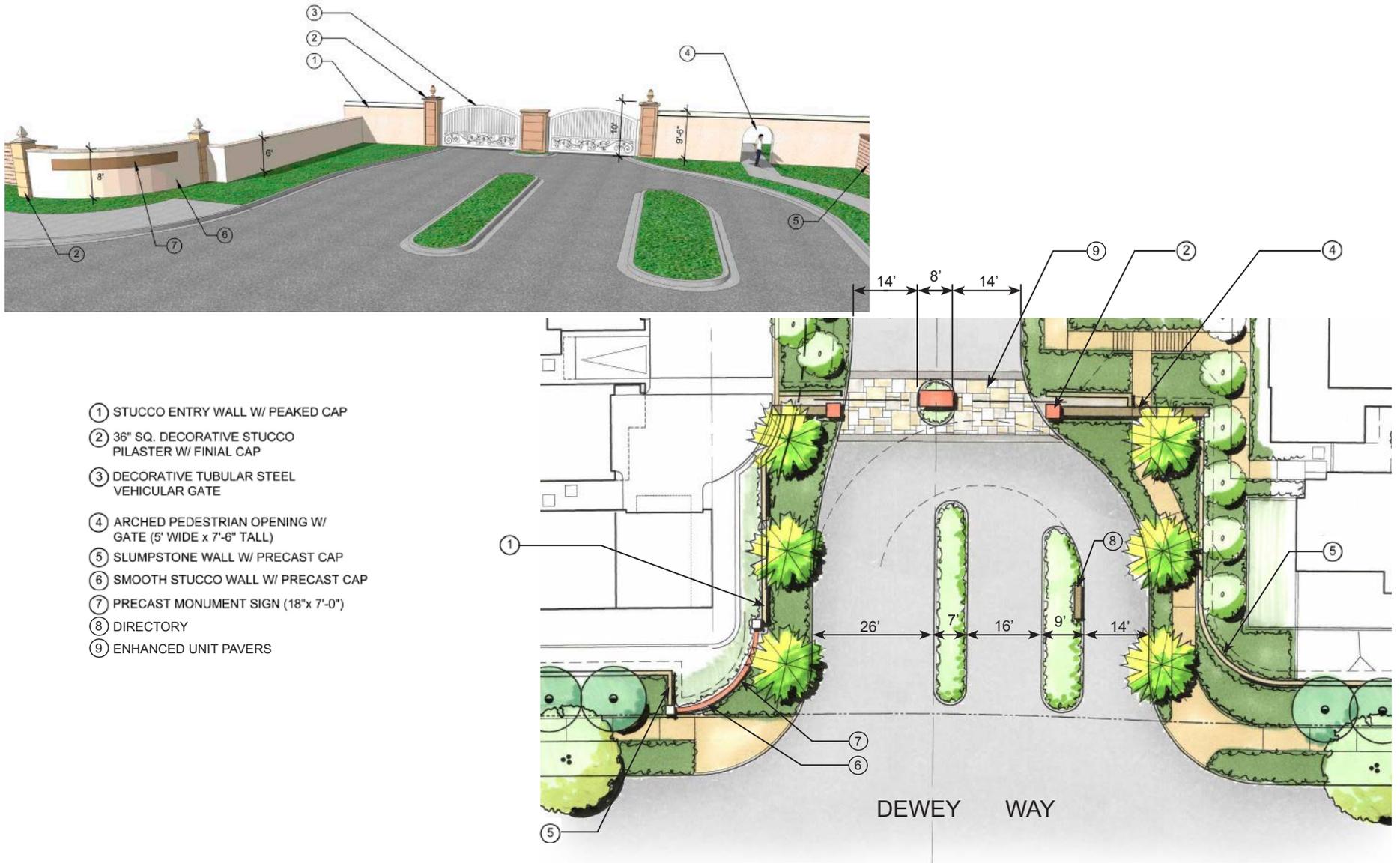


- ① DECORATIVE TUBULAR STEEL VEHICULAR GATE
- ② STUCCO ENTRY WALL W/ PEAKED CAP
- ③ PRECAST CONCRETE MONUMENT SIGN ON CURVED/ RAKED STUCCO WALL
- ④ 36" SQ. DECORATIVE STUCCO PILASTER W/ FINIAL CAP
- ⑤ ARCHED PEDESTRIAN OPENING W/ GATE (5' WIDE x 7'-6" TALL)
- ⑥ DIRECTORY
- ⑦ PEDESTRIAN WALKWAY
- ⑧ ENHANCED UNIT PAVERS

Source: SITESCAPES

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Figure 31
Primary Residential Entry at 11th Street

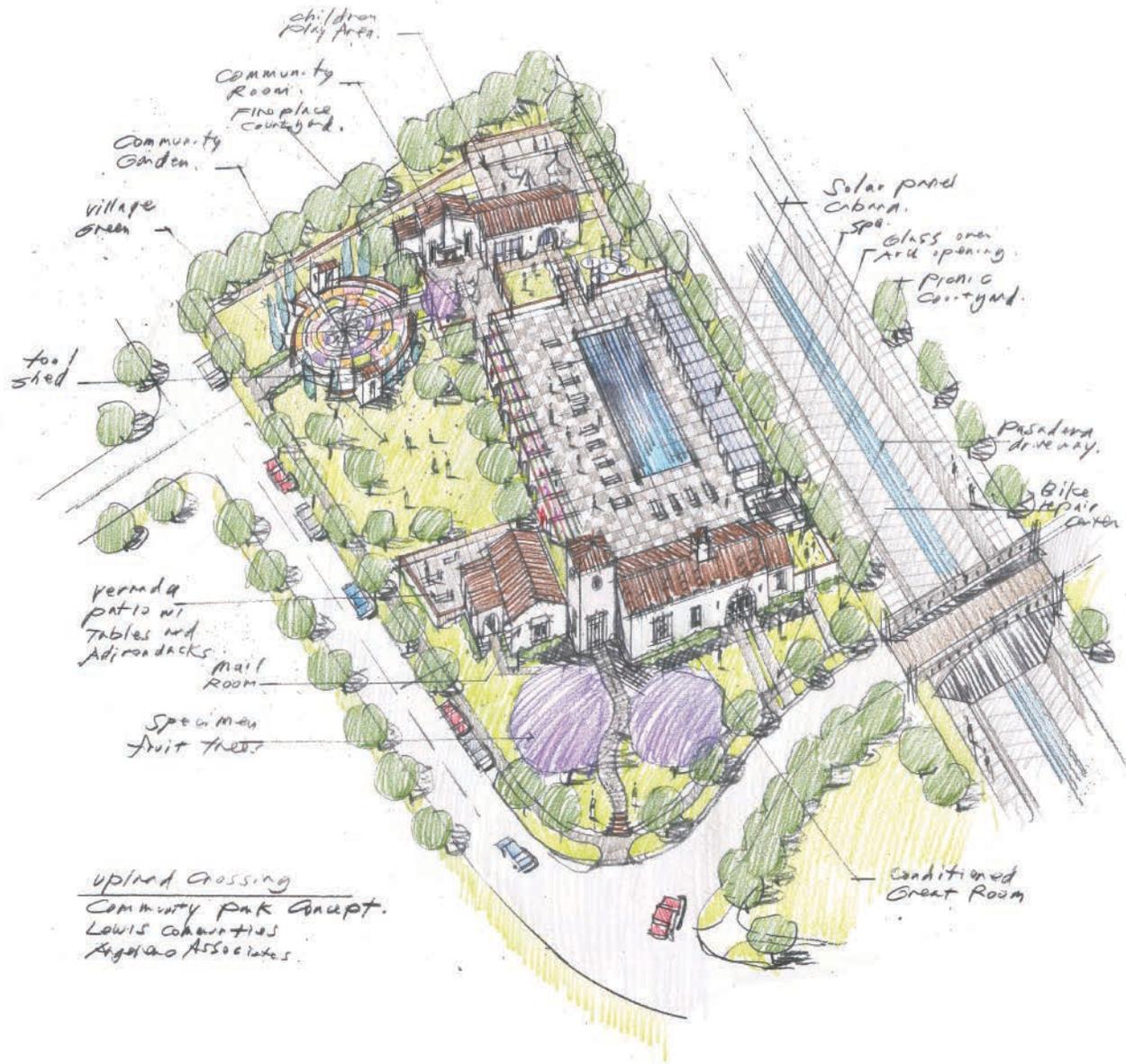


Source: SITESCAPES

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Figure 32
Primary Residential Entry at Dewey Way





Source: Angeleno Associates, Inc.

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Figure 33
Private Recreation Area Concept





FRONT ELEVATION (RECREATION CENTER)



LEFT ELEVATION (RECREATION CENTER)

Source: Angeleno Associates, Inc.

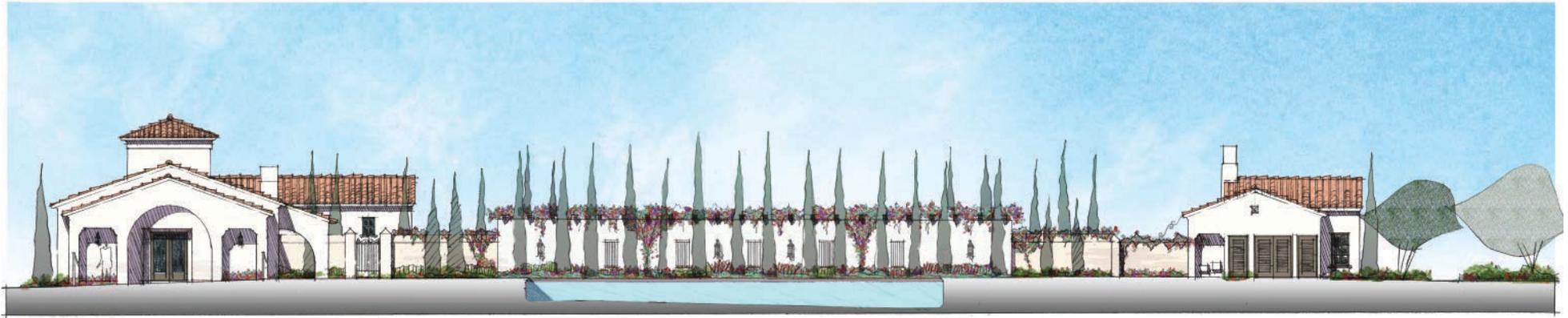




COMMUNITY POOL BUILDING

LEFT ELEVATION

RECREATION CENTER



RECREATION CENTER

RIGHT ELEVATION

COMMUNITY POOL BUILDING

Source: Angeleno Associates, Inc.





FRONT ELEVATION



REAR ELEVATION

Source: Angeleno Associates, Inc.

12-foot wide access road with a variable width landscape planter containing trees, shrubs and ground cover to buffer the adjacent residences from the channel. The east side will only be accessible to maintenance vehicles. The existing chain link fencing along the channel walls will be replaced with a new 6-foot tall fence in accordance with San Bernardino County Flood Control standards.

The pedestrian trail is accessible from sidewalks along the south side of Foothill Boulevard, as well as from the south at 11th Street. Residents from Planning Area 1 and 2 can access the trail from a gated entry as shown on Figure 9 – Pedestrian Circulation Plan.

The San Antonio Creek Channel is owned by the Los Angeles Division of the Army Corps of Engineers, but is maintained by the San Bernardino County Flood Control District. All encroachments require approval by these agencies. The project Homeowners Association will be responsible for maintaining the landscaping within the channel right-of-way, and San Bernardino County Flood Control will maintain the access roads.

A chain link fence will be installed for safety along the channel. Along the east side of the channel, a 12-foot wide access road and an approximately 15-foot wide landscape area will provide a buffer for adjacent residents, subject to regulatory approval, as shown on Figure 37, San Antonio Creek Channel Plan View and Section. As previously described, the sidewalks along the perimeter streets will be buffered from passing vehicles by landscaping varying in width from 6 feet to 12 feet. Additionally, all interior streets include sidewalks on each side of the street, providing convenient pedestrian access throughout the project area.

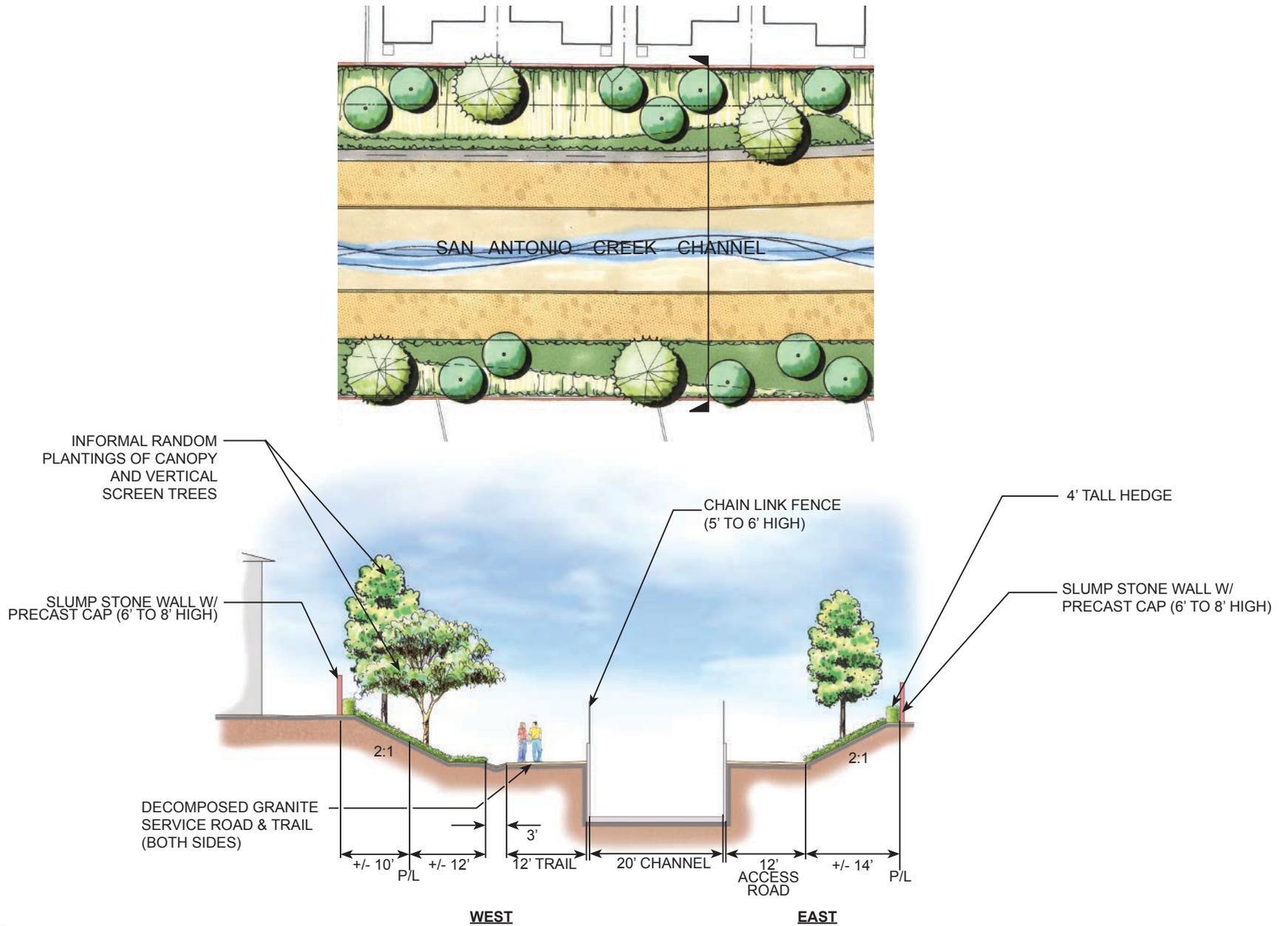
5. PLANT PALETTE

It is the intent of these guidelines to provide flexibility and diversity in plant material selection, while maintaining a limited palette in order to

give greater unity and thematic identity to the Upland Crossing Specific Plan. The plant material palette has been selected for its appropriateness to the project theme, climatic conditions, soil conditions, and concern for maintenance. Wherever possible, overall plant material groupings shall be designed to minimize water application for the entire landscape setting. Consideration should be given to the location of trees to avoid potential long term problems with tree roots and shall be consistent with the City of Upland landscaping standards. Use of existing plant materials in key areas will be a desired feature of the Upland Crossing Specific Plan. Table 8, Plant Palette, provides a listing of plant material identified for use in the streetscapes, entries, and other special areas of the Upland Crossing Specific Plan. Other species can also be used, subject to the approval by the City of Upland.

a. Planting Guidelines

1. Trees shall be no less than 15 gallons in size and shall be of species and variety approved by City staff. All four perimeter streets (Foothill Boulevard, Monte Vista Avenue, 11th Street, and Dewey Way), and loop interior streets within the project site shall include a mix of 15-gallon and 24-inch box trees. To achieve the desired visual and aesthetic effect, tree size may vary with approval of the City of Upland Planning Division.
2. All 15-gallon trees shall be staked with a minimum of one 8' by 2" diameter (pressure treated lodge pole tree stakes or equivalent).
3. All 24-gallon trees shall be staked with a minimum of two 10' by 2" diameter (pressure treated lodge pole tree stakes or equivalent).



Source: SITESCAPES

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Figure 37
San Antonio Creek Channel Plan View and Section

**TABLE 8
PLANT PALETTE**

TYPE		BOTANICAL NAME	COMMON NAME
TREES	VERTICAL BACKGROUND	<i>Eucalyptus sideroxylon 'Rosea'</i>	Pink Iron Bark
		<i>Melaleuca quinquenervia</i>	Cajeput Tree
		<i>Pinus canariensis</i>	Canary Island Pine
		<i>Pinus eldarica</i>	Mondell Pine
		<i>Tristania conferta</i>	Brisbane Box
	LARGE CANOPY	<i>Jacaranda mimosifolia</i>	Jacaranda
		<i>Magnolia grandiflora 'Majestic Beauty'</i>	Southern Magnolia
		<i>Cinnamomum camphora</i>	Camphor Tree
		<i>Quercus ilex</i>	Holly Oak
		<i>Tipuana tipu</i>	Tipu Tree
		<i>Ulmus parvifolia 'True Green'</i>	Evergreen Elm
	MEDIUM CANOPY	<i>Cupaniopsis anacardioides</i>	Carrotwood
		<i>Koelreuteria bipinnata</i>	Chinese Flame
		<i>Lagerstroemia farnesia 'Muskogee'</i>	Crape Myrtle (Lavendar)
		<i>Magnolia grandiflora 'Russet'</i>	Southern Magnolia
		<i>Platanus acerifolia 'Bloodgood'</i>	London Plane Tree
	ACCENT	<i>Carya illinoensis</i>	Pecan
		<i>Chitalpa tashkentensis 'Pink Dawn'</i>	Chitalpa
		<i>Citrus sinensis</i>	Orange
		<i>Dyospiros kaki</i>	Persimmon
<i>Olea europaea</i>		Olive	
<i>Prunus spp.</i>		Cherry	
<i>Pyrus calleryana 'Chanticleer'</i>		Chanticleer Pear	
<i>Pyrus pyrifolia</i>		Asian Pear	
<i>Tabebuia chrysotricha</i>		Golden Trumpet Tree	
PALMS	<i>Cocos plumosa</i>	Queen Palm	
	<i>Washingtonia robusta</i>	Mexican Fan Palm	
SHRUBS	ACCENT	<i>Agapanthus africanus 'Peter Pan'</i>	Dwarf Lily-of-the-Nile
		<i>Agave vilmoriniana</i>	Octopus Agave
		<i>Aloe spp.</i>	Aloe
		<i>Anigozanthos manglesii</i>	Kangaroo Paw
		<i>Camellia japonica 'Debutante'</i>	Camellia
		<i>Hemerocallis 'Dwarf Yellow'</i>	Day Lily
		<i>Nandina domestica</i>	Heavenly Bamboo
<i>Phormium tenax 'Bronze Baby'</i>	New Zealand Flax (Reddish)		

**TABLE 8
PLANT PALETTE (CONTINUED)**

TYPE		BOTANICAL NAME	COMMON NAME
SHRUBS (CONTINUED)	ACCENT (CONTINUED)	<i>Podocarpus macrophyllus</i> 'Maki'	Shrubby Yew Pine
		<i>Punica granatum</i> 'Nana'	Dwarf Pomegranate
		<i>Rosa F.</i> 'Trumpeter'	Red Shrub Rose
		<i>Salvia clevelandii</i>	Cleveland Sage
		<i>Strelizia reginae</i>	Bird-of-Paradise
		<i>Vaccinium spp.</i>	Blueberry
	LOW MOULDING/ CASCADING	<i>Callistemon v.</i> 'Little John'	Dwarf Weeping Bottlebrush
		<i>Cistus hybridus</i>	White Rockrose
		<i>Lantana montevidensis</i>	Spreading Lantana
		<i>Lavandula angustifolia</i> 'Irene Doyle'	English Lavender
		<i>Lomandra longifolia</i> LM300	Dwarf Breeze Mat Rush
		<i>Rosmarinus o.</i> 'Prostratus'	Trailing Rosemary
		<i>Salvia greggii</i>	Red Salvia
	FOREGROUND	<i>Buxus m. japonica</i>	Japanese Boxwood
		<i>Elymus condensatus</i> 'Canyon Prince'	Giant Wild Rye
		<i>Pittosporum tobira</i> 'Creme de Mint'	Variegated Dwarf Tobira
		<i>Raphiolepis indica</i> 'Ballerina'	Dwarf Indian Hawthorne
		<i>Salvia greggii</i>	Red Salvia
		<i>Stipa tenuissima</i>	Mexican Feather Grass
	MEDIUM HEDGE	<i>Cotoneaster lacteus</i>	Parney Cotoneaster
		<i>Feijoa sellowiana</i>	Pineapple Guava
		<i>Grevillea</i> 'Noellii'	N.C.N.
		<i>Leucophyllum laevigatum</i>	Chihuahuan Sage (Texas Ranger)
		<i>Ligustrum japonicum</i> 'Texanum'	Wax Leaf Privet
		<i>Moraea bicolor</i>	Fortnight Lily
		<i>Myrtus communis</i>	True Myrtle
		<i>Pittosporum tobira</i> 'Variegata'	Variegated Tobira
<i>Raphiolepis indica</i> 'Clara'		Dwarf Indian Hawthorne	
<i>Rosmarinus o.</i> 'Tuscan Blue'		Rosemary	
<i>Westringia fruticosa</i> 'Wynabbie Gem'		Coast Rosemary	
<i>Xylosma congestum</i>		Xylosma	
VINES			<i>Bougainvillea</i> 'San Diego Red'
		<i>Ficus repens</i>	Creeping Fig
		<i>Pyracanthus fortuneana</i>	Pyracantha
		<i>Vitis</i> 'Niabell'	Niabell Grape Vine
		<i>Wisteria sinensis</i>	Chinese Wisteria

4. All new development areas shall provide street trees spaced according to the growth characteristics of the species selected. Trees may be clustered. Tree species shall conform to the City of Upland Street Tree Guidelines and standards established by the Master Landscape Plan submitted as a part of implementing this Specific Plan.
5. One (1) tree shall be provided for every 10 stalls located in the overall parking field. Trees shall be of a species that provides visibility to signage and store fronts. A vertical type tree which has open foliage is appropriate.
6. All open parking area and their adjacent vehicular access ways shall incorporate landscaping, which may be comprised of trees, shrubs, and groundcovers. Planting areas shall be a minimum of 5' by 5' in diamond shape or equal inside dimension.
7. All plant material shall be planted in the following minimum sizes:
 - Trees – 15 gallon
 - Shrubs – 1 gallon
 - Primary Accent Trees – 24" box
8. Planting ratios for major street medians and parkways shall be as follows:
 - Turf – 35 percent
 - Ground Cover and Shrubs – 65 percent

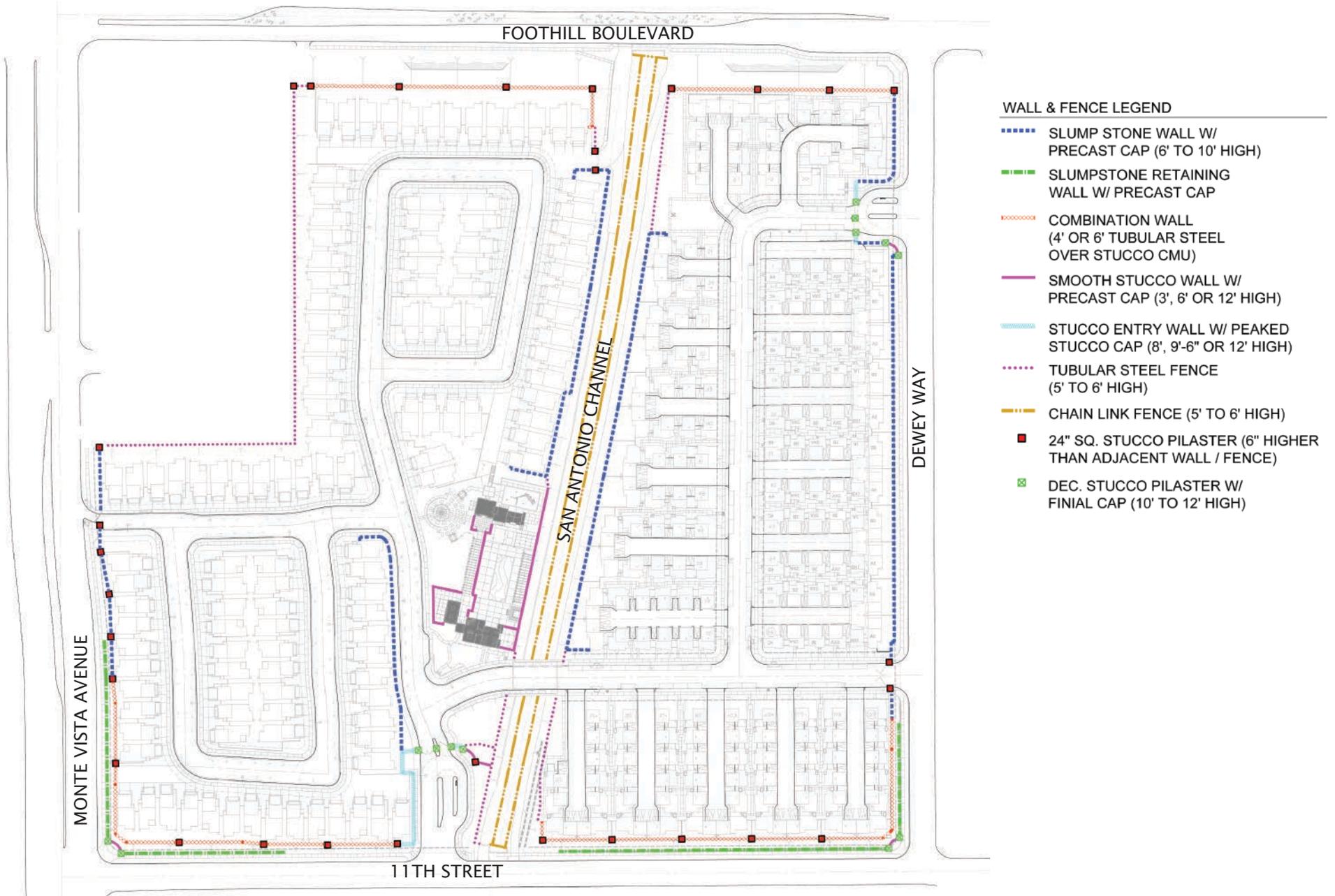
6. WALLS AND FENCES

Walls and fences are visually prominent and can form a unifying element of design. On the other hand, if not properly located and designed, they can be a detriment by providing an overbearing sense of "confinement" with little or no visual relief. While a project without walls may be a desirable objective, it is recognized that walls and fencing cannot be completely avoided for reasons of privacy, public health, safety, and project identity.

The intent of the Upland Crossing Specific Plan is to maintain open views and encourage access to public areas within the project. Where walls and fencing are required (e.g., noise attenuation, privacy) or desirable (e.g., entry monumentation and residential buffers), the improvements shall be designed and constructed in a manner compatible with the scale, texture, and color of the surrounding environment. Where unavoidable, designs shall integrate walls and fences with other site components including signage, structures, and landscape and hardscape.

Walls and fencing within the Upland Crossing Specific Plan should reflect an appropriate balance of function and aesthetic quality. All walls and fences within the project should be designed as integral elements of building architecture or complementary to the architecture and landscape character. Walls and fencing will be used to establish identity, provide protection from roadway and other noise, and allow privacy and security in residential areas.

As shown on Figure 38, Wall and Fence Plan, a combination wall (4' or 6' tubular steel over stucco CMU) with 24" stucco pilasters will be constructed on property lines along Foothill Boulevard, 11th Street, and along part of Monte Vista Avenue. A slump stone wall with precast cap (6' to 8') will be constructed along Dewey Way, along both sides of San Antonio Channel, along part of Monte Vista Avenue, and along an internal roadway. Chain link fence (5' to 6') will also line both sides of



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Figure 38
Wall and Fence Plan



the channel for pedestrian safety in accordance with Flood Control standards. A slump stone retaining wall with precast cap will be constructed along 11th Street and along parts of Dewey Way and Monte Vista Avenue. A tubular steel fence (5' to 6') will be constructed along the western perimeter of the project area and will connect with Monte Vista Avenue. A slump stone wall with precast cap (6' to 10') with pilasters will be constructed along Monte Vista Avenue from north of the residential exit to about half way along Monte Vista Avenue. Decorative stucco pilasters (10' to 12') will be provided at the entry along Dewey Way, 11th Street, and at the corner of Dewey Way and 11th Street. 24" square stucco pilasters will also be provided at the other entry points along Dewey Way, Foothill Boulevard, Monte Vista Avenue, and 11th Street. Stucco entry walls with peaked stucco cap (8', 9'-6", or 12') will be constructed along the 11th Street entry, connecting from the entry pilaster to the decorative stucco pilaster along the entry driveway. This same wall will be constructed along the Dewey Way entry, connecting with the decorative stucco pilasters at the entry. Smooth stucco walls with precast cap (3', 6' or 12') will be constructed around the recreational area. Details of the entry walls are shown on Figure 40, Entry Wall Details; details of the tubular steel fencing and pilasters are shown on Figure 41, Tubular Steel Fence, Combination Wall, and Pilaster Details; and details of the slump stone and stucco walls are shown on Figure 42, Slump Stone and Stucco Wall Details. Within the planning areas, tubular steel, vinyl, plexiglass view fencing, or masonry walls shall be allowed.

a. Wall and Fence Guidelines

The following guidelines shall be used for all walls and fences located within the Upland Crossing Specific Plan:

1. Walls shall not exceed 10 feet in height along the project perimeter and interior roadways, excluding corner monument walls which may not exceed 12 feet;

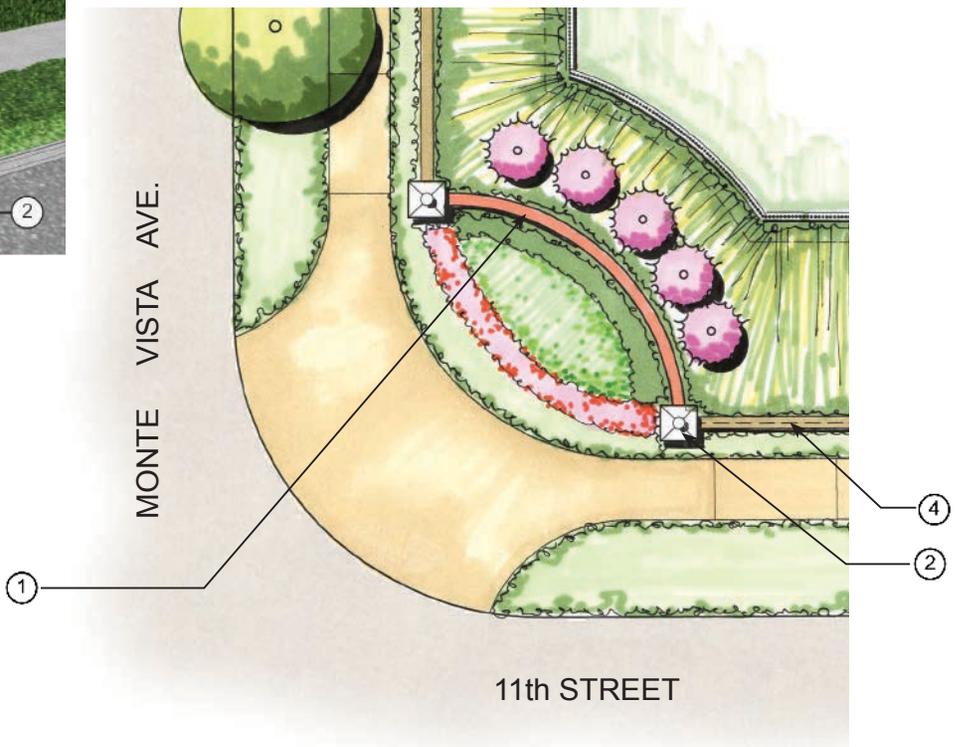
2. Fences not exceeding 6 feet in height may be erected and maintained;
3. Combination retaining and wall/fence may be erected and maintained;
4. Appropriate materials include stone, stone veneer, split face/precision block, concrete, stucco pilasters, wrought iron, vinyl, plexiglass, and wood;
5. Solid walls longer than 100' in length and visible from public roadways shall incorporate design elements to reduce monotony;
6. The placement of a wall shall minimize the visual impact of the wall and maximize its effectiveness as mitigation for noise and privacy;
7. Wall materials and patterns shall be compatible with adjacent building materials;
8. Walls shall be treated as extensions of structures and use materials and colors complementary to the overall architectural design; and
9. The softening of wall surfaces visible from public rights-of-way with plantings is required.

7. SPECIAL HARDSCAPE TREATMENT

Hardscape design elements shall be incorporated into the overall design scheme for plaza, courtyard, and transitional spaces within the project in a coordinated and consistent manner. Hardscape treatments will function as a visual and physical connection between buildings and

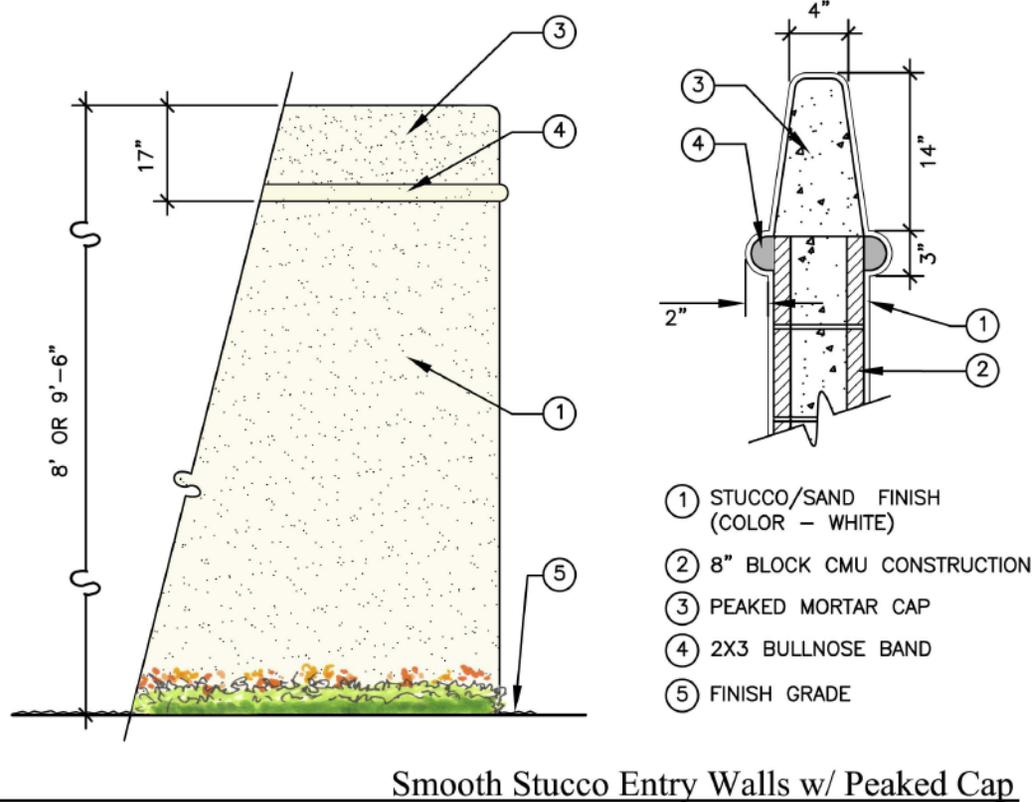


- ① SMOOTH STUCCO WALL W/ PRECAST CAP
- ② 36" SQ. DECORATIVE STUCCO PILASTER W/ FINIAL CAP TO MATCH SCRIPPS COLLAGE
- ③ PRECAST CONCRETE MONUMENT SIGN (18"H.x 7' LONG)
- ④ SLUMPSTONE WALL W/ PRECAST CAP
- ⑤ COMBINATION WALL AT TOP-OF-SLOPE



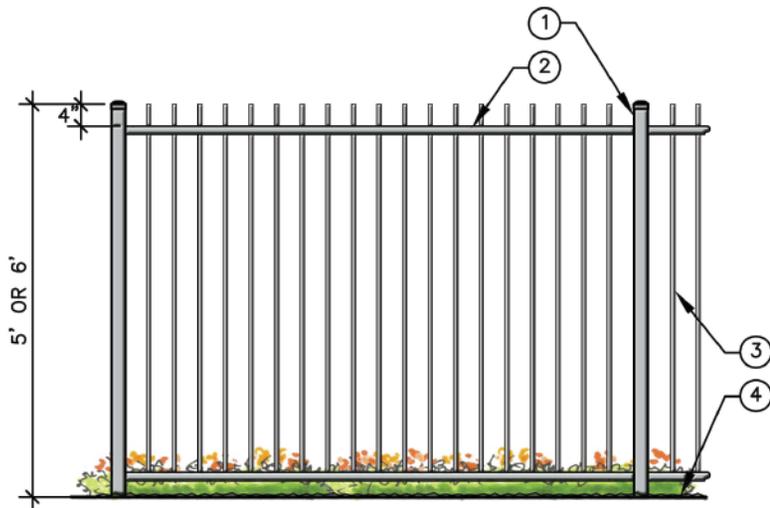
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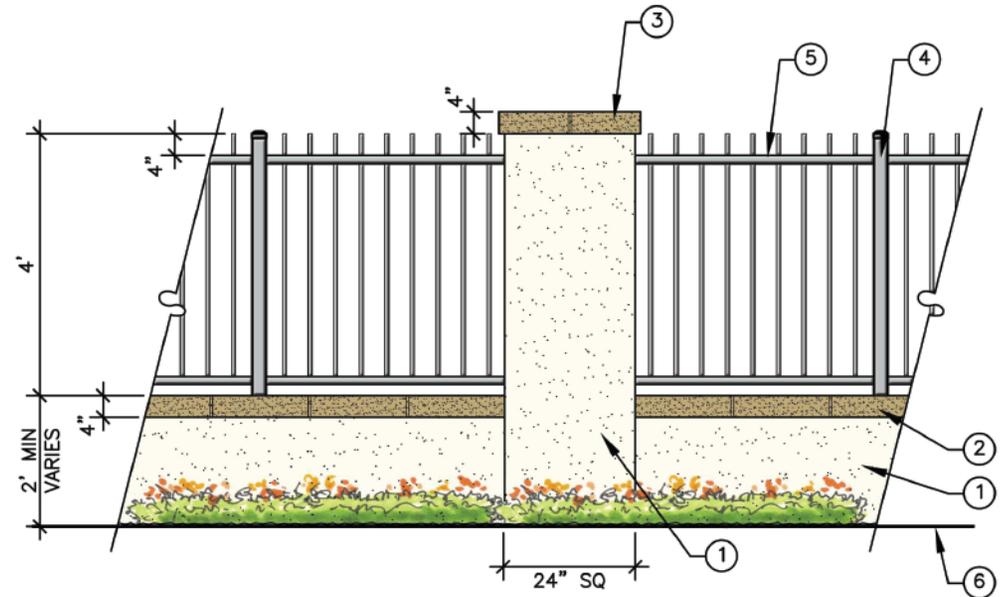
Source: Sitescapes





- ① 3" SQ. TUBULAR STEEL POST
@ 6' O.C.
- ② 1X3 TOP RAIL
- ③ 3/4" SQ. TUBULAR STEEL
PICKETS
- ④ FINISH GRADE

Tubular Steel Fence

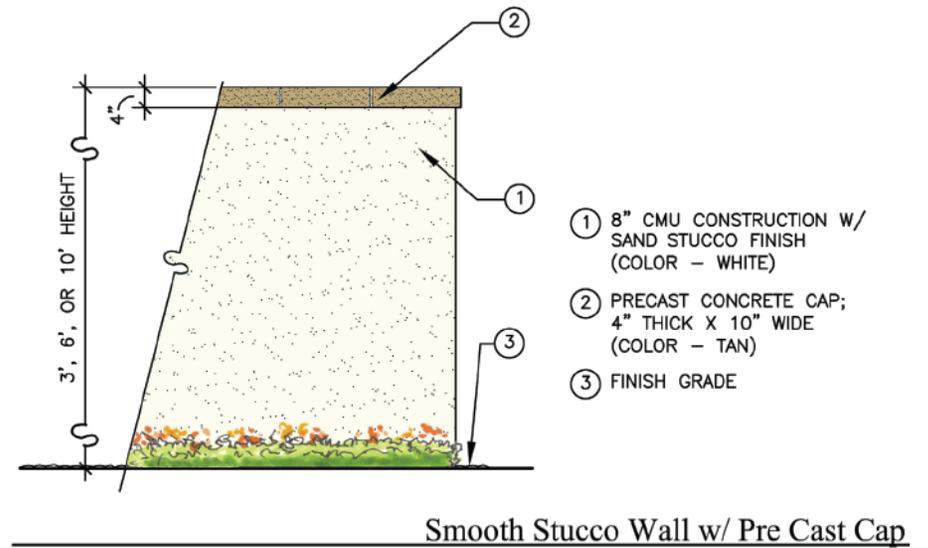
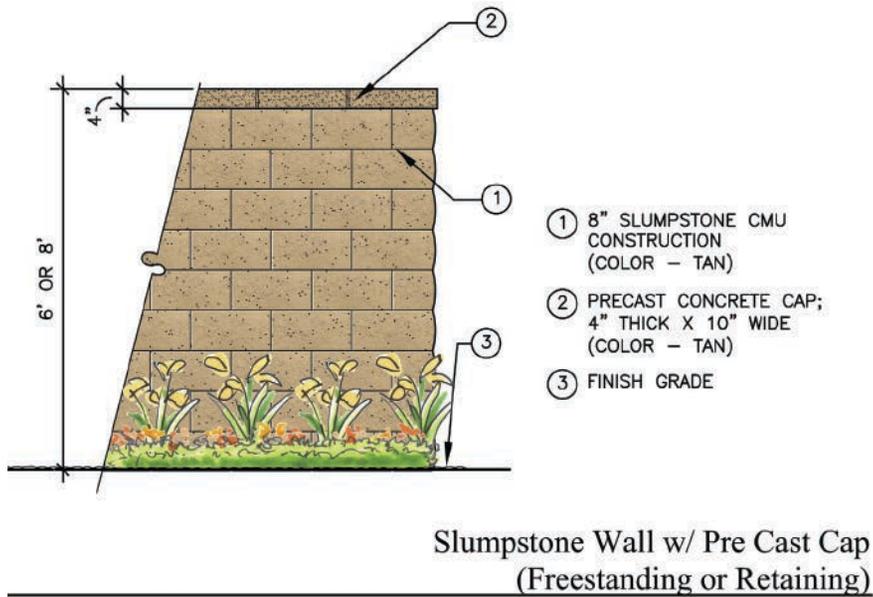


- ① 8" CMU CONSTRUCTION W/ SMOOTH
STUCCO FINISH (COLOR - WHITE)
- ② PRECAST CONCRETE CAP; 4" THICK X
10" WIDE (COLOR - TAN)
- ③ 24" SQ. PILASTER W/ STUCCO FINISH
& CAP TO MATCH WALL. SEE PLAN
FOR PILASTER LOCATIONS
- ④ 3" SQ. TUBULAR STEEL POST
@ 6' O.C.
- ⑤ 1X3 TOP RAIL
- ⑥ FINISH GRADE

Combination Wall

Source: SITESCAPES





Source: SITESCAPES



landscape materials within the project area. The special hardscape design elements shall include, but not be limited to the following:

- Decorative paving such as colored stamped AC, colored concrete, concrete bands, or similar enhancements are encouraged at major entries;
- Light Fixtures (including tree up-lights);
- Bollards;
- Benches;
- Trash Receptacles; and/or
- Signage.

8. LIGHTING

The design issue of lighting includes street lighting, as well as building and landscape accent lighting, and sign illumination. The following basic principles shall be considered in the provision of lighting:

1. Street lights shall provide a safe and desirable level of illumination for both motorists and pedestrians without intruding into residential areas;
2. Lighting fixtures shall relate to the human scale, especially in pedestrian areas;
3. Lighting and lighting fixtures shall complement the design and character of each area in which they are placed;
4. All lighting shall be pedestrian oriented and friendly but shall not be obtrusive or offensive;
5. All street lighting shall be decorative and conform to City standards or an approved theme lighting program, and shall be approved by the City Engineer;

6. Illuminated entries shall consider direct lighting low to the ground and be limited to only the immediate vicinity of the entry;
7. Lighting of entries shall not be distracting, create visual hot spots, or glare;
8. Lighting shall be designed to minimize light spillage onto adjacent and nearby properties; and
9. Light fixtures shall be decorative and located throughout the project, as approved by the City of Upland Planning Division.



V. IMPLEMENTATION PLAN

A. DESIGN REVIEW

The Upland Crossing Specific Plan will be processed through the City of Upland’s two-part development review process: 1) the Planning Approval process, and 2) the Plan Checking process, as appropriate. The Planning Approval process involves review of plans for zoning compliance and design review (see Chapter 17.16 of the City of Upland Municipal Code). Prior to the issuance of grading and building permits which may require approval by the City of Upland Planning Commission, the project also will be processed through Plan Checking, where a detailed technical review of the working drawings will occur. During this review process, the City will determine whether the drawings are in conformance with this Specific Plan, the Building Code, City Standards, and Conditions of Approval.

1. SUBSEQUENT APPLICATION PROCESS

Approval of the Upland Crossing Specific Plan indicates acceptance by the City Council of a general framework of development. Part of that framework establishes specific development standards that constitute the zoning regulations for the Upland Crossing Specific Plan. It is further anticipated that this Specific Plan will be implemented through a final subdivision map, parcel map, and tentative tract maps.

It is anticipated that certain modifications to the Specific Plan text, exhibits, and/or project may be necessary during the development of the Upland Crossing Specific Plan. Any modifications to the Specific Plan shall occur in accordance with the subsequent application process described in this section.

Modifications to the Specific Plan may occur through two separate processes. The first category, “Substantial Conformance,” as defined

below, allows for minor changes to the Specific Plan without a public hearing and may be approved by the Development Services Director. All other proposed changes are considered “Formal Amendments” and are required to be reviewed for approval by the Planning Commission and the City Council. In all cases amendments must be found to be in substantial conformance with the goals and standards of the Upland Crossing Specific Plan.

Amendments may be requested at any time pursuant to Section 65453(a) of the California Government Code. If the amendment is deemed major by the Director of Development Services, it will be processed in the same manner as the original Specific Plan. Proposed amendments deemed to be minor modifications, as defined herein, will be processed administratively by the Administrative Review process.

Depending on the nature of the proposed Specific Plan Amendment, a supplemental analysis may be required, pursuant to the California Environmental Quality Act (CEQA), Section 15162.

1. SUBSTANTIAL CONFORMANCE

Upon determination by the Development Services Director, certain modifications to the Specific Plan text, graphics, and/or project design may not require a Formal Amendment (i.e., through public hearing). The following minor modifications to the Specific Plan do not require a Specific Plan Amendment and are subject to review and approval by the Development Services Director. The Development Services Director shall retain the discretion to refer any such request for modification to the Planning Commission.



Planning Commission approval is required for the following changes:

- Expansions or reductions of the net acreage covered by a given Planning Area within the Specific Plan by no more than 10% of that stated within this Specific Plan;
- Relocation of the recreation area in Planning Area 2 to another area of the project site so long as the size and amenities are consistent with this Specific Plan; and
- Modifications to Architectural Design Guidelines that include the addition of new styles.

The following changes will be subject to the review and approval by the Administrative Committee:

- Realignment or modifications of streets serving the project, lot lines, easement locations, and grading adjustments;
- Implementation of additional landscape treatments;
- Modifications to the phasing plan;
- Site plan modifications; and
- Transfer of dwelling units between Planning Areas, pursuant to Subsection V.E.

Design Review approval is required for the following changes:

- Landscape, wall material, wall alignment, and streetscape modifications which are consistent with the design guidelines contained in this Specific Plan;

- Modifications to Architectural Design Guidelines that include variation of materials within the particular architectural style and minor variations in colors; and
- Modifications to unit or building size that do not exceed the number of units analyzed in the Environmental Impact Report.

2. FORMAL AMENDMENTS

All Specific Plan modifications which do not meet the criteria of a Substantial Conformance as defined in this section shall be deemed to require a Formal Amendment. This Specific Plan was prepared pursuant to California Government Code 65450, *et. seq.* Amendments to the Specific Plan shall be processed in accordance with the applicable requirements of the law, which include Section 65450, *et. seq.* of the California Government Code and with Chapter 17.16 of the City of Upland Municipal Code.

C. CONDITIONAL USE PERMITS

For procedures to implement and approve Conditional Use Permits (CUPs) within the Upland Crossing Specific Plan boundaries, refer to Chapter 17.16.020 of the City of Upland Municipal Code.

Proposals for amending zone boundaries or classifications of property uses within such zones as are defined by this part, or the granting of any other amendments, conditional use permits or variances as provided herein shall be set for public hearing and public notice given of the intention to consider the granting of such proposals in accordance with the procedures and conditions in the Municipal Code.

D. PHASING

Construction of the Upland Crossing Specific Plan project will be



phased in a logical sequence, in response to market demands. Development of either the project west of the San Antonio Creek Channel or east of the Channel may precede the other.

Improvements associated with the west side of the Channel include:

1. Prior to issuance of production permits, applicant shall construct the following improvements:
 - a. Sewer:
 - The main along Monte Vista south of the project limits to the existing City interceptor near Monte Vista Avenue and Richton Drive in Montclair.
 - The upstream end of the sewer main system shall terminate at the northerly limits of the project at Monte Vista Avenue.
 - In-tract sewer to be determined based on the City approved Phasing Plan.
 - b. Water:
 - Perimeter 12-inch water line backbone looping system in Zone 2 along 11th Street between the San Antonio Channel and Monte Vista Avenue and Monte Vista between 11th and Foothill.
 - Pressure reducing station connecting Zones 1 and 2.
 - In-tract water to be determined based on the City approved Phasing Plan.
 - c. Drainage:
 - Project drainage, including 11th Street, shall be conveyed beneath the United States Army Corps of Engineers; San

Antonio Creek Channel. Facilities shall be designed to the satisfaction of the City and Army Corps.

- In-tract drainage to be determined based on the City approved Phasing Plan.
- d. Streets:
 - 11th Street – installation of curb, gutter, asphalt, and sidewalk (north side only) from Monte Vista to Army Corp Channel boundary.
 - Monte Vista – construction of EVA access across existing median.
 - “Y” Street – installation of curb, gutter, asphalt, and sidewalk from 11th Street north to intersection of “V” Street.
 - “V” Street – installation of curb, gutter, asphalt, and sidewalk from intersection of “Y” Street west to Monte Vista Avenue.
 - Remaining In-tract streets to be determined based on the City approved Phasing Plan.
2. Prior to the first certificate of occupancy, applicant shall complete the following:
 - 11th Street – installation of sidewalk, landscaping and irrigation on north side only.
 - Monte Vista – installation of sidewalk, landscaping and irrigation on east side and median.
 - Foothill Boulevard – installation of sidewalk, landscaping and irrigation from western tract boundary to Dewey Way.
 - “Y” Street – installation sidewalk from 11th Street north to intersection of “V” Street.



- “V” Street – installation sidewalk from intersection of “Y” Street west to Monte Vista Avenue.
 - Primary entry at “Y” Street and 11th Street shall be installed and landscaped.
 - Secondary exit/EVA at “V” Street and Monte Vista shall be installed and landscaped.
3. Prior to the 25th Certificate of occupancy, applicant shall complete the following:
- Construction of the community recreation center including pool and community garden.
 - Construction of the Emergency Vehicle Access to Foothill Boulevard, including revisions to the median per the approved improvement plans.
 - Construction of the 11th Street Bridge across the United States Army Corps of Engineer channel.
 - 11th street – installation of curb gutter, asphalt, and sidewalk (north side only) from San Antonio Creek Bridge east to Dewey Way, including landscaping on north side only.

Improvements associated with the east side of the Channel include:

1. Prior to issuance of production permits, applicant shall construct the following improvements:
- a. Sewer:
- System connecting the east side of the property to the existing sewer in Dewey Way.
 - In-tract sewer to be determined based on the City approved Phasing Plan.

- b. Water:
- Perimeter 12-inch water line backbone looping system in Zone 2 in 11th Street between the San Antonio Channel and Monte Vista Avenue, and Monte Vista between 11th and Foothill.
 - Pressure reducing station connecting Zones 1 and 2.
 - In-tract water to be determined based on the City approved Phasing Plan.
- c. Drainage:
- Project drainage, including 11th Street, shall be conveyed to the existing City storm drain system in the Dewey right-of-way.
 - In-tract drainage to be determined based on the City approved Phasing Plan.
- d. Streets:
- 11th Street – installation of curb, gutter, asphalt from Dewey Way west to Army Corp Channel boundary.
 - Dewey Way – installation of curb, gutter, asphalt from 11th Street to Foothill Boulevard.
 - Foothill Boulevard – installation of sidewalk from Dewey Way west to boundary of Tract 18249.
 - “M” Street – installation of curb, gutter, and asphalt.
 - “Q” Street – installation of curb, gutter, and asphalt.
 - “F” Street – installation of curb, gutter, and asphalt from Dewey Way to intersection of “Q” Street.
 - Remaining in-tract streets to be determined based on the City approved Phasing Plan.



2. Prior to the first certificate of occupancy, applicant shall complete the following:
 - 11th Street – installation of sidewalk, landscaping and irrigation on north side only.
 - Dewey Way – installation of sidewalk, landscaping and irrigation on west side only.
 - Foothill Boulevard – installation of landscaping and irrigation from Dewey Way west to the boundary of Tract 18249.
 - Primary entrance at “M” Street and Dewey Way to be installed with landscaping and irrigation
 - Secondary entrance at “F” Street and Dewey Way to be installed with landscaping and irrigation.

3. Prior to the 25th Certificate of occupancy, applicant shall complete the following:
 - Construction of the community recreation center including pool and community garden in Tract 18249.
 - Construction of the “F” Street Bridge.
 - Construction of “F” Street from “Q” Street intersection west to bridge.
 - Construction of the 11th Street Bridge across the United States Army Corps of Engineer channel.
 - 11th street – installation of curb gutter, asphalt, and sidewalk (north side only) from San Antonio Creek Bridge west to Monte Vista Avenue, including landscaping on north side only.

E. MAINTENANCE REQUIREMENTS

Maintenance of landscaping, utilities, and infrastructure will be maintained by a variety of parties as outlined in this section:

1. **HOMEOWNERS ASSOCIATION**

- Common Open Space Landscaping
- Community Gardens
- Front Yard Landscaping and Area Drains
- Paseo Landscaping and Area Drains
- San Antonio Creek Channel Landscaping
- “F” Street Bridge
- In-Tract Parkway Landscaping
- 11th Street Parkway Landscaping (north side only)
- Monte Vista Parkway Landscaping (east side only - excludes median)
- Foothill Boulevard Parkway Landscaping (south side only - excludes median)
- Dewey Way Parkway Landscaping (west side only – excludes median)
- In-Tract Private Streets

2. **CITY OF UPLAND**

- Water System
- Sewer System
- Storm Drain System (within streets)
- 11th Street Bridge
- Public Streets

3. **SAN BERNARDINO COUNTY FLOOD CONTROL DISTRICT**

- Access Roads
- Flood Control Channel



The community gardens and other passive open space within the project will be maintained by the Homeowners Association. A resident may enter into a contract with the Homeowners Association or a 3rd Party of the HOA which provides the rules of conduct and aesthetics to ensure on-going maintenance of the community garden plots. The HOA's contract landscape company will maintain plots that are out of compliance with the contract, and will maintain vacant plots and common areas within the garden. This maintenance provision shall be included in the CC&R's submitted to the City for review and approval. Prior to recordation of a Final Map, the applicant shall submit the CC&R's to the Planning Department for review and approval by the City Attorney.

VI. GENERAL PLAN CONSISTENCY ANALYSIS

This Specific Plan provides a site specific, detailed description of regulations, standards, and guidelines for implementing General Plan goals and policies. To achieve this, the Specific Plan must be in conformance with and be consistent with the General Plan. The California Government Code states that a *Specific Plan shall include a statement of the relationship of the Specific Plan to the General Plan, and further, that it may not be adopted or amended unless found to be consistent with the General Plan.*

Consistency with the General Plan is achieved when the various land uses within the Specific Plan are compatible with the objectives, policies, general pattern of land uses and programs contained in the General Plan. Consistency is defined as follows: *An action, program, or project is consistent with the General Plan if, considering all its aspects, it will further the objectives and policies of the General Plan and not obstruct their attainment.* This statement from the Governor’s Office of Planning and Research (OPR) describes how a Specific Plan should be consistent with the General Plan.

The Upland Crossing Specific Plan implements the goals and policies of the City of Upland’s General Plan. As such, the Upland Crossing Specific Plan is consistent with the goals and policies of the General Plan (Source: City of Upland General Plan). The Upland Crossing Specific Plan serves to implement the following General Plan Goals:

1. Land Use Element Goal 1C: Create cohesive neighborhoods with compatible land use patterns.

Strategy:

1. *Designate appropriate areas to meet the demand for diverse types of housing, with properly related amenities and facilities.*

The Upland Crossing Specific Plan implements this goal by providing several residential product type options in an integrated project within walking distance to on-site recreation areas, open space and trails. The Specific Plan also provides complementary landscaping to create a cohesive and desirable community.

2. Circulation Element Goal 1: To develop transportation planning, services and facilities that are coordinated with and support the Land Use Plan.

Strategies:

1. *Any new development of property shall be required to provide adequate right-of-way width for possible future needs and to provide for traffic patterns necessary to accommodate future growth needs.*
2. *Private streets shall be permitted only where they are found to satisfy applicable City standards and where their maintenance and operation can be shown to satisfy City requirements.*

The Upland Crossing Specific Plan implements this goal by providing adequate right-of-way widths on Foothill Boulevard, Monte Vista Avenue, 11th Street, and the future Dewey Way to accommodate future traffic patterns within the Specific Plan area. All interior streets within the Specific Plan area are private and will be maintained by the homeowners associations which will meet all applicable City standards.

3. **Circulation Element Goal 2: To minimize the impact of existing and future roadways on adjacent land uses, particularly residential, and ensure compatibility between land uses and roadway facilities to the greatest extent possible.**

Strategies:

1. *Direct access of new residential developments onto major arterial streets shall be discouraged.*
2. *Wherever possible, a buffer zone shall be required between residential land uses and arterial highway facilities.*
3. *All roadways shall be encouraged to be designed in a manner that will enhance the interplay of vehicular and pedestrian safety.*

The Upland Crossing Specific Plan implements this goal by providing direct access into the new neighborhood via entrances on 11th Street and Dewey Way, which will alleviate congestion on Foothill Boulevard. An approximate 64-foot landscaped area between Foothill Boulevard and the residential development will act as a buffer zone for noise and air pollutants. All project roadways will be designed for adequate landscaping to separate the pedestrian pathway from the road.

4. **Circulation Element Goal 3: To accommodate alternative modes of transportation to the private automobile in the City, including non-motorized transportation (bicycle and pedestrian), public transportation and recreational trails.**

Strategy:

1. *All new development shall be required to provide sidewalks, in accordance with the Master Plan of Streets and Highways.*

The Upland Crossing Specific Plan implements this goal by providing sidewalks on all perimeter streets and interior private streets.

5. **Circulation Element Goal 4: To achieve a balance between parking supply and demand so that an adequate supply of parking is provided to meet the demands generated by the Land Use Element.**

Strategy:

1. *All new development shall be required to provide adequate parking to meet their parking demands on-site or in consolidated parking facilities in close proximity to their site.*

The Upland Crossing Specific Plan provides two parking spaces per residential unit in the form of a garage, as shown in Table 4, Residential Development Standards. In addition, the Specific Plan provides 0.5 guest parking space per residential unit which will provide adequate parking for this residential community.

6. Circulation Element Goal 5: To promote the aesthetic qualities of the street system.

Strategies:

1. *Wherever feasible, street construction and improvement projects shall be designed with a concern for street aesthetics, including street trees, landscaping and paving materials.*
2. *All new development shall be encouraged to provide landscaped parkways, appropriate pedestrian amenities and other streetscape improvements that improve the aesthetics of the roadway to both vehicular and pedestrian traffic.*

The Upland Crossing Specific Plan implements this goal by providing landscaping with appropriate scale trees, shrubs, grass, and pedestrian pathways that will be aesthetically pleasing as well as provide project identity.

7. Scenic Highways Goal 1: To protect and enhance the scenic attributes of Euclid Avenue, Mountain Avenue, Foothill Boulevard and Benson Avenue.

Objective:

1. *To encourage the design of road and street improvements that will enhance vehicular and pedestrian safety.*

The Upland Crossing Specific Plan implements this goal by providing landscaping on Foothill Boulevard with appropriate scale trees, shrubs, grass, and pedestrian pathways that will be aesthetically pleasing and will enhance the scenic attributes of

this road. The pedestrian pathway will be adequately setback to provide safety from vehicles passing by.

8. Hazards Goal 1: To provide a safe and healthful environment for the Upland community.

Objectives:

1. *To reduce loss of life, injuries, and damage to property caused by seismic events and seismic-related conditions.*
2. *To reduce loss of life, injuries, damage to property, and loss of natural resources caused by wildland and urban fires.*
3. *To protect life and property in the event of a natural disaster.*
4. *To prevent injury or loss of life and damage to property due to flood hazards.*

The Upland Crossing Specific Plan implements this goal by providing residential homes that will be built according to the design standards of the City building code. The specific plan area is not located in a high fire hazard zone. Adequate drainage flow will be implemented to prevent injury or loss of life and damage to any property on site due to flood hazards.

9. **Open Space/Conservation Goal 1: To provide a sufficient range of recreation opportunities to meet the needs of all individuals (all ages), families, and groups who reside in the City of Upland.**

Objective:

1. *To coordinate the development of parks and recreation with other elements in the community to insure the most logical location and, where possible, combine with other compatible facilities such as schools, flood control, or water conservation areas, etc.*

The Upland Crossing Specific Plan implements this goal by providing a recreation area located in the central portion of the specific plan area, adjacent to the San Antonio Creek Channel. This recreation area will provide sufficient open space and recreation for all individuals, families and groups residing in this project area, including a swimming pool, barbeque area, and tot lot.

AIRPORT LAND USE PLAN CONSISTENCY:

10. **Noise Element Goal 1: To plan for an appropriate range of land uses within areas impacted by noise emanating from airport operations which uses would not be substantially adversely affected by such nuisances and/or disturbances.**

The Upland Crossing Specific Plan implements this goal by providing recreational uses that are less sensitive to the noise emanating from the nearby airport. In addition, 6-foot walls, or a height appropriate as recommended by the draft Noise Impact Analysis dated March 2006, will be built around the residential uses to mitigate the noise per the noise study conducted for this specific plan area. All residential development will be

acoustically insulated to reduce interior noise level to no greater than 45 dBA CNEL.

11. **Airport Height Restrictions (Obstructions) Goal 1: To ensure the safe passage of aircraft in, out and around the airport by safeguarding and preserving navigable airspace.**

The Upland Crossing Specific Plan Area is located outside the published flight pattern of nearby Cable Airport, and the building height of all proposed buildings in the specific plan area are in conformance according to the Federal Aviation Regulations Part 77 (FAR Part 77).

12. **Airport Safety Element Goal 1: To minimize the level of risk to people and property from accident involving aircraft.**

The Upland Crossing Specific Plan Area is located outside Safety Area 1 and is located in Safety Area 2- Moderate Crash Hazard. All structures in this specific plan area will not penetrate the airport imaginary surfaces as defined in Federal Aviation Regulations Part 77. All structures in this area will not reflect glare, emit electronic interference, or produce smoke so as to endanger aircraft operations.